

City of Fort Bragg



Application for Active Transportation Program Grant

City of Fort Bragg Chestnut Street Multi-Use Facility and SRTS Program

May 19, 2014

City of Fort Bragg – Chestnut Street Multi-Use Facility and SRTS Program
Application for Active Transportation Program Grant

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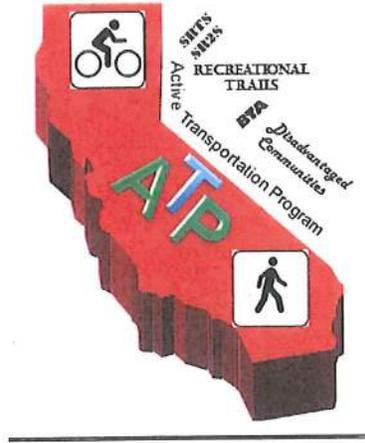
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ACTIVE TRANSPORTATION PROGRAM CYCLE 1

APPLICATION Part 1 (Includes Sections I, V, VI, VII, VIII & XI)

Please read the Application Instructions at
<http://www.dot.ca.gov/hq/LocalPrograms/atp/index.html>
prior to filling out this application

Project name: City of Fort Bragg - Chestnut Street Multi-Use Facility and SRTS Program

For Caltrans use only: TAP STP RTP SRTS SRTS-NI SHA
 DAC Non-DAC Plan

I. GENERAL INFORMATION

Project name: City of Fort Bragg - Chestnut Street Multi-Use Facility and SRTS Program

(fill out all of the fields below)

1. APPLICANT (Agency name, address and zip code) City of Fort Bragg, 416 North Franklin Street Fort Bragg, CA 95437	2. PROJECT FUNDING ATP funds Requested \$ <u>259,000.00</u> Matching Funds \$ _____ (If Applicable) Other Project funds \$ <u>792,000.00</u> TOTAL PROJECT COST \$ <u>1,051,000.00</u>
3. APPLICANT CONTACT (Name, title, e-mail, phone #) Tom Varga, Public Works Director tvarga@fortbragg.com 707-961-1592	5. PROJECT COUNTY(IES): <p style="text-align: center;">Mendocino</p>
4. APPLICANT CONTACT (Address & zip code) 416 North Franklin Street Fort Bragg, CA 95437	7. Application # <u>1</u> of <u>1</u> (in order of agency priority)
6. CALTRANS DISTRICT #- Click Drop down menu below District 1	

Area Description:

8. Large Metropolitan Planning Organization (MPO)- Select your "MPO" or "Other" from the drop down menu>	Other
9. If "Other" was selected for #8- select your MPO or RTPA from the drop down menu>	Mendocino COG
10. Urbanized Area (UZA) population (pop.)- Select your UZA pop. from drop down menu>	Small Urban (Pop =or<200,000 but > than 5,000)

Master Agreements (MAs):

11. Yes, the applicant has a FEDERAL MA with Caltrans.
12. Yes, the applicant has a STATE MA with Caltrans. 01-5088
13. If the applicant does not have an MA. Do you meet the Master Agreement requirements? Yes No
 The Applicant MUST be able to enter into MAs with Caltrans

Partner Information:

14. Partner Name*: N/A	15. Partner Type N/A
16. Contact Information (Name, phone # & e-mail) N/A	17. Contact Address & zip code N/A

Click here if the project has more than one partner; attach the remaining partner information on a separate page

*If another entity agrees to assume responsibility for the ongoing operations and maintenance of the facility, documentation of the agreement must be submitted with the application, and a copy of the Memorandum of Understanding or Interagency Agreement between the parties must be submitted with the request for allocation.

Project Type: (Select only one)

18. Infrastructure (IF) 19. Non-Infrastructure (NI) 20. Combined (IF & NI)

Project name: City of Fort Bragg - Chestnut Street Multi-Use Facility and SRTS Program

I. GENERAL INFORMATION-continued

Sub-Project Type (Select all that apply)

21. Develop a Plan in a Disadvantaged Community (select the type(s) of plan(s) to be developed)
 Bicycle Plan Safe Routes to School Plan Pedestrian Plan
 Active Transportation Plan

(If applying for an Active Transportation Plan- check any of the following plans that your agency already has):

- Bike plan Pedestrian plan Safe Routes to School plan ATP plan

22. Bicycle and/or Pedestrian infrastructure
Bicycle only: Class I Class II Class III
Ped/Other: Sidewalk Crossing Improvement Multi-use facility

Other: _____

23. Non-Infrastructure (Non SRTS)
 24. Recreational Trails*- Trail Acquisition

***Please see additional Recreational Trails instructions before proceeding**

25. Safe routes to school- Infrastructure Non-Infrastructure

If SRTS is selected, provide the following information

26. SCHOOL NAME & ADDRESS: Redwood Elementary School, 324 South Lincoln Street, Fort Bragg, CA.
27. SCHOOL DISTRICT NAME & ADDRESS: Fort Bragg Unified School District, 312 South Lincoln Street, Fort Bragg, CA 95437

28. County-District-School Code (CDS) 6025126	29. Total Student Enrollment 450	30. Percentage of students eligible for free or reduced meal programs ** 74.82
31. Percentage of students that currently walk or bike to school 12%	32. Approximate # of students living along school route proposed for improvement 80	33. Project distance from primary or middle school Directly adjacent, 0 feet.

**Refer to the California Department of Education website: <http://www.cde.ca.gov/ds/sh/cw/filesafdc.asp>

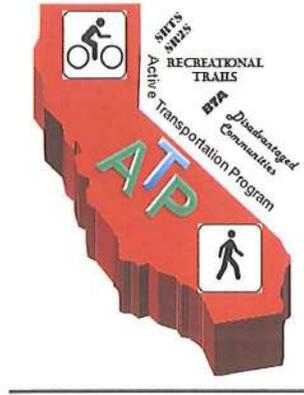
- Click here if the project involves more than one school; attach the remaining school information including school official signature and person to contact, if different, on a separate page

ADDITIONAL SCHOOL INFORMATION, FOR SECTION I. GENERAL INFORMATION

ADDITIONAL SCHOOLS IN THE PROJECT AREA:

Dana Gray Elementary School		
1197 Chestnut Street, Fort Bragg, CA 95437		
SCHOOL DISTRICT NAME & ADDRESS:		
Fort Bragg Unified School District		
312 South Lincoln Street, Fort Bragg, CA 95437		
County District School Code (CDS)	Total Student Enrollment	Percentage of students eligible for free or reduced meal programs
6025100	392	66.33
% of students that currently walk or bike to school	Approx number of students living along school route	Project distance from primary school
26%	80	Directly adjacent; 0 feet

Fort Bragg High School		
300 Dana Street, Fort Bragg, CA 95437		
SCHOOL DISTRICT NAME & ADDRESS:		
Fort Bragg Unified School District		
312 South Lincoln Street, Fort Bragg, CA 95437		
County District School Code (CDS)	Total Student Enrollment	Percentage of students eligible for free or reduced meal programs
2331361	513	59.12
% of students that currently walk or bike to school	Approx number of students living along school route	Project distance from high school
No current data	80	Directly adjacent; 0 feet



ACTIVE TRANSPORTATION PROGRAM CYCLE 1

APPLICATION Part 2 (Includes Narrative Sections II, III & IV)

II. PROJECT INFORMATION

(Please read the "ATP instructions" document prior to attaching your responses to all of the questions in Sections II. Project Information, Section III. Screening Criteria and Section IV. Narrative Questions - 20 pages max)

1. **Project Location** The infrastructure project is located along Chestnut Street in the City of Fort Bragg in Mendocino County, California. Chestnut Street begins at State Route One (Main Street) and heads east to the Fort Bragg City Limits, east of Fort Bragg High School. The scope of this project begins at the intersection of Chestnut and Franklin Streets, located one block east of State Route One (Main Street), and continues east along Chestnut Street, a distance of approximately 4800 feet, to just east of the intersection of Chestnut Street and Ebbing Way, at Fort Bragg High School. The non-infrastructure component will occur at Redwood Elementary, Dana Gray Elementary, Fort Bragg Middle School and Three Rivers Charter School. See project map (Attachment 1).

2. **Project Coordinates**

Latitude 39° 26'10.63"
(Decimal degrees)

Longitude 123° 47'
(Decimal degrees)

3. **Project Description:**

Construction of an eight to twelve-foot wide multi-use pedestrian and bicycle facility on the north side of Chestnut Street for a distance of approximately 4800 feet from the intersection of Chestnut and Franklin Streets, to the Fort Bragg High School driveway 140 feet east of the intersection of Chestnut Street and Ebbing Way. The plan features:

- Proposed street sections that can be implemented within the limits of existing rights-of-way;
- New curb and gutter and a new multiuse path replacing existing narrow walkways along the north side of the street;
- Permitted on-street parking shifted from the south side of Chestnut Street to the north side;
- ADA compliant ramps and intersections;
- Relocated joint service utility poles, street signs and drainage inlets; and
- Traffic calming bulbouts and bump outs

Accessibility and Safety Features: Improved pedestrian accessibility will be provided along the north side of Chestnut Street with the completion of a continuous, widened multiuse trail, the removal and City of Fort Bragg – Chestnut Street Multi-Use Facility and SRTS Program

relocation of obstacles, the construction of ADA compliant ramps, and parking restriction near each street intersection, driveway and alley crossing. Bulbouts at McPherson Street and Whipple Street and bump outs placed adjacent to parking spaces will encourage slower traffic speeds within the Chestnut Street corridor.

Non-Infrastructure Component: The non-infrastructure component of the project involves continuation of Education, Encouragement, Enforcement, and Evaluation activities previously funded through SRTS funding, including classroom curriculum, presentations at assemblies, development of a SRTS webpage or website, development of SRTS maps, "Safety Tip of the Month" at each school site, educational materials to be distributed at community events, annual IWALK days, weekly walk to school program, walking school buses & bicycle trains, park & walk and carpooling programs, "caught you being good" campaign, enhanced police patrols, neighborhood watch, and collection and submittal of data using student tally and parent survey forms.

4. Project Status

The public has participated in the design of the project (Attachment 6, Public Participation) and the project plan was developed in 2012. The project is currently partially funded with Federal funds (DEMO6L-5088[028]). A Preliminary Environmental Study (PES) was completed and signed on January 27, 2014. Based on the PES, NEPA documentation is expected to be a Categorical Exclusion with required technical studies (23 CFR 771 activity (c)(3) construction of bicycle and pedestrian lanes, paths, and facilities). The needed studies are currently in progress and expected to be completed by August 15, 2014. The project is expected to meet CEQA exemption 15301(c) (existing highways and streets, sidewalks, gutters, bicycle and pedestrian trails, and similar facilities [this includes road grading for the purpose of public safety]). CEQA and NEPA determinations will be made when technical studies are completed.

Additional right of way acquisition is not needed for this project. Preliminary plans have been completed and approved by City Council as shown in the Chestnut Corridor Conceptual Design and Right of Way Feasibility Study.

III. SCREENING CRITERIA

1. Demonstrated Needs of the Applicant

The project purpose is to improve safety and provide for safe bicycle and pedestrian facilities along the main route to Dana Gray Elementary (third through fifth grades), Redwood Elementary (Kindergarten through second grades), and Fort Bragg High School (ninth through twelfth grades) of the Fort Bragg Unified School District.

The project is needed because pedestrian facilities are inadequate and no bicycle facilities exist in the project area. Existing Chestnut Street sidewalks are typically three to 3½ feet in width except for limited locations adjacent to school properties and within existing commercial zones. In addition, there are numerous physical obstructions including utility poles, fire hydrants and drainage structures, and existing gaps in the walkways which impede safe, continuous pedestrian access. Improved walkways rank as a high priority for Chestnut Street residents.

The goals of the project are to increase safety for bicycles and pedestrians by providing appropriate pedestrian and bicycle facilities, and by calming vehicle traffic, and to encourage increased walking and bicycling to reduce greenhouse gases and improve public health.

The proposed 8-12 foot wide multi-use path is expected to improve active transportation mobility to schools, community centers, and recreational fields by providing a wide, ADA compliant path for use by pedestrians, wheelchairs, parents with strollers, bicycles and other active transportation modes where the current sidewalk is insufficient and no bicycle facility exists.

Non-infrastructure funding will be used to launch important new Education and Encouragement activities that were recommended in the City's *Safe Routes to School Proposed Outreach Program* (June 2009) but have been only partially implemented to date. Fort Bragg Outreach Program activities will complement and fill gaps in the **Mendocino County Safe Routes to School Plan**, reinforcing the County's awareness and education campaign and ensuring a consistent message in Coastal Mendocino County's incorporated and unincorporated areas. Fort Bragg's program will focus on safe use of new pedestrian and bikeways constructed with ATP infrastructure funding, as well as prior Safe Routes

program are to increase safe walking and bicycling and also to encourage non-vehicular transportation as a means to improve overall health. The need for the Fort Bragg program was highlighted in the City's **2005** and **2011 Residential Streets Safety Plans**, both of which identified priority safety concerns related to areas of pedestrian/bicycle/vehicular conflicts.

2. Consistency with Regional Transportation Plan (100 words or less)

The project is consistent with the 2010 Regional Transportation Plan, adopted September 19, 2011, specifically, "Complete Streets," "Local Streets and Roads" and "Non-Motorized Transportation" goals, objectives and policies for meeting the needs of all surface transportation modes, providing new bicycle facilities where none exist, providing a safe and efficient transportation network, including the addition/improvement of bicycle and pedestrian facilities when planning and implementing local road improvements, and providing a safe and useable network of bicycle and pedestrian facilities throughout the region as a means to lessen dependence on vehicular travel and improve the health of Mendocino County's residents.

IV. NARRATIVE QUESTIONS

1. **POTENTIAL FOR INCREASED WALKING AND BICYCLING, ESPECIALLY AMONG STUDENTS, INCLUDING THE IDENTIFICATION OF WALKING AND BICYCLING ROUTES TO AND FROM SCHOOLS, TRANSIT FACILITIES, COMMUNITY CENTERS, EMPLOYMENT CENTERS, AND OTHER DESTINATIONS; AND INCLUDING INCREASING AND IMPROVING CONNECTIVITY AND MOBILITY OF NON-MOTORIZED USERS. (0-30 POINTS)**

A. Describe how your project encourages increased walking and bicycling, especially among students.

Chestnut Street is the main east/west roadway in the southern sector of town and is the primary route providing access to Redwood Elementary, Dana Gray Elementary, and Fort Bragg High School, all of which are located directly adjacent to Chestnut Street on the eastern side of town. Chestnut is the most widely used street in town for vehicle traffic and pedestrians heading to and from school. Sidewalks are currently an average of three to 3.5 feet wide and contain gaps and obstructions which require students and parents with students and strollers to walk in the adjacent vehicle lanes. There are no current or historic bicycle lanes or other bicycle facilities along Chestnut Street. The proposed multi-use facility will provide a safe path of travel for walking and bicycling students which will encourage more parents to allow their students to walk and bike to and from school.

A. Describe the number and type of possible users and their destinations, and the anticipated percentage increase in users upon completion of your project. Data collection methods should be described.

Students travelling to and from school are the main users for the multi-use facility, however it will also substantially improve safe walking and bicycling conditions for south Fort Bragg residents, as Chestnut Street serves as the main east/west corridor in south Fort Bragg and provides access to the community center, aquatic center, recreational sports fields and the south Fort Bragg neighborhood. According to 2013 Safe Routes to School tallys, approximately 306 students currently walk to and from Dana Gray, Redwood, and Fort Bragg High Schools, and approximately eight students currently bike to school. Chestnut Street is the main route used to get to these schools. A California study showed schools that received infrastructure improvements through the Safe Routes to School Program yielded walking and bicycling increases that were often in the range of 20% to 200%¹. A 30% increase in pedestrians and bicyclists is projected. This increase is

¹ http://www.saferoutespartnership.org/sites/default/files/pdf/school_bus_cuts_national_stats_FINAL.pdf
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attributed to students and parents walking and bicycling to school as a result of increased safety. Current Safe Routes to School responses from parent indicate that safety is one of the main reasons listed for driving students to school rather than walking or bicycling.

- B. Describe how this project improves walking and bicycling routes to and from, connects to, or is part of a school or school facility, transit facility, community center, employment center, state or national trail system, points of interest, and/or park.

The Chestnut Street multi-use facility will be located on the most heavily used route to and from the following schools; Redwood Elementary, Dana Gray Elementary, and Fort Bragg High School. These schools are all located on the easternmost side of town and Chestnut Street, which runs westward nearly a mile to State Route 1 (Main Street).² Chestnut Street would additionally serve pedestrians and bicyclists in south Fort Bragg heading to and from the CV Starr Community Center, located just north of Redwood School. The CV Starr Community Center features a lap swimming pool, a recreational pool with water slide and lazy river, a fitness center, dog park, outdoor BBQ facility, and exercise and enrichment classes for students and the community at large.

- C. Describe how this project increases and/or improves connectivity, removes a barrier to mobility and/or closes a gap in a non-motorized facility.

The multi-use facility will provide ADA compliant pedestrian access for the 0.9 mile length where the current sidewalk is obstructed by utility poles and signs and does not maintain a minimum three foot width at numerous locations nor a minimum five foot width for most of the length of the sidewalk. There are numerous locations where no ramps exist at intersections, preventing wheelchair access to the sidewalk from the crosswalk. Additionally, there are currently no bicycle facilities along the entire length of the Chestnut Street corridor where the improvements are proposed, and bicycles must share the travel lanes with no sharrows or other markings. The project will provide for an off-street facility for bicycles travelling east and westward within the 8- to 12-foot wide multi-use facility. The new facility will connect to north/south Class II bicycle facilities at Franklin Street, Harrison Street, and Lincoln Street, providing an important connection for bicycle facilities in the south Fort Bragg neighborhood (see Attachment 8, Existing Bikeways Map from the 2009 Bike Master Plan).

² State Route 1 (Main Street) is the arterial corridor providing north/south access to and from town.
City of Fort Bragg – Chestnut Street Multi-Use Facility and SRTS Program

- Projects with significant potential- 21 to 30 points
- Projects with moderate potential- 11 to 20 points
- Projects with minimal potential- 1 to 10 points
- Projects with no potential- 0 points

IV. NARRATIVE QUESTIONS- continued

2. POTENTIAL FOR REDUCING THE NUMBER AND/OR RATE OF PEDESTRIAN AND BICYCLIST FATALITIES AND INJURIES, INCLUDING THE IDENTIFICATION OF SAFETY HAZARDS FOR PEDESTRIANS AND BICYCLISTS. (0-25 POINTS)

A. Describe the potential of the project to reduce pedestrian and/or bicycle injuries or fatalities.

According to traffic data provided from the 2011 Residential Streets Safety Plan (Table 3, Page 18), average peak volume traffic counts on Chestnut Street were as high as 318 vehicles per hour (VPH) westbound and 228 VPH eastbound in the morning, and while the average vehicle speeds recorded were between 22 and 28 MPH, disturbingly, speeds as high as 55 MPH were recorded. Sidewalks are narrow, with obstructions and gaps. Pedestrians regularly walk in the roadway. There are no bicycle facilities. According to Police Department Traffic Accident Reports, between 2000 and 2013, 10% of all injury accidents involving bicycles and 3% of all injury accidents involving pedestrians within City Limits occurred on Chestnut Street (See Attachment 9, Police Report Data). The project would provide pedestrians and bicyclists with a proper, unobstructed pathway to eliminate walking and reduce bicycling in the roadway. The plan would relocate the on-street parking aisle to the north side of the road to act as an added separation between the multi-use facility and vehicle traffic, and would include narrowing of the roadway, including the addition of bump-outs and bulbouts, to improve pedestrian safety at crossings and calm traffic.

B. Describe if/how your project will achieve any or all of the following:

- Reduces speed or volume of motor vehicles: Traffic speed is expected to be reduced by the proposed bulbouts and intersections and addition of bump outs in the on-street parking zone, which will narrow the appearance of the roadway. The roadway will additionally be physically reduced in width by four feet to calm traffic and provide width for the multi-use trail.
- Improves sight distance and visibility: Red zones will be added and extended at street corners to improve sight distance, and bulbouts will additionally improve visibility of pedestrians at crossings.
- Improves compliance with local traffic laws: Reduction of speeds through traffic calming measures will improve compliance with speed limits in the project area. Posted speeds for Chestnut Street are 15

IV. NARRATIVE QUESTIONS- continued

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IV. NARRATIVE QUESTIONS- continued

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- Improves compliance with local traffic laws: Reduction of speeds through traffic calming measures will improve compliance with speed limits in the project area. Posted speeds for Chestnut Street are 15

to 25 miles per hour, however average speeds recorded during the 2011 Residential Streets Safety Plan traffic study were 22 to 28 MPH, and speeds as high as 55 MPH were recorded.

o Eliminates behaviors that lead to collisions: The improvements will eliminate the need to walk and bicycle in the vehicle lanes, and will thereby reduce the potential for collisions with motor vehicles. The improvements will additionally increase sight distance and visibility and slow traffic. Educational elements will reinforce these desirable behaviors.

o Addresses inadequate traffic control devices: The plan includes addition of new stop signs at the intersection of Chestnut and Conry Streets to improve safety.

o Addresses inadequate bicycle facilities, crosswalks or sidewalks: The project will provide for bicycle facilities for the 0.9 mile distance of Chestnut Street where none currently exist, will improve narrow and obstructed sidewalks on the north side of Chestnut for this length, and will improve crossings with bulbouts.

C. Describe the location's history of events and the source(s) of data used (e.g. collision reports, community observation, surveys, audits) if data is not available include a description of safety hazard(s) and photos.

Chestnut Street has historically had narrow and obstructed sidewalks and has never had bicycle facilities. Schools have been located at the east end of Chestnut Street for more than 50 years. Photos showing the narrow and obstructed sidewalks are included as Attachment 6. According to Police Department Traffic Accident Reports (see Attachment 9), between 2000 and 2013, 10% of all injury accidents involving bicycles occurred on Chestnut Street, and 3% of all injury accidents involving pedestrians occurred on Chestnut Street.

- Projects with significant potential- 16 to 25 points
- Projects with moderate potential- 8 to 15 points
- Projects with minimal potential- 1 to 7 points
- Projects with no potential- 0 points

IV. NARRATIVE QUESTIONS- continued

3. PUBLIC PARTICIPATION and PLANNING (0-15 POINTS)

- A. Describe the community based public participation process that culminated in the project proposal or plan, such as noticed meetings/public hearings, consultation with stakeholders, etc.

Addressing pedestrian traffic and safety issues on Chestnut Street was first identified as a high priority in the 2005 Residential Streets Safety Plan (2005 RSSP), was further explored as a recommended project as a part of the 2011 Residential Streets Safety Plan (2011 RSSP), and a specific project was identified as the prioritized project in the 2012 Chestnut Street Corridor Conceptual Plan and Right of Way Feasibility Study (Chestnut Street Study).

High priority projects were identified in the 2005 RSSP through public input at a City Council meeting, from City Council members, City staff and local residents. Participants were asked to rank the top five locations of safety concerns from a total of 20 areas identified during other phases of development of the 2005 RSSP. The number one priority area was identified as Fort Bragg High School/Dana Gray Elementary School on Chestnut Street. The area description on page 12 of the study states in part: "Pedestrian travel to/from the schools is hampered by parked cars, trash cans and signs blocking the already narrow sidewalks. Speeding vehicles were observed around the high school in greater proportion than in any other school site. The third ranked priority location was identified as the intersection of Chestnut Street at Sanderson Street. Page 13 of the study states in part, "The roadway is narrow with no bike lanes; the limited sidewalk width is compromised by parked cars and trash cans blocking the walkways."

The 2011 RSSP lists Chestnut Street from Franklin to Dana Street as one of four locations prioritized for safety improvements. Page 23 of the Chestnut Street Study lists concerns including speeding, heavy traffic (key east/west roadway in the southern sector of Fort Bragg), and pedestrian and cross traffic safety issues (high speeds, narrow sidewalks, numerous obstructions). A traffic safety open house, held on April 29, 2011 was noticed in the newspaper (Page 79, 2011 RSSP). Additionally, residents were surveyed – a total of 38 surveys were submitted from Chestnut Street residents (Pages 71-72 2011 RSSP).

B. Describe the local participation process that resulted in the identification and prioritization of the project:

Due to high rates of 2011 RSSP survey participant opposition to removal of off street parking or provision of Right of Way to the City to facilitate safety improvements, the City Council directed staff to further develop feasible alternatives which address the limited rights of way (40 feet) and physical obstacles present along the Chestnut Street corridor, and to address input from the community. The intent of the Chestnut Street Study was to develop a pedestrian and bicycle safety improvement plan with the highest level of public consensus possible.

The Chestnut Street Study public outreach effort included an online survey, a Chestnut Street Facebook page, door to door surveys of Chestnut Street and surrounding area residents, a public workshop held on June 8, 2012, a City Council workshop public meeting on June 25, 2012, and a follow-up presentation at a City Council meeting held on August 27, 2012.

The public workshop was noticed in the newspaper, flyers were provided and sent home with all students of Redwood, Dana Gray and Fort Bragg High Schools, and area residents were notified of the workshop. Approximately 50 residents attended the June 8, 2012 public workshop. Most attendees were project area residents. Also in attendance was a parent representative from the school district, a member of the police department, and a Caltrans traffic safety employee. Participants were in favor of wider sidewalks, adding stop signs, enhanced sidewalks and speed “tables” to reduce traffic speed, ADA improvements such as ramps, and a member of the Police Department suggested additional electronic speed advisory signs. Participants generally agreed that sidewalks must be accessible to everyone. Participants recommended that sidewalk improvements be focused on the north side of the street, and there was support for a multi-use trail on the north side. Participants agreed that a “Safe Route to School” currently does not exist on Chestnut Street and that the do nothing alternative is not acceptable. Residents expressed mixed levels of support for removal of off-street parking, right-of-way acquisition, or conversion of Chestnut Street into a one-way street.

Staff received 37 total survey responses from the door to door and online effort. The ranking effort indicates that improvement of sidewalks and traffic calming are the most important priorities of the survey participants. Bicycle accommodation also ranked as an important priority. Although a couple

participants viewed retention of off-street parking as the most important priority, significantly more respondents viewed retention of on-street parking as less important or the least important priority.

The multi-use path facility design resulted from a substantial public participation process and is a context sensitive solution responding to the need for pedestrian and bicycle transportation improvements, constraints of a right of way as narrow as 40 feet, the infeasibility of right of way acquisition due to the highly developed nature of the corridor and public input indicating low support for right of way acquisition and a strong desire to maintain on-street parking. Links to the 2011 Residential Street Safety Plan and Chestnut Street Corridor Conceptual Plan and Right of Way Feasibility Study are included as an attachment. Additionally, Section III of the Chestnut Street Corridor Conceptual Plan and Right of Way Feasibility Study, which outlines the public participation process, is included as Attachment 6.

C. Is the project cost over \$1 Million? Yes

If Yes- is the project Prioritized in an adopted city or county bicycle transportation plan, pedestrian plan, safe routes to school plan, active transportation plan, trail plan, circulation element of a general plan, or other publicly approved plan that incorporated elements of an active transportation plan? Yes

- Projects with substantial participation of community members- 11 to 15 points
- Projects with moderate participation of community members - 6 to 10 points
- Projects with minimal participation of community members- 1 to 5 points
- Projects with no participation of community members- 0 points

IV. NARRATIVE QUESTIONS- continued

4. COST EFFECTIVENESS (0-10 POINTS)

- A. Describe the alternatives that were considered. Discuss the relative costs and benefits of all the alternatives and explain why the nominated one was chosen.

Final options developed in response to public review and City Council comments are outlined in the 2012 Chestnut Street Corridor Conceptual Plan and ROW Feasibility Study (Chestnut Street Study) on pages III-6 through III-15, included as Attachment 6. Option A is the selected project. Option B consisted of a multi-use path on the south side of the street, a six foot wide sidewalk on the north side, and elimination of on-street parking to accommodate these improvements. Option C consists of a separated 8 foot wide bicycle path and 6 foot wide sidewalk on the north side, reduction to one way vehicle traffic and retention of parking from Harrison to Dana Street, and two-way traffic with elimination of on-street parking between Dana Street and the end of the project area.

Relative costs and benefits of the alternatives are outlined on pages III-14 and III-15 of the Chestnut Street Study. The selected project cost was estimated below one million dollars for construction, whereas the other two options were estimated to cost approximately three million dollars each to construct. In addition to being the lowest cost option, the selected option retains on-street parking by relocating it on the north side to provide a buffer between traffic and the pedestrian/bicycle facility, narrows travel lanes and includes bulbouts and bump outs to calm traffic. Option B, which was not selected, costs three times as much to construct, and would eliminate on-street parking. Option C, which was also not selected, would result in a one way road for a portion of the length of Chestnut Street, which would inconvenience residents, and might result in increased traffic on other local roads, and also costs three times as much as the selected option.

- B. Calculate the ratio of the benefits of the project relative to both the total project cost and funds requested (i.e., $\frac{\text{Benefit}^*}{\text{Total Project Cost}}$ and $\frac{\text{Benefit}^*}{\text{Program Funds Requested}}$).

*Benefits must directly relate to the goals of the Active Transportation Program.

$$\frac{\$1,862,508(\text{Benefit})}{\$1,051,000 (\text{Total Project Cost})} \text{ and } \frac{\$1,862,508(\text{Benefit})}{\$259,000(\text{Program Funds Requested})}$$

The monetary value of benefit due to increased safety of non-motorized users is estimated to be \$1,283,700 with a project lifespan of 20 years and taking into consideration projected reduction in pedestrian injury accidents and bicycle injury accidents. The monetary value of enhanced public health is conservatively estimated at \$578,808 assuming a 20 year lifespan of the facility and projecting only conservative student use increases. Increases in use by the community in general are not taken into consideration as no data is available. See Attachment 10, Cost/Benefit Analysis, for all assumptions and source information.

<ul style="list-style-type: none"> • Applicant considers alternatives and exceptionally justifies the project nominated - 5 points • Applicant considers alternatives and adequately justifies the project nominated - 3 to 4 points • Applicant considers alternatives and minimally justifies the project nominated - 1 to 2 points • Applicant did not consider alternatives or justify the project nominated - 0 points
<ul style="list-style-type: none"> • Applicant logically described how project benefits were quantified and has a benefit-cost ratio greater than 1 - 5 points • Applicant logically described how project benefits were quantified and has benefit-cost ratio less than 1- 3 points • Applicant did not logically describe how project benefits were quantified - 0 points

IV. NARRATIVE QUESTIONS- continued

5. IMPROVED PUBLIC HEALTH (0-10 points)

Project improvements are targeted to improve conditions for accessing school for children in kindergarten through 12th grade. Households of children attending the two Fort Bragg elementary schools and high school located along Chestnut Street are predominantly low-income, with school data showing that over 70% of families in each of the two elementary schools and over 60% of Fort Bragg High School families qualify for free or reduced price meals. Per the **North Coast Opportunities Community Action Plan 2014-15** (page 21), poverty and food insecurity are associated with increased obesity. According to the **Children Now 2012-2013 County Scorecard**, 16% of Mendocino County children are overweight for their age, compared with 11% statewide; and only 64% of Mendocino

provided by the Chestnut Street project will result in safe bicycling facilities where none currently exist; and will significantly improve walking facilities where very narrow sidewalks and numerous sidewalk barriers impede ability to safely walk. The engineering improvements combined with an effective, educational and motivational Safe Routes to School Program will encourage safe biking and walking, resulting in increased physical activity and improved health of Fort Bragg's children.

- Applicant exceptionally described how the project will improve public health and addresses high risk populations- 7 to 10 points
- Applicant adequately described how the project will improve public health and addresses high risk populations - 4 to 6 points
- Applicant minimally described how the project will improve public health - 1 to 3 points
- Applicant did not describe how the project will improve public health - 0 points

IV. NARRATIVE QUESTIONS- continued

6. **BENEFIT TO DISADVANTAGED COMMUNITIES** (0-10 points)

A. I. Is the project located in a disadvantaged community? **Yes**

II. Does the project significantly benefit a disadvantaged community? **Yes**

a. Which criteria does the project meet? (Answer all that apply)

- o Median household income for the community benefited by the project: **\$36,389**

According to the American Community Survey five year estimates, 2008-2012, the median household income for Fort Bragg, CA is \$36,389, which is 59% of California median income \$61,400. The guidelines indicate that a community qualifies as disadvantaged if the median household income is below 80% of the statewide median.

B. Describe how the project demonstrates a clear benefit to a disadvantaged community and what percentage of the project funding will benefit that community. For projects using the school based criteria, describe specifically how the school students and community will benefit.

Fort Bragg is an economically disadvantaged community. Per the City's 2009 Household Income Survey, nearly 60% of Fort Bragg families are considered low income with incomes at or below 80% of the State Median Household. However, Fort Bragg USD data from the same timeframe shows that for grades kindergarten through 8th grade, 70% of children were from families eligible for free and reduced price lunch. In addition, Census Tract 105, which encompasses most of the project area, has historically been home to the City's lowest income households, with incomes averaging more than 22% below Fort Bragg's median. The Fort Bragg area continues to experience economic challenges. Reduced incomes

City of Fort Bragg – Chestnut Street Multi-Use Facility and SRTS Program

in combination with high gas prices put increasing pressure on families to reduce unnecessary car trips. The school district expects that as a result, bus ridership for eligible children will increase in the foreseeable future, but due to school district financial pressures, bus routes are likely to be cut, and children living in close proximity to schools will be encouraged to walk or ride bikes to school. This trend, in combination with a concerted education, encouragement, and enforcement effort will result in more children walking and biking to school along safe school routes, including the Chestnut Street multi-use path.

- Project clearly and significantly addresses health, safety, and/or infrastructure challenges in the disadvantaged community- 5 points
- Project adequately addresses health, safety, and/or infrastructure challenges in the disadvantaged community - 3 points
- Project minimally addresses health, safety, and/or infrastructure challenges in the disadvantaged community - 1 points

- 80% to 100% of project funding benefits the disadvantaged community- 5 points
- 60% to 79% of project funding benefits the disadvantaged community- 4 points
- 40% to 59% of project funding benefits the disadvantaged community- 3 points
- 20% to 39% of project funding benefits the disadvantaged community- 2 points
- 1% to 19% of project funding benefits the disadvantaged community- 1 points
- 0% of project benefits the disadvantaged community- 0 points

IV. NARRATIVE QUESTIONS- continued

7. USE OF CALIFORNIA CONSERVATION CORPS (CCC) OR A CERTIFIED COMMUNITY CONSERVATION CORPS (0 to -5 points)

The applicant must send the following information to the CCC and CALCC prior to application submittal to Caltrans:

Project Description Project Map	Detailed Estimate Preliminary Plan	Project Schedule
------------------------------------	---------------------------------------	------------------

The corps agencies can be contacted at:
 California Conservation Corps at: www.ccc.ca.gov
 Community Conservation Corps at: <http://calocalcorps.org>

- A. The applicant has coordinated with the CCC to identify how a state conservation corps can be a partner of the project. Yes
- a. Name, e-mail, and phone # of the person contacted and the date the information was submitted to them: Virginia Clark, Virginia.clark@ccc.ca.gov, 916-341-3147; via email May 6, 2014.
- B. The applicant has coordinated with a representative from the California Association of Local Conservation Corps (CALCC) to identify how a certified community conservation corps can be a partner of the project. Yes

a. Name, e-mail, and phone # of the person contacted and the date the information was submitted to them: Cynthia Vitale, calocorps@gmail.com, 916-558-1516; via email May 6, 2014

C. The applicant intends to utilize the CCC or a certified community conservation corps on all items where participation is indicated? Yes

I have coordinated with a representative of the CCC; and the following are project items that they are qualified to partner on:

Removal of debris associated with sidewalk/curb/gutter demolition. Installation, monitoring, and maintenance of BMPs. School bike and walk safety assemblies, parent education at Back to School. (Attachment 11)

I have coordinated with a representative of the CALCC; and the following are project items that they are qualified to partner on:

The City requested coordination, however due to the location, the CALCC is unable to partner – See the attached email from CALCC (Attachment 11).

Points will be deducted if an applicant does not seek corps participation or if an applicant intends not to utilize a corps in a project in which the corps can participate*.

- The applicant intends to partner with a conservation corps to the maximum extent possible- 0 points
- The applicant did not seek partnership with a conservation corps, or indicated that they do not intend to partner with the corps to the maximum extent possible- (-)5 points

**If the applicant has indicated intended use of the CCC or CALCC in the approved application, a copy of the agreement between the implementing agency and the CCC or CALCC must be provided by the implementing agency, and will be incorporated as part of the original application, prior to request for authorization of funds for construction.*

IV. NARRATIVE QUESTIONS- continued

8. APPLICANT'S PERFORMANCE ON PAST GRANTS (0 to -10 points)

A. Describe any of your agency's ATP type grant failures during the past 5 years, and what changes your agency will take in order to deliver this project.

The City of Fort Bragg has no past ATP grant experience as this is a new program. The City has recently successfully completed construction associated with a Safe Routes to School Cycle 3 grant and did so within budget. Construction went well, and the non-infrastructure component is currently being implemented. The City has not had any Safe Routes to School or ATP Grant failures within the past five years.

- The applicant has no past grant experience or has performed satisfactorily on past grants - 0 points
- The applicant has not performed satisfactorily on past grants and/or has not adequately described how they will deliver this project (-)10 points

Project name: City of Fort Bragg - Chestnut Street Multi-Use Facility and SRTS Program

V. PROJECT PROGRAMMING REQUEST

Applicant must complete a Project Programming Request (PPR) and attach it as part of this application. The PPR and can be found at http://www.dot.ca.gov/hq/transprog/allocation/ppr_new_projects_9-12-13.xls

PPR Instructions can be found at <http://www.dot.ca.gov/hq/transprog/ocip/2012stip.htm>

Notes:

- Fund No. 1 must represent ATP funding being requested for program years 2014/2015 and 2015/2016 only.
- Non-infrastructure project funding must be identified as Con and indicated as "Non-infrastructure" in the Notes box of the Proposed Cost and Proposed Funding tables.
- Match funds must be identified as such in the Proposed Funding tables.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

General Instructions

<input checked="" type="checkbox"/> New Project						Date:	5/20/14
District	EA	Project ID		PPNO	MPO ID	TCRP No.	
01							
County	Route/Corridor	PM Bk	PM Ahd	Project Sponsor/Lead Agency			
MEN	Chestnut Street			City of Fort Bragg			
				MPO	Element		
				Non-MPO			
Project Manager/Contact		Phone		E-mail Address			
Tom Varga		707-961-1592		tvarga@fortbragg.com			
Project Title							
Chestnut Street Multi-Use Facility							
Location, Project Limits, Description, Scope of Work							<input type="checkbox"/> See page 2
The project is located in the City of Fort Bragg in Mendocino County, along the Chestnut Street corridor from its intersection with Franklin Street, east approximately 0.9 miles to the City Limits and end of paved road. The project consists of relocation of utilities and construction of an 8 to 12 foot wide multi-use facility on the north side of Chestnut Street in the vicinity of the existing sidewalk, relocation of on-street parking from the south to north side of the road, and associated traffic calming improvements, including bulb-outs, bump-outs, new stop signs, extended no-parking zone striping at corners, and narrower vehicle travel lanes.							
<input checked="" type="checkbox"/> Includes ADA Improvements				<input checked="" type="checkbox"/> Includes Bike/Ped Improvements			
Component	Implementing Agency						
PA&ED	City of Fort Bragg						
PS&E	City of Fort Bragg						
Right of Way	N/A						
Construction	City of Fort Bragg						
Purpose and Need							<input type="checkbox"/> See page 2
Chestnut Street is the primary east/west connection between State Route 1 and the educational hub of Fort Bragg High, Dana Gray Elementary, and Redwood Elementary Schools, recreational fields, and the community center. The AADT of the corridor exceeds 3,000 vehicles a day and is also heavily travelled by pedestrians and cyclists. The existing sidewalks do not meet ADA standards and are obstructed by utility and sign poles, fire hydrants, and other structures, and there are no bicycle facilities.							
Project Benefits							<input type="checkbox"/> See page 2
The project would provide ADA pedestrian facilities and bicycle facilities for the entire 0.9 mile stretch of roadway, would calm traffic, reducing the risk of accidents and improving safety for pedestrians and bicyclists. The project would also encourage walking and bicycling.							
<input type="checkbox"/> Supports Sustainable Communities Strategy (SCS) Goals				<input checked="" type="checkbox"/> Reduces Greenhouse Gas Emissions			
Project Milestone							Proposed
Project Study Report Approved							08/01/12
Begin Environmental (PA&ED) Phase							01/15/14
Circulate Draft Environmental Document					Document Type	CE/CE	07/15/14
Draft Project Report							
End Environmental Phase (PA&ED Milestone)							08/15/14
Begin Design (PS&E) Phase							09/01/14
End Design Phase (Ready to List for Advertisement Milestone)							02/15/15
Begin Right of Way Phase							N/A
End Right of Way Phase (Right of Way Certification Milestone)							N/A
Begin Construction Phase (Contract Award Milestone)							07/01/15
End Construction Phase (Construction Contract Acceptance Milestone)							11/01/15
Begin Closeout Phase							11/30/15
End Closeout Phase (Closeout Report)							12/31/15

ADA Notice For individuals with sensory disabilities, this document is available in alternate formats. For information call (916) 654-6410 or TDD (916) 654-3880 or write Records and Forms Management, 1120 N Street, MS-89, Sacramento, CA 95814.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

Date: 5/20/14

District	County	Route	EA	Project ID	PPNO	TCRP No.
01	MEN	Chestnut Street				
Project Title: City of Fort Bragg - Chestnut Street Multi-Use Facility and SRTS Program						

Proposed Total Project Cost (\$1,000s)									Notes
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	
E&P (PA&ED)	49,000							49,000	Non-Infrastructure SRTS funding is included in the "CON" section, including \$13000 allotted for FY 2014/15 and \$13000 allotted for FY 2015/16
PS&E		127,500						127,500	
R/W SUP (CT)									
CON SUP (CT)		25,000						25,000	
R/W									
CON		13,000	836,500					849,500	
TOTAL	49,000	165,500	836,500					1,051,000	

Fund No. 1:	ATP Grant								Program Code
Proposed Funding (\$1,000s)									20.30.720
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									Non-Infrastructure SRTS funding is included in the "CON" section, including \$13000 allotted for FY 2014/15 and \$13000 allotted for FY 2015/16
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON		13,000	246,000					259,000	
TOTAL		13,000	246,000					259,000	

Fund No. 2:	Demo6L-5088(028)								Program Code
Proposed Funding (\$1,000s)									1-1400-012
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									
PS&E		127,500						127,500	
R/W SUP (CT)									
CON SUP (CT)		25,000						25,000	
R/W									
CON			590,500					590,500	
TOTAL		152,500	590,500					743,000	

Fund No. 3:	MCOG Overall Work Program WE 21 FY 2011/12								Program Code
Proposed Funding (\$1,000s)									
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)	49,000							49,000	Funding for Chestnut Corridor Conceptual Plan and Street Right of Way Feasibility Study, which includes the preliminary plans and construction estimate
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL	49,000							49,000	

Project name: City of Fort Bragg - Chestnut Street Multi-Use Facility and SRTS Program

VI. ADDITIONAL INFORMATION

Only fill in those fields that are applicable to your project

FUNDING SUMMARY

ATP Funds being requested by Phase (to the nearest \$1000)	Amount
PE Phase (includes PA&ED and PS&E)	\$
Right-of-Way Phase	\$
Construction Phase-Infrastructure	\$ 233,000
Construction Phase-Non-infrastructure	\$ 26,000
Total for ALL Phases	\$ 259,000

All Non-ATP fund types on this project* (to the nearest \$1000)	Amount
DEMO 6L-5088(028)	\$ 743,000
MCOG Overall Work Program WE 21	\$ 49,000
	\$
	\$
	\$
	\$

*Must indicate which funds are matching

Total Project Cost	\$ 1,051,000
Project is Fully Funded	Yes

ATP Work Specific Funding Breakdown (to the nearest \$1000)	Amount
Request for funding a Plan	\$
Request for Safe Routes to Schools Infrastructure work	\$ 233,000
Request for Safe Routes to Schools Non-Infrastructure work	\$
Request for other Non-Infrastructure work (non-SRTS)	\$ 26,000
Request for Recreational Trails work	\$

ALLOCATION/AUTHORIZATION REQUESTS SCHEDULE

	Proposed Allocation Date	Proposed Authorization (E-76) Date
PA&ED or E&P		
PS&E		
Right-of-Way		
Construction	06/01/2015	06/01/2015

All project costs MUST be accounted for on this form, including elements of the overall project that will be, or have been funded by other sources.

Project name: Chestnut Street Multi-Use Facility and Safe Routes to School Program

VIII. APPLICATION SIGNATURES

Applicant: The undersigned affirms that the statements contained in the application package are true and complete to the best of their knowledge.

Signature: [Handwritten Signature]
Name: Rinda Ruffing
Title: City Manager

Date: 5-18-14
Phone: 707 961 2829
e-mail: L.Ruffing@fortbragg.com

Local Agency Official (City Engineer or Public Works Director): The undersigned affirms that the statements contained in the application package are true and complete to the best of their knowledge.

Signature: [Handwritten Signature]
Name: TOM Z. VARGA
Title: PUBLIC WORKS DIRECTOR

Date: 5/19/14
Phone: 207-961-2823
e-mail: tvarga@fortbragg.com

School Official: The undersigned affirms that the school(s) benefited by this application is not on a school closure list.

Signature: [Handwritten Signature]
Name: Don Armstrong
Title: Superintendent

Date: 5/19/14
Phone: (707) 961-2850 x3525
e-mail: darmstrong@sbusd.us

Person to contact for questions:

Name: _____
Title: _____
Phone: _____
e-mail: _____

Caltrans District Traffic Operations Office Approval*

If the application's project proposes improvements on a freeway or state highway that affects the safety or operations of the facility, it is required that the proposed improvements be reviewed by the district traffic operations office and either a letter of support or acknowledgement from the traffic operations office be attached () or the signature of the traffic personnel be secured below.

Signature: _____
Name: _____
Title: _____

Date: _____
Phone: _____
e-mail: _____

*Contact the District Local Assistance Engineer (DLAE) for the project to get Caltrans Traffic Ops contact information. DLAE contact information can be found at <http://www.dot.ca.gov/hq/LocalPrograms/dlae.htm>

Project name:

Chestnut Street Multi-Use Facility and Safe Routes to School Program

VIII. ADDITIONAL APPLICATION ATTACHMENTS

Check all attachments included with this application.

- Vicinity/Location Map- **REQUIRED for all IF Projects**
 - North Arrow
 - Label street names and highway route numbers
 - Scale

- Photos and/or Video of Existing Location- **REQUIRED for all IF Projects**
 - Minimum of one labeled color photo of the existing project location
 - Minimum photo size 3 x 5 inches
 - Optional video and/or time-lapse

- Preliminary Plans- **REQUIRED for Construction phase only**
 - Must include a north arrow
 - Label the scale of the drawing
 - Typical Cross sections where applicable with property or right-of-way lines
 - Label street names, highway route numbers and easements

- Detailed Engineer's Estimate- **REQUIRED for Construction phase only**
 - Estimate must be true and accurate. Applicant is responsible for verifying costs prior to submittal
 - Must show a breakdown of all bid items by unit and cost. Lump Sum may only be used per industry standards
 - Must identify all items that ATP will be funding
 - Contingency is limited to 10% of funds being requested
 - Evaluation required under the ATP guidelines is not a reimbursable item

- Documentation of the partnering maintenance agreement- Required with the application if an entity, other than the applicant, is going to assume responsibility for the operation and maintenance of the facility

- Documentation of the partnering implementation agreement-Required with the application if an entity, other than the applicant, is going to implement the project.

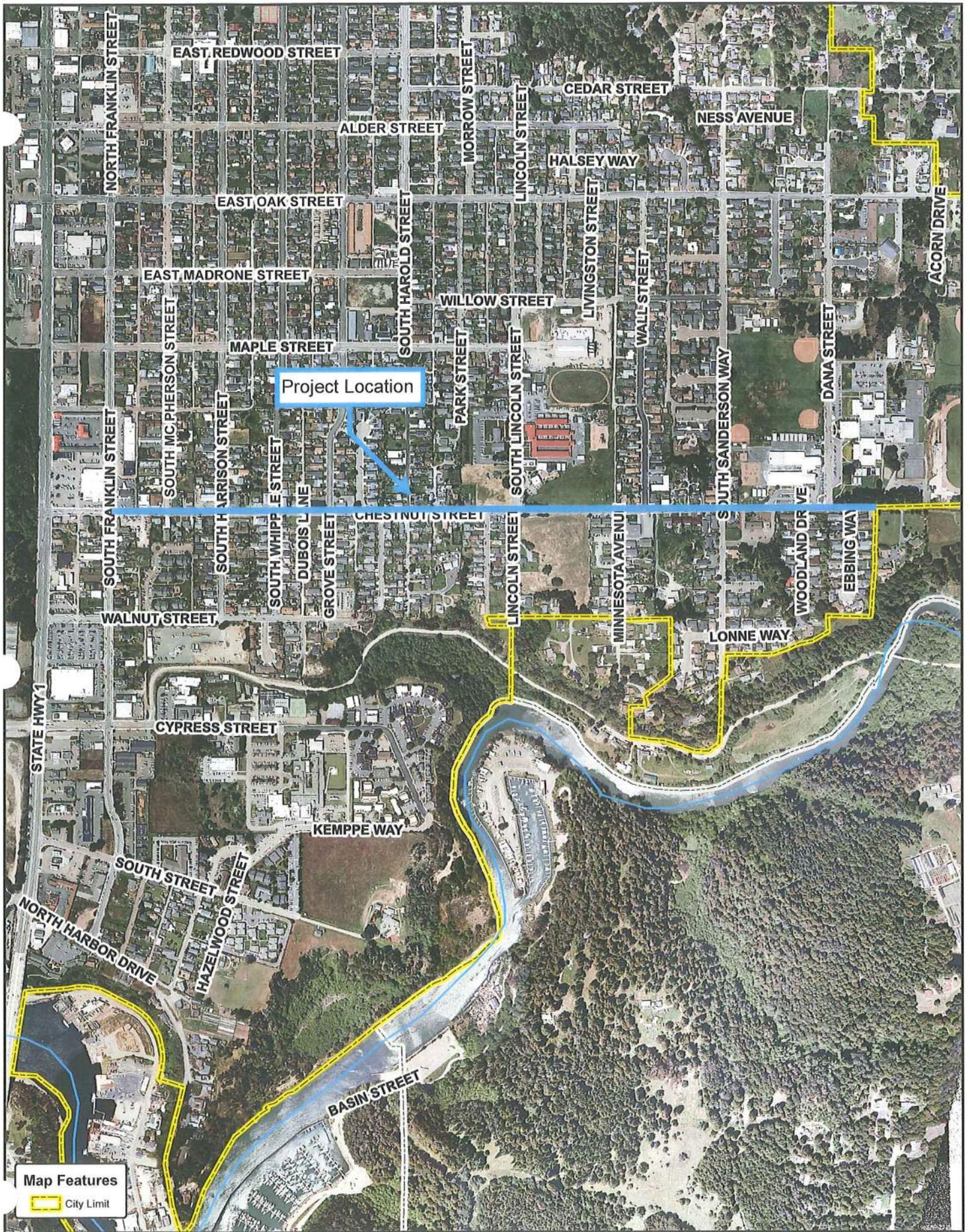
- Letters of Support from Caltrans (Required for projects on the State Highway System(SHS))

- Digital copy of or an online link to an approved plan (bicycle, pedestrian, safe routes to school, active transportation, general, recreation, trails, city/county or regional master plan(s), technical studies, and/or environmental studies (with environmental commitment record or list of mitigation measures), if applicable. Include/highlight portions that are applicable to the proposed project.

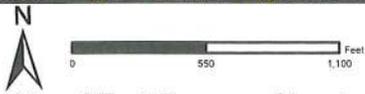
- Documentation of the public participation process (required)

- Letter of Support from impacted school- when the school isn't the applicant or partner on the application (required)

- Additional documentation, letters of support, etc (optional)

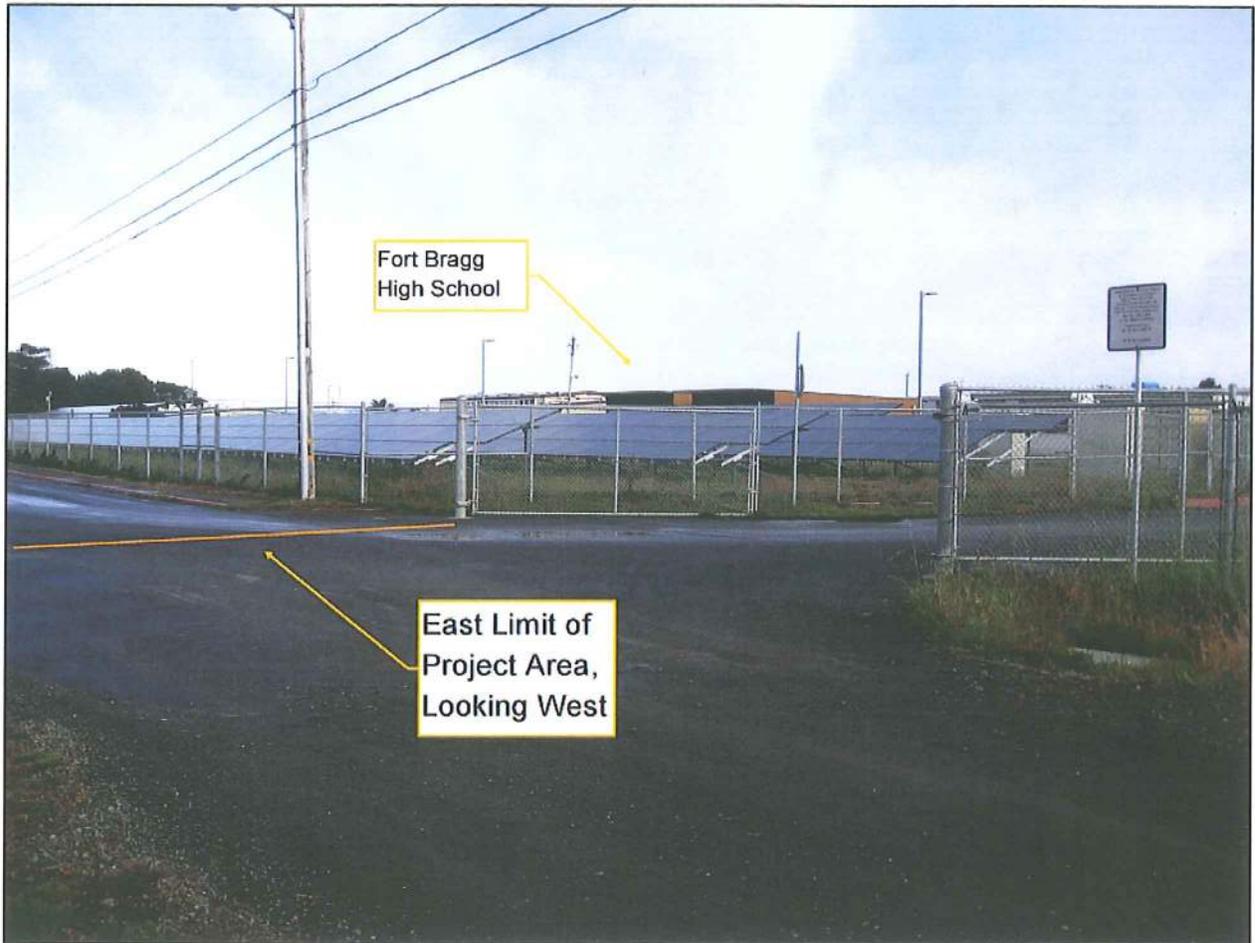


Feature and boundary locations depicted are approximate only.



City of Fort Bragg





Eastern edge of project at Chestnut Street just east of Ebbing Way



Northern sidewalk of Chestnut Street, facing west



Corner of Chestnut and Dana Streets showing placement of signage and utility poles



Intersection of Chestnut and Dana Streets



Crosswalk at Chestnut and Dana Streets



Chestnut Street sidewalk in front of Dana Gray Elementary, showing utility poles, utility box and signage in sidewalk



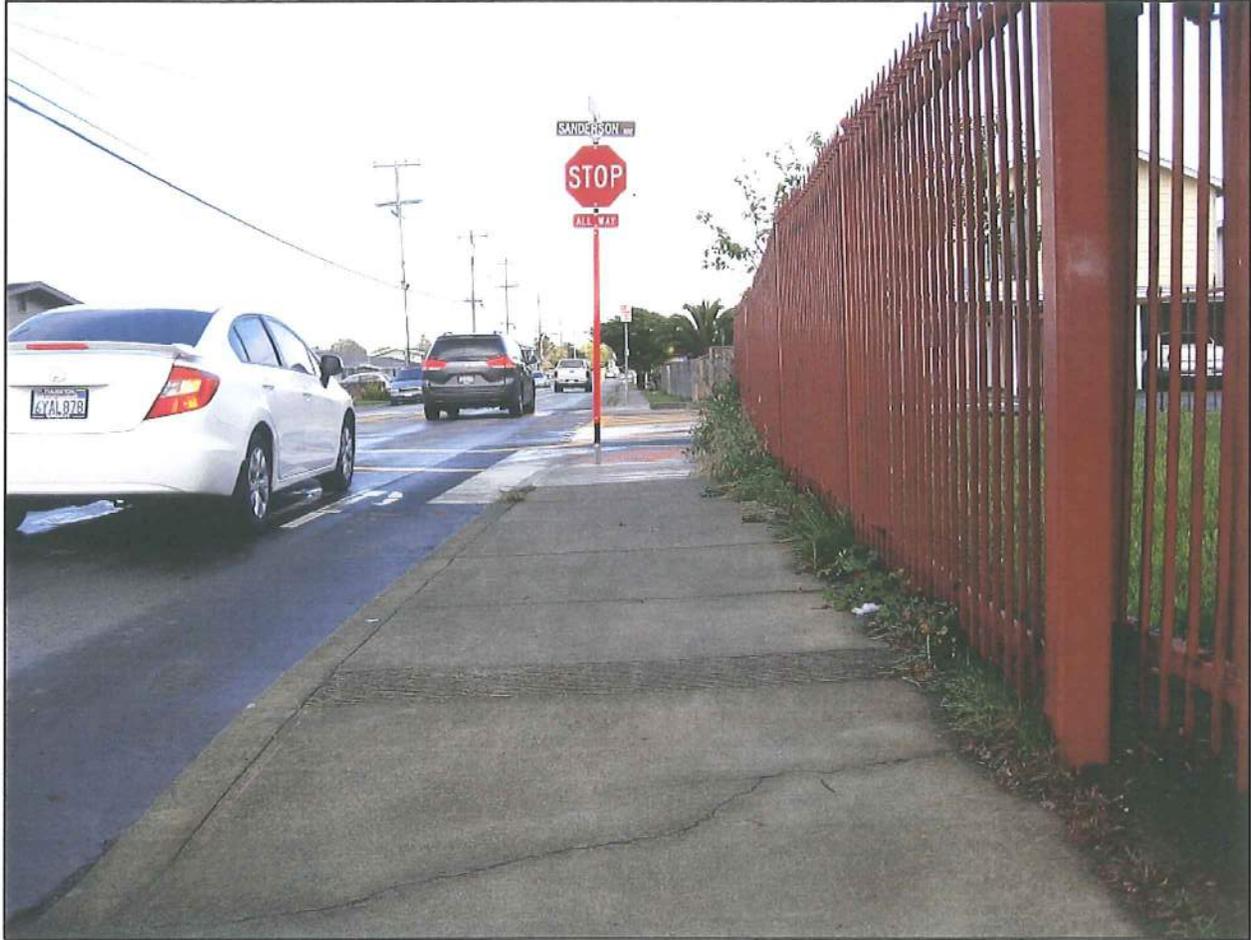
Dana Gray Elementary School exit driveway and crosswalk



Crosswalk near west entry to Dana Gray Elementary School



Project area heading west



Chestnut Street sidewalk approaching Sanderson Way



Crosswalk at Chestnut and Sanderson



Chestnut Street sidewalk looking west showing sign pole in walkway



Project area heading west
showing pole in center of walkway



Sidewalk heading west
showing placement of garbage receptacles



Chestnut Street approaching Wall Street



Crosswalk at intersection of Chestnut and Wall Streets



Approaching southeast corner of Redwood Elementary School



Proceeding west on Chestnut Street sidewalk past Redwood Elementary School



Sidewalk south of Redwood Elementary School



Intersection of Chestnut and South Lincoln Streets



Project area heading west



Uneven and cracked sidewalk



Narrow obstructed sidewalk
on Chestnut Street proceeding west



Cracked curb -- crosswalk at
Chestnut and Park Streets intersection



Intersection of Chestnut and Park Streets



Chestnut Street sidewalk heading west



Intersection of Chestnut and South Harold Streets



Chestnut Street sidewalk looking east, showing uneven surfaces at curb and drainage grate



Curb at western end of crosswalk at
Chestnut and South Harold Streets



Narrow walkway with sign pole obstruction



Project area heading west



Utility pole in center of sidewalk



Intersection of Chestnut Street and Susie Court



Sidewalk approaching South Corry Street



Curb at Chestnut and South Corry looking west



Raised curb at alley between South Corry and South Whipple Streets, looking east



Project area heading west, showing obstructions
in sidewalk



Approaching intersection of Chestnut Street
and South Whipple Street



Close-up of curb showing cracked, uneven and degraded condition



Northwest corner of Chestnut and South Whipple Streets



Project area heading west



Approaching intersection of Chestnut and South Harrison Streets



Curb at the northeast corner of Chestnut and
South Harrison Streets



Curb at the northwest corner of Chestnut and South Harrison Streets



Proceeding west along Chestnut Street sidewalk



Utility pole narrows traveled way of sidewalk



Intersection of Chestnut and South McPherson Streets
looking west



Uneven raised curb on sidewalk at corner of
Chestnut and South McPherson Streets



Close-up of curb



Chestnut Street approaching South Franklin Street



Western edge of project area, intersection of Chestnut and South Franklin Streets

IV. RECOMMENDED CHESTNUT STREET CORRIDOR IMPROVEMENTS

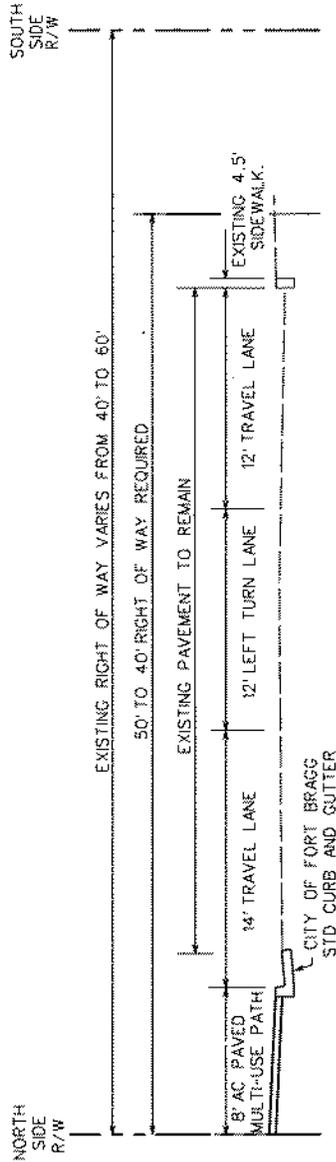
RECOMMENDED PROJECT

Chestnut Street Option A, with minor revisions, has been selected by the Fort Bragg City Council as the Recommended Project for the Chestnut Street Corridor. As previously described in Section III, this option features a multiuse path or a widened combination pedestrian / bicycle way on the north side of the street, on-street parking shifted from the south side to the north side of the street and limited improvements on the south side. Detailed cross sections for the Recommended Project are presented in **Figure IV-1A** and **IV-1B**. As shown in Figures IV-1A and IV-1B and as discussed below, refinements to Option A are included in the Recommended Project to respond to existing physical obstacles, right of way restrictions, cost reduction opportunities and existing land uses. Additional traffic calming features have also added to the Recommended Plan.

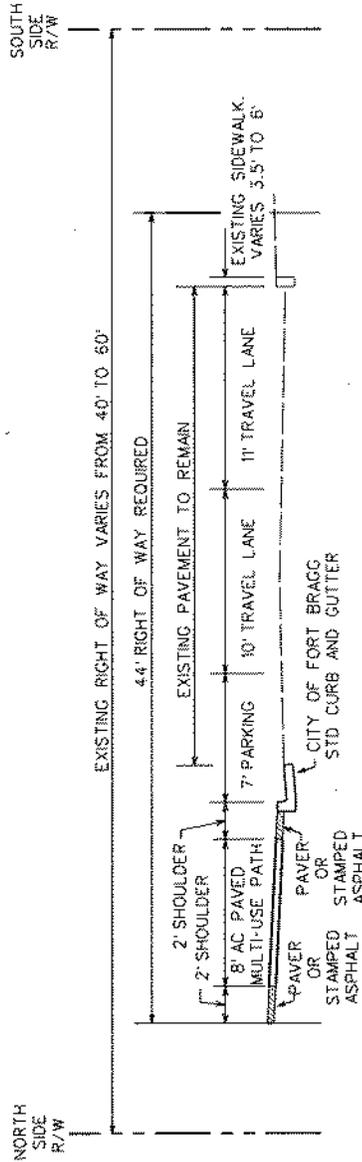
To improve the westbound Chestnut Street left turn movement to Franklin Street southbound, the left turn lane was lengthened. This modification requires removal of parking along the north side of Chestnut Street from Franklin Street to McPherson Street. To accommodate the widened street, the north side multiuse path was reduced to an eight foot width along the left turn lane. The multiuse path widens to the desired twelve foot section near the corner of Chestnut Street and McPherson Street.

Between Susie Court and Harold Street (APN 018-100-04) there is an existing structure (garage) constructed at the back of sidewalk. The existing sidewalk width at this location is three to 3 ½ feet. To widen the walkway to an eight foot wide multiuse path and preserve the existing garage, the new curb and gutter must be adjusted further into the street. A narrower street section with no on street parking is proposed at this location. A similar physical obstruction (garage) exists at Parcel 018-100-68 located between Harold Street and Lincoln Street. The existing structure is built at the back of a two to three foot walkway. To retain the existing garage the widening necessary to construct an eight foot multiuse path will be accomplished by narrowing the street section and eliminating parking adjacent to the garage.

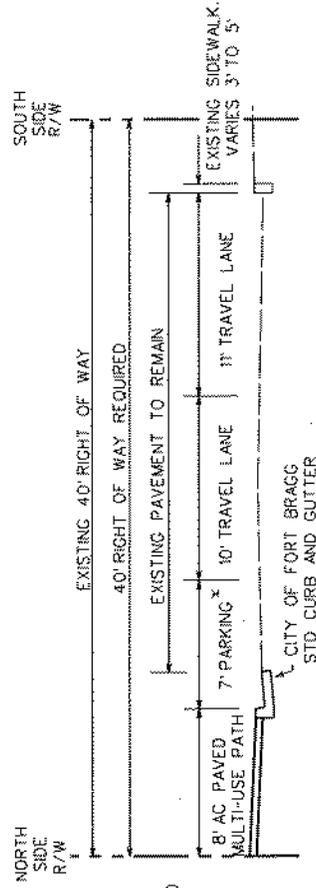
To avoid right of way acquisition and reduce costs, the multiuse path width from Sanderson Way to the end of the project has been reduced in width from twelve feet to eight feet. The Recommended Project provides an eight foot wide multiuse path section along the frontage of Parcel 008-332-12 east of Sanderson Way until the right of way and sidewalk widens at Dana Gray School. As shown in Figure IV-1B, east of this location the proposed multiuse path would transition to the existing eight to nine foot wide walkway which now exists along the frontage of Dana Gray School. East of Dana Street an eight foot wide multiuse path is proposed adjacent to the Fort Bragg High School parcel instead of the previously proposed twelve foot wide path. The existing 3 ½ foot wide sidewalk section would remain and widening to eight feet would occur at the existing back of walk.



FRANKLIN STREET TO McPHERSON STREET



MCPHERSON STREET TO WHIPPLE STREET



WHIPPLE STREET TO 80' EAST OF SANDERSON WAY

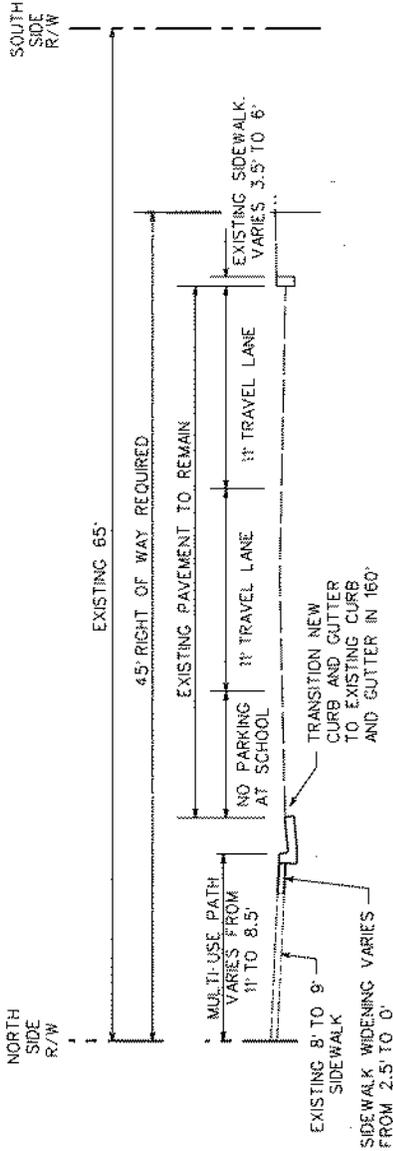
x NO PARKING WITHIN 10' OF CURBS ADJACENT TO RIGHTS OF WAY (PARCELS 018-100-04-00 AND 018-100-68-00)



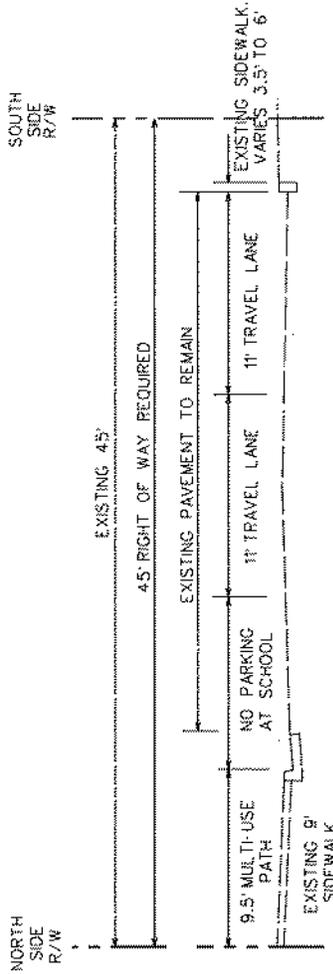
FILE: S:\21610_Chestnut_SRTS\Figures\KASL_Improvements.mxd
DATE: 10/26/2012

TYPICAL SECTIONS
RECOMMENDED CHESTNUT CORRIDOR IMPROVEMENTS
(FRANKLIN STREET TO 80' EAST OF SANDERSON WAY)

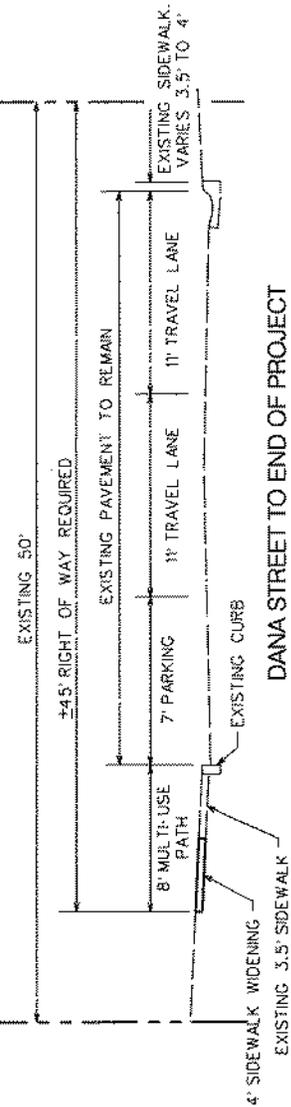
FIGURE IV-1A



80' EAST OF SANDERSON TO DANA GRAY SCHOOL DRIVEWAY



DANA GRAY SCHOOL DRIVEWAY TO DANA STREET



TYPICAL SECTIONS
 RECOMMENDED CHESTNUT CORRIDOR IMPROVEMENTS
 (80' EAST OF SANDERSON WAY TO END OF PROJECT)



FILE: 312730_01_Chestnut_SideWalk_Signage_Survey_Report.dwg - P. APPROVED.dwg
 DATE: 10/26/2012

In addition to the previously proposed electronic speed advisory sign and raised high visibility crosswalks (speed tables), bulbouts have been included at the McPherson Street and Whipple Street intersections. Small paved, traffic calming, "bump out" islands are added to better delineate the limits of the on-street parking and reduce the apparent width of the vehicle lanes. The bump outs would also serve as possible locations for relocated joint poles and signs.

PRELIMINARY IMPROVEMENT PLANS

Figures IV-2a through IV-2I are block by block scaled planning level exhibits that may be utilized by the City of Fort Bragg to develop engineering drawings for the future Chestnut Street improvements. These exhibits are presented at the end of Section IV. The recommended block by block Chestnut Street improvements and the traffic calming features proposed with the recommended Corridor Plan are summarized in the following;

Franklin Street to McPherson Street (Figure IV-2a)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Lengthen left turn stacking lane, westbound, at the Franklin Street intersection
- Construct eight foot to twelve foot wide multiuse path, north side
- Construct ADA compliant ramps at Franklin Street and McPherson Street
- Construct bulbout at the McPherson Street intersection
- Relocate joint pole to back of walk or top back of curb
- Conform driveways to new face of curb location
- Relocate drain inlet at Franklin Street and construct new drain lateral
- Relocate street signs
- Place crosswalk, centerline and turn lane striping and markings and red curb painting
- No on street parking on north or south sides of street

McPherson Street to Harrison Street (Figure IV-2b)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct twelve foot wide multiuse path, north side
- Construct ADA compliant ramps at McPherson Street and Harrison Street
- Construct bulbout at the McPherson Street intersection
- Construct bump out islands
- Relocate joint pole to back of walk, top back of curb or bump out island
- Conform driveway to new face of curb location
- Relocate drain inlet at Harrison Street and construct new drain lateral
- Relocate street signs
- Permit limited on-street parking, north side, as shown
- Place crosswalk and street markings and red curb painting
- Place centerline and right edge striping

Harrison Street to Whipple Street (Figure IV-2c)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct twelve foot wide multiuse path, north side
- Construct ADA compliant ramps at Harrison Street and Whipple Street
- Construct bulbout at Whipple Street intersection
- Construct bump out island
- Relocate joint poles to back of walk, top back of curb or bump out island
- Relocate street signs
- Relocate drain inlet at Harrison Street and construct new drain lateral
- Conform driveway to new face of curb location
- Permit on street parking, north side,
- Place crosswalk and street markings
- Place centerline and right edge striping and red curb painting

Whipple Street to Corry Street (Figure IV-2d)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Whipple Street, Corry Street, and the mid-block alley
- Construct bulbout at Whipple Street
- Construct bump out islands
- Relocate joint pole to back of walk or top back of curb
- Conform driveway to new face of curb location
- Relocate drain inlet at Whipple Street and construct new drain lateral
- Relocate street signs
- Permit on street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and red curb painting
- Place stop markings, stop limit and new stop sign east bound Chestnut Street at Corry Street

Corry Street to Harold Street (Figure IV-2e)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Corry Street, Susie Court and Harold Street
- Construct bump out islands
- Relocate joint pole to back of walk or to top back of curb
- Conform driveways to new face of curb locations
- Relocate drain inlets at Corry Street and Susie Court and construct new drain laterals
- Relocate street signs

- Permit on-street parking, north side, as shown. Prohibit parking adjacent to garage at Parcel 018-100-04
- Place crosswalk, centerline and right edge striping and red curb painting
- Place stop markings, stop limit and new stop sign west bound Chestnut Street at Corry Street

Harold Street to Mid Block Harold Street (Figure IV-2f)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Harold Street and at mid-block alley
- Construct bump out islands
- Conform driveways to new face of curb locations
- Relocate drain inlet at Harold Street and construct new drain lateral
- Relocate street sign
- Permit on-street parking, north side, as shown. Prohibit parking adjacent to garage at Parcel 018-100-68
- Place crosswalk, centerline and right edge striping and red curb painting

Mid-Block Harold Street to Lincoln Street (Figure IV-2g)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramp at Lincoln Street
- Construct bump out island
- Relocate joint pole to back of walk, top back of curb or bump out
- Conform driveway to new face of curb location
- Relocate drain inlet opposite Olsen Lane and construct new drain lateral
- Relocate street signs
- Permit on-street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and markings and red curb painting
- High visibility raised crosswalk, west leg of intersection at Lincoln to be constructed with the Cycle III Safe Routes to School (SRTS) Project

Lincoln Street to Mid-Block Lincoln Street (Figure IV-2h)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramp at Lincoln Street
- Construct bump out island
- Relocate drain inlet at Lincoln Street and construct new drain lateral
- Relocate street signs
- Permit on-street parking, north side, as shown

- Place crosswalk, centerline and right edge striping and markings and red curb painting

Mid-Block Lincoln Street to Wall Street (Figure IV-2i)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramp at Wall Street
- Construct bump out islands along
- Relocate joint poles to back of walk, top back of curb or bump out
- Conform driveways to new face of curb locations
- Relocate inlet opposite Minnesota Avenue and construct new drain lateral
- Relocate street signs
- Permit on-street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and markings and red curb painting
- Furnish and install electronic, solar powered, speed advisory sign for westbound movement
- Cross walk improvements shown at Minnesota Avenue were constructed with the 2011 SRTS Project

Wall Street to Sanderson Way (Figure IV-2j)

- Remove and replace existing curb and gutter
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Wall Street and Sanderson Way
- Construct bump out islands
- Conform driveways to new face of curb locations
- Relocate street signs
- Permit on street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and markings and red curb painting
- High visibility raised crosswalk, west leg of intersection at Sanderson Way to be constructed with the Cycle III SRTS Project

Sanderson Way to Woodland Drive (Figure IV-2k)

- Remove and replace existing curb and gutter, west of Dana Gray School to accommodate new street section
- Remove sidewalk along frontage of Parcel 008-332-12
- Construct eight foot wide multiuse path, north side, along frontage of Parcel 008-332-12
- Transition new eight foot wide multiuse path to existing sidewalk along frontage of Dana Gray School
- Existing sidewalk along frontage of Dana Gray School and existing curb and gutter east of school driveway to be retained
- Construct ADA compliant ramp at Sanderson Way
- Relocate joint poles to back of walk or top back of curb

- Conform driveway to new face of curb location
- Relocate drain inlet at Sanderson Way and construct new drain lateral
- Relocate street signs
- Install no parking signs
- Place crosswalk, centerline and right edge striping and markings and red curb painting

Woodland Drive to End of Project (Figure IV-2l)

- Existing curb and gutter and existing eight foot to nine foot wide sidewalk along frontage of Dana Gray School to be retained
- Widen existing sidewalk to eight feet wide east of Dana Street to end of project; existing sidewalk and curb to be retained
- Construct ADA compliant ramp at Dana Street
- Relocate joint poles to back of walk or top back of curb
- Parking allowed from Dana Street to the end of project, north side
- Place crosswalk, centerline and right edge striping and red curb painting
- Relocate chain link fence along Fort Bragg High School to new back of walk
- High visibility raised crosswalk, east leg of intersection at Dana Street to be constructed with the Cycle III SRTS Project

TRAFFIC CALMING FEATURES

A summary of the traffic calming features proposed for the Chestnut Corridor include:

- Electronic Speed Advisory Sign for westbound movement, west of Wall Street.
- High visibility raised crosswalks or speed tables at the Lincoln Street, Sanderson Way and Dana Street intersections. These improvements are scheduled to be constructed with the Cycle III Safe Routes to School project.
- New stop signs and stop ahead pavement markings at Corry Street intersection
- Bulbouts at McPherson Street and Whipple Street
- Bump out islands. These medians are proposed at various locations along the Chestnut Street Corridor to reduce the apparent width of the vehicle lanes and to provide protection for parked vehicles.
- Reduced vehicle lane widths throughout.

Chestnut Street Traffic Calming construction details itemized herein and shown in Figures IV-2a through IV-2l are presented in **Figure IV-3**. Figure IV-3 follows Figures IV-2a through IV-2l located at the end of this section.

ACCESSIBILITY AND SAFETY FEATURES

Improved pedestrian accessibility will be provided along the north side of Chestnut with the completion of a continuous, widened multiuse trail, the removal and relocation of obstacles, the construction of ADA compliant ramps, placement of improved high visibility crosswalk striping and parking restriction near each street intersection, driveway and alley crossing. Bulbouts at McPherson Street and Whipple Street and the high visibility raised crosswalks to be placed at Lincoln Street, Sanderson Way and Dana Street with the Safe Routes to School Cycle III Project will encourage slower traffic speeds within the Chestnut Street corridor.

INFILL IMPROVEMENTS

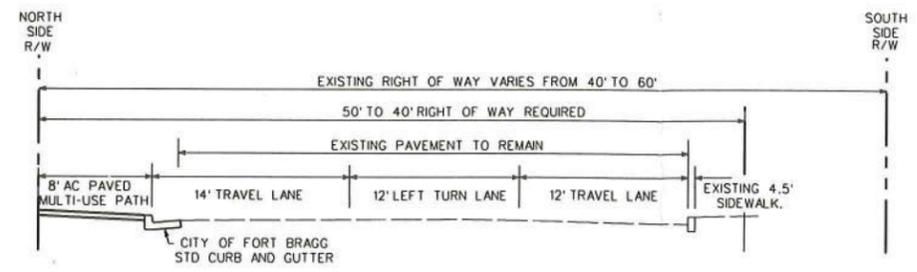
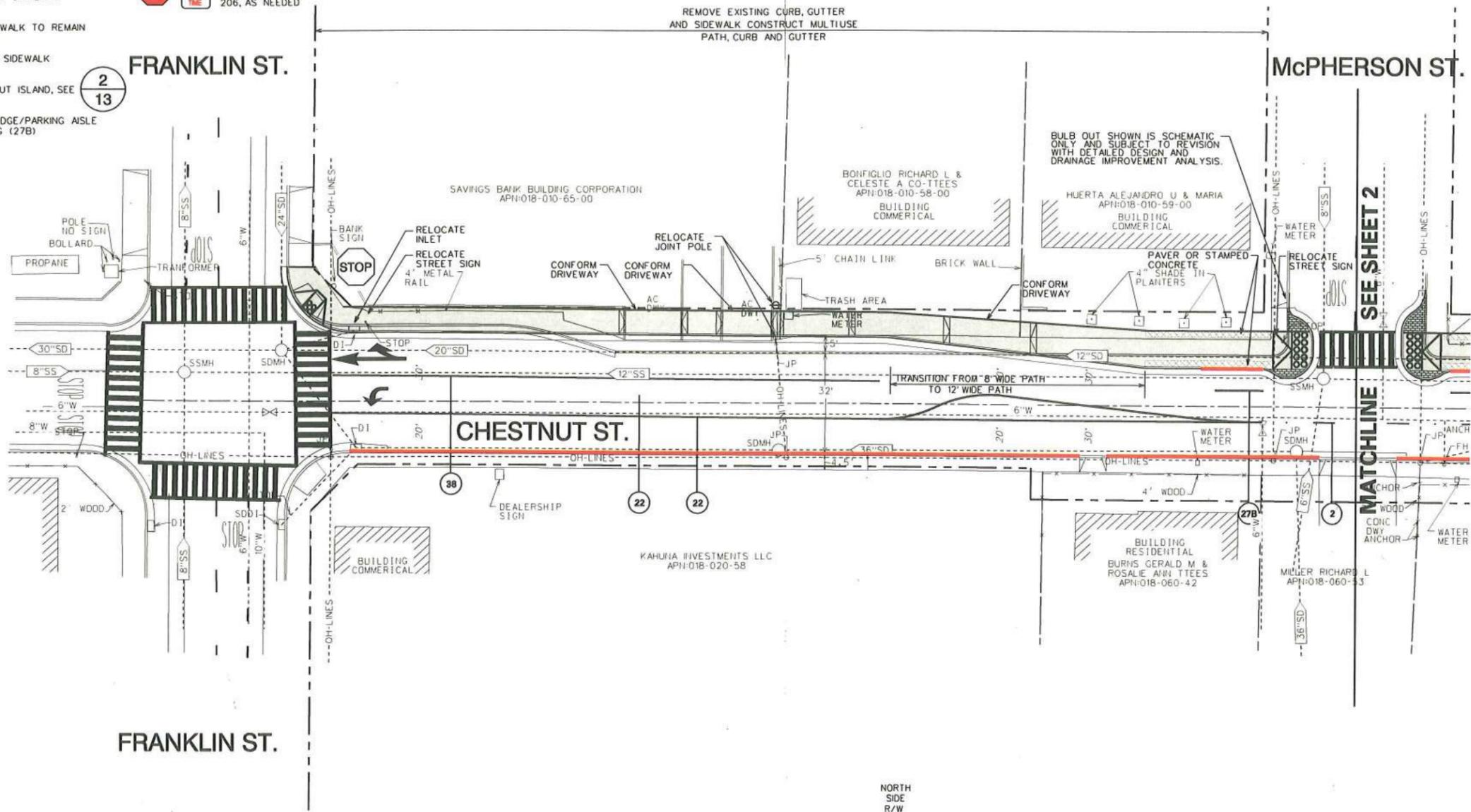
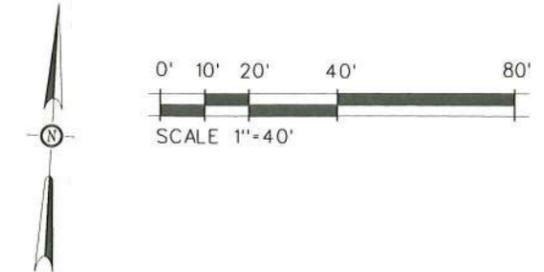
The future construction of City of Fort Bragg compliant curb, gutter and sidewalks are proposed along the south side of Chestnut Street to fill in the existing sidewalk "gaps" west of Spring Street (±150 feet) and east of Lincoln (±450 feet). These frontage improvements are to be constructed as a condition of the development of properties adjacent to the proposed sidewalks and are not included as part of the Chestnut Street Corridor improvements detailed in this Study.

COST ESTIMATES

Block by block quantity and cost estimates for the Recommended Chestnut Street Corridor Project improvements are presented in **Table IV-1**. In **Table IV-2** is summarized the costs estimated for the north and south sides of the street.

LEGEND

-  RIGHT OF WAY
-  MULTIUSE PATH W/ PAVERS OR STAMPED CONC.
-  PEDESTRIAN RAMPS
-  HIGH VISIBILITY STRIPED CROSSWALK
-  RAISED CROSSWALK TO BE CONSTRUCTED WITH CYCLE III SRTS PROJECT
-  CONFORM DRIVEWAY
-  EX SIDEWALK TO REMAIN
-  FUTURE SIDEWALK
-  BUMP OUT ISLAND, SEE 2/13
-  RIGHT EDGE/PARKING AISLE STRIPING (27B)
-  NO PARKING (RED PAINT ON FACE OF CURB)
-  RELOCATED JOINT POLE OR SERVICE POLE
-  NEW STREET SIGN
-  PAVEMENT STRIPING PER CALTRANS STANDARDS
-  PAVEMENT MARKINGS PER CALTRANS STANDARDS
-  EXISTING SIGN
-  PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



NO.	REVISIONS DESCRIPTION	DATE	BY

SCALE: 1"=40'
 JOB NO. 2719-10
 OCTOBER 2012



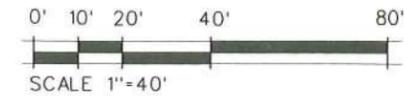
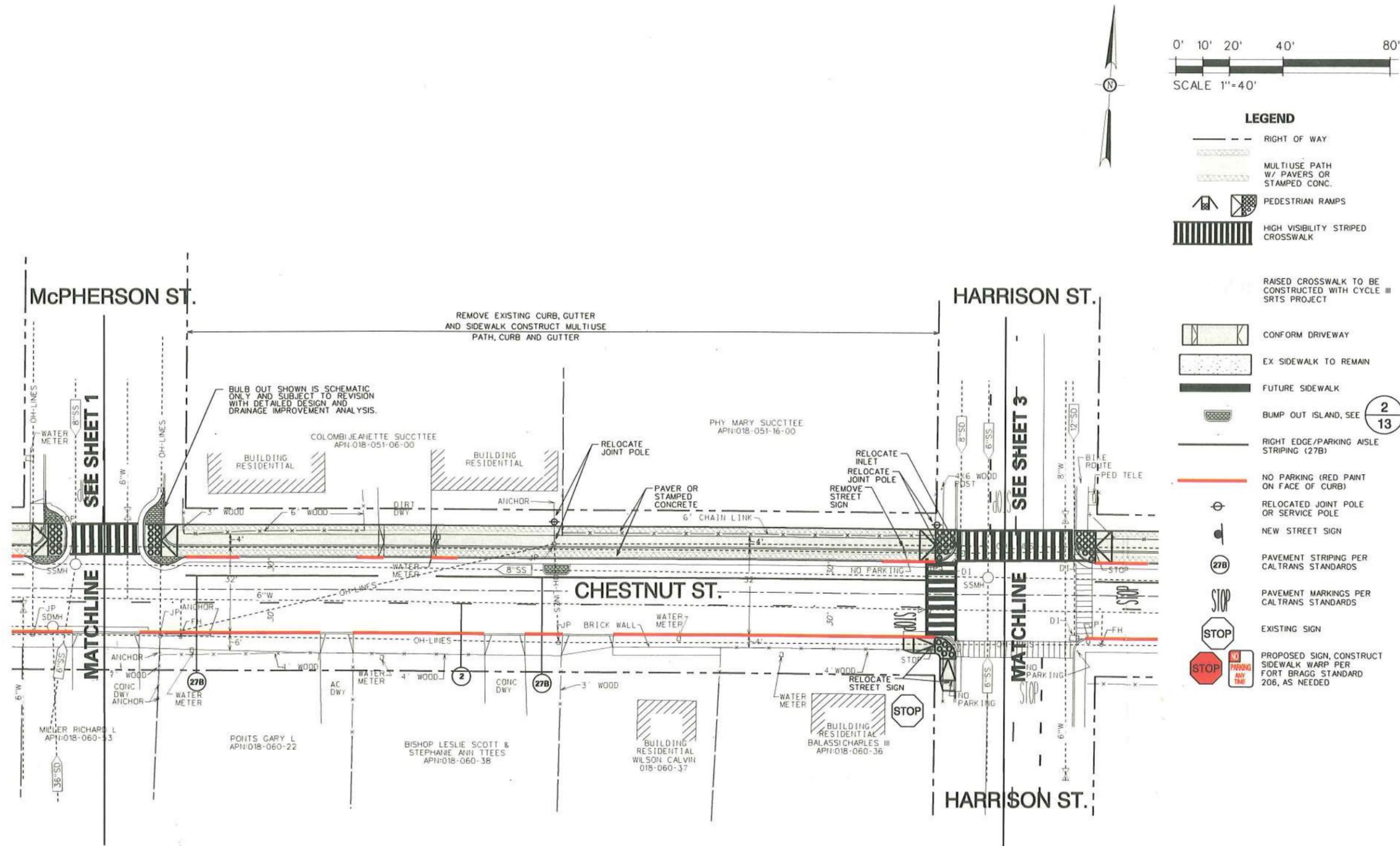
**CHESTNUT STREET IMPROVEMENTS
 RECOMMENDED PROJECT**
 CITY OF FORT BRAGG, CALIFORNIA
**CHESTNUT STREET
 FRANKLIN ST TO McPHERSON ST**

KASL
 7777 Overback Lane
 Suite 104
 Citrus Heights, CA 95621
 Tel: (916) 728-1800
 Fax: (916) 728-4888
 CIVIL - WATER RESOURCES - SURVEYING

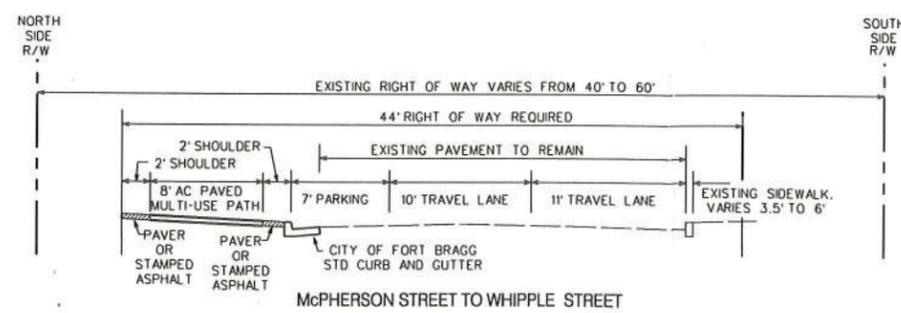
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FRANKLIN STREET TO McPHERSON STREET

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DATE: 11/12/2012



- LEGEND**
- RIGHT OF WAY
 - MULTIUSE PATH W/ PAVERS OR STAMPED CONC.
 - PEDESTRIAN RAMPS
 - HIGH VISIBILITY STRIPED CROSSWALK
 - RAISED CROSSWALK TO BE CONSTRUCTED WITH CYCLE III SRTS PROJECT
 - CONFORM DRIVEWAY
 - EX SIDEWALK TO REMAIN
 - FUTURE SIDEWALK
 - BUMP OUT ISLAND, SEE 2/13
 - RIGHT EDGE/PARKING AISLE STRIPING (27B)
 - NO PARKING (RED PAINT ON FACE OF CURB)
 - RELOCATED JOINT POLE OR SERVICE POLE
 - NEW STREET SIGN
 - PAVEMENT STRIPING PER CALTRANS STANDARDS
 - PAVEMENT MARKINGS PER CALTRANS STANDARDS
 - EXISTING SIGN
 - PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



NO.	DESCRIPTION	DATE	BY

REVISIONS

NO. DESCRIPTION DATE BY

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SCALE: 1"=40'

JOB NO. 2719-10

OCTOBER 2012



**CHESTNUT STREET IMPROVEMENTS
RECOMMENDED PROJECT**
CITY OF FORT BRAGG, CALIFORNIA

**CHESTNUT STREET
McPHERSON ST TO HARRISON ST**

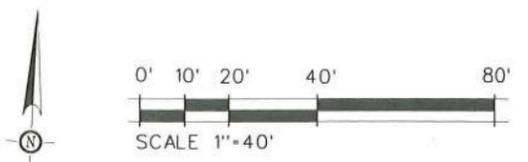
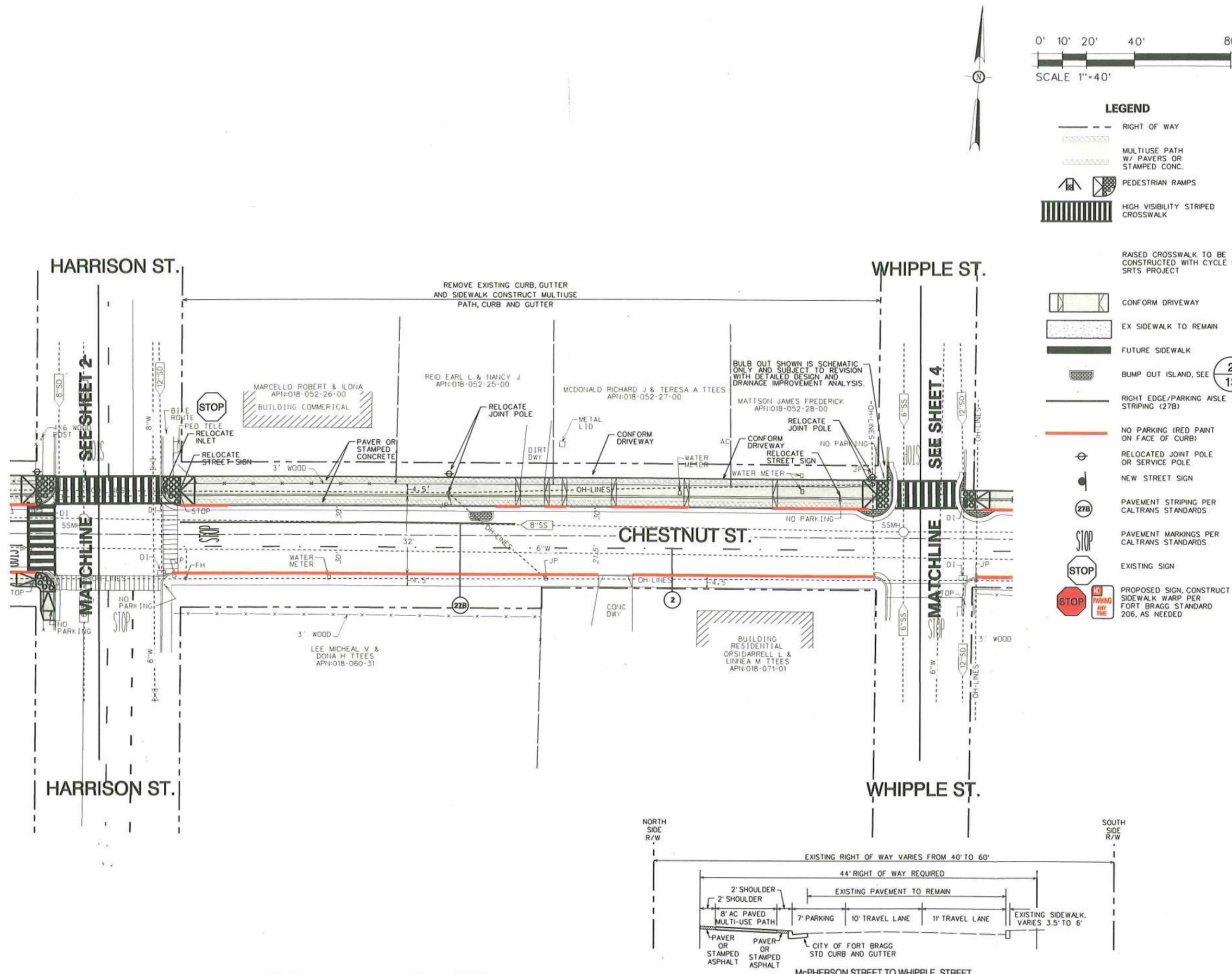
7777 Overback Lane
Suite 104
Olney Heights, CA 95010
Tel: (916) 752-1800
Fax: (916) 752-4888

KASL

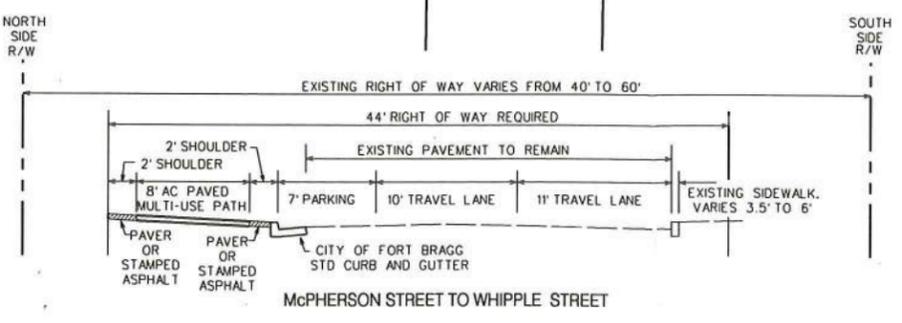
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SHEET **IV-2b**

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DATE: 11/12/2012



- LEGEND**
- RIGHT OF WAY
 - MULTIUSE PATH W/ PAVERS OR STAMPED CONC.
 - PEDESTRIAN RAMPS
 - HIGH VISIBILITY STRIPED CROSSWALK
 - RAISED CROSSWALK TO BE CONSTRUCTED WITH CYCLE III SRTS PROJECT
 - CONFORM DRIVEWAY
 - EX SIDEWALK TO REMAIN
 - FUTURE SIDEWALK
 - BUMP OUT ISLAND, SEE 2/13
 - RIGHT EDGE/PARKING AISLE STRIPING (27B)
 - NO PARKING (RED PAINT ON FACE OF CURB)
 - RELOCATED JOINT POLE OR SERVICE POLE
 - NEW STREET SIGN
 - PAVEMENT STRIPING PER CALTRANS STANDARDS
 - PAVEMENT MARKINGS PER CALTRANS STANDARDS
 - EXISTING SIGN
 - PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



NO.	REVISIONS DESCRIPTION	DATE	BY

SCALE: 1" = 40'
JOB NO. 2719-10
OCTOBER 2012

**CHESTNUT STREET IMPROVEMENTS
RECOMMENDED PROJECT**
CITY OF FORT BRAGG, CALIFORNIA

**CHESTNUT STREET
HARRISON ST TO WHIPPLE ST**

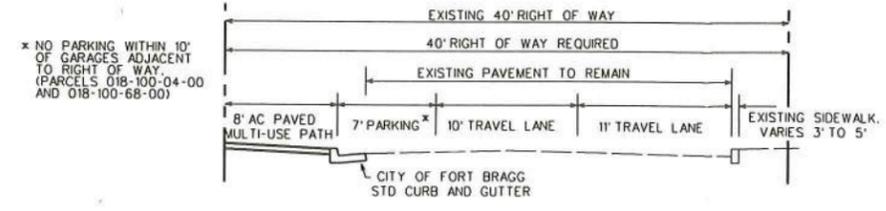
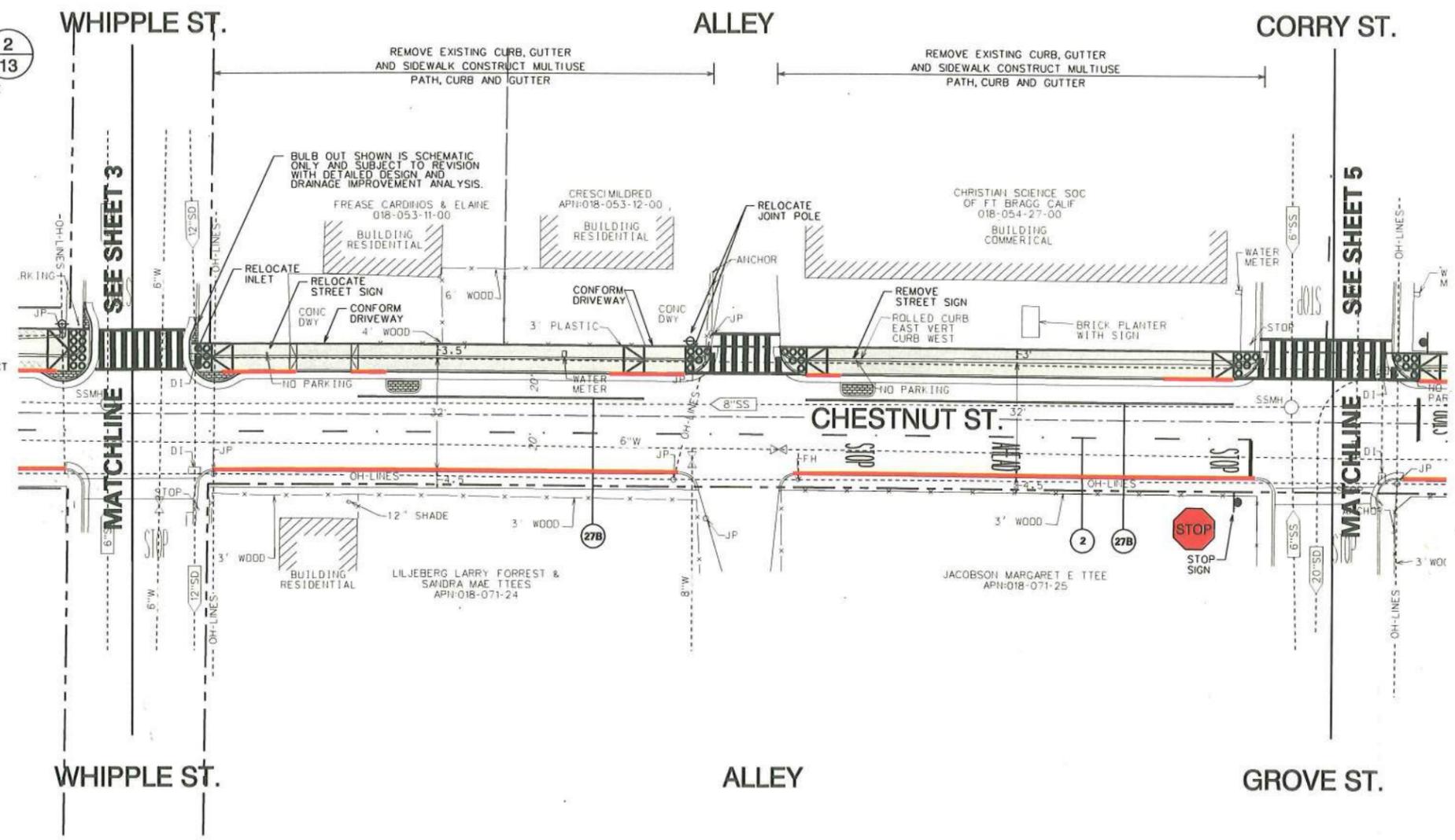
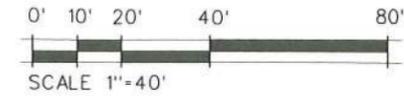
7777 Overback Lane
Suite 104
Orinda Hills, CA 94503
Tel: 916 752-1800
Fax: 916 752-4088

KASL
CIVIL - WATER RESOURCES - SURVEYING

SHEET **IV-2c**

LEGEND

- RIGHT OF WAY
- MULTIUSE PATH
- PEDESTRIAN RAMPS
- HIGH VISIBILITY STRIPED CROSSWALK
- RAISED CROSSWALK TO BE CONSTRUCTED WITH CYCLE III SRTS PROJECT
- CONFORM DRIVEWAY
- EX SIDEWALK TO REMAIN
- FUTURE SIDEWALK
- BUMP OUT ISLAND, SEE 2
13
- RIGHT EDGE/PARKING AISLE STRIPING (27B)
- NO PARKING (RED PAINT ON FACE OF CURB)
- RELOCATED JOINT POLE OR SERVICE POLE
- NEW STREET SIGN
- PAVEMENT STRIPING PER CALTRANS STANDARDS
- PAVEMENT MARKINGS PER CALTRANS STANDARDS
- EXISTING SIGN
- PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



WHIPPLE STREET TO 80' EAST OF SANDERSON WAY

NO.	REVISIONS DESCRIPTION	DATE	BY

OCTOBER 2012
 SCALE: 1"=40'
 JOB NO. 2719-10



**CHESTNUT STREET IMPROVEMENTS
 RECOMMENDED PROJECT**
 CITY OF FORT BRAGG, CALIFORNIA
**CHESTNUT STREET
 WHIPPLE ST TO GROVE ST**

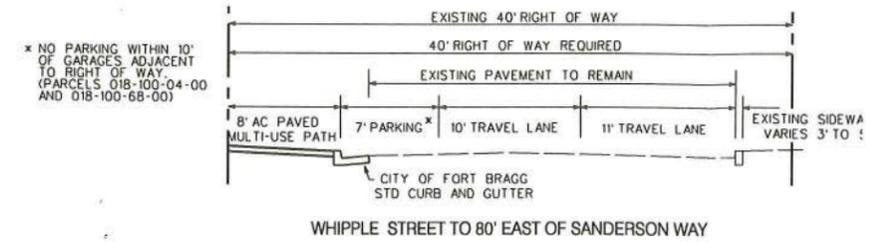
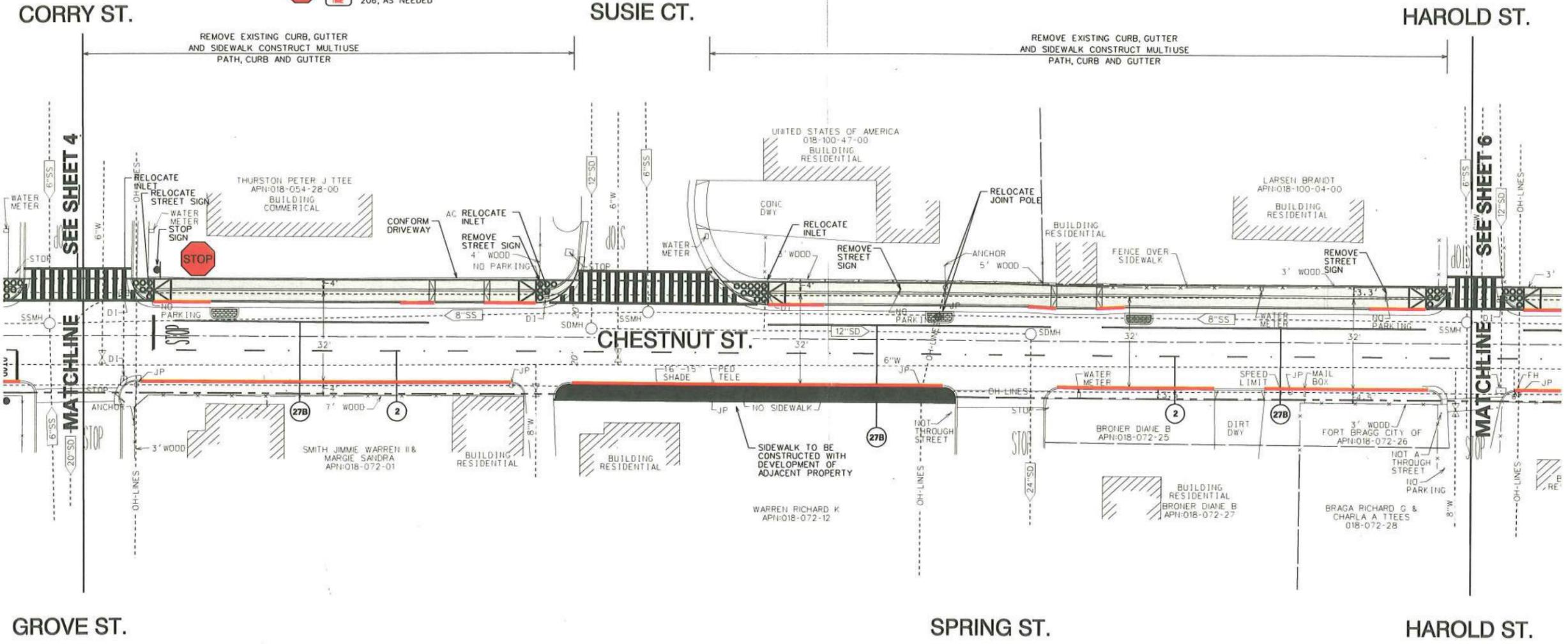
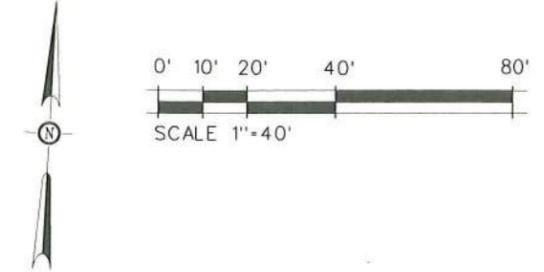
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 Fax: (916) 728-4088
 CIVIL - WATER RESOURCES - SURVEYING

McPHERSON ST TO HARRISON ST
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 DATE: 11/12/2012
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FILE: S:\2719-10 Chestnut St\PLANS\PLAN SHEETS\OPTION A FINAL\1A-05.dgn
DATE: 11/12/2012

LEGEND

- RIGHT OF WAY
- MULTIUSE PATH
- PEDESTRIAN RAMPS
- HIGH VISIBILITY STRIPED CROSSWALK
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- PAVEMENT MARKINGS PER CALTRANS STANDARDS
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- PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



NO.	REVISIONS DESCRIPTION	DATE	BY

OCTOBER 2012
SCALE: 1"=40'
JOB NO. 2719-10

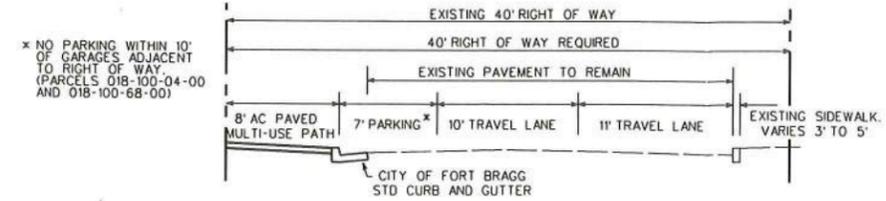
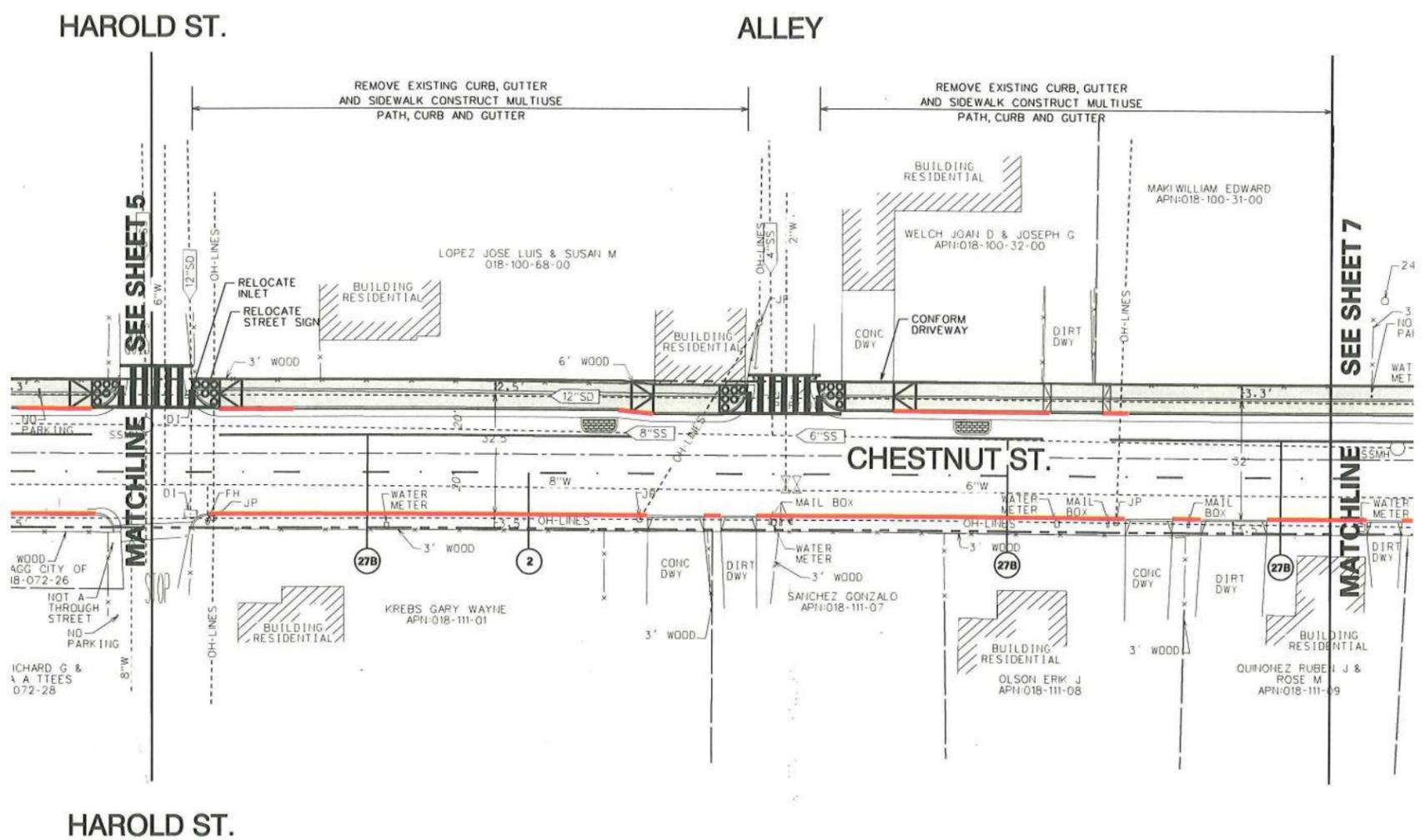
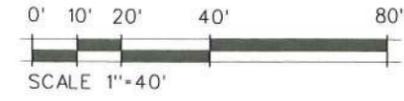


**CHESTNUT STREET IMPROVEMENTS
RECOMMENDED PROJECT**
CITY OF FORT BRAGG, CALIFORNIA
**CHESTNUT STREET
GROVE ST TO HAROLD ST**

KASL
7777 Overback Lane
Suite 104
Crown Heights, CA 95010
Tel: (916) 752-1800
Fax: (916) 752-4888
CIVIL - WATER RESOURCES - SURVEYING

LEGEND

- RIGHT OF WAY
- MULTIUSE PATH
- PEDESTRIAN RAMPS
- HIGH VISIBILITY STRIPED CROSSWALK
- RAISED CROSSWALK TO BE CONSTRUCTED WITH CYCLE III SRTS PROJECT
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- PAVEMENT MARKINGS PER CALTRANS STANDARDS
- EXISTING SIGN
- PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



* NO PARKING WITHIN 10' OF GARAGES ADJACENT TO RIGHT OF WAY. (PARCELS 018-100-04-00 AND 018-100-68-00)

NO.	REVISIONS DESCRIPTION	DATE	BY

OCTOBER 2012
SCALE: 1"=40'
JOB NO. 2719-10



**CHESTNUT STREET IMPROVEMENTS
RECOMMENDED PROJECT**
CITY OF FORT BRAGG, CALIFORNIA
**CHESTNUT STREET
HAROLD ST TO MID BLOCK**

KASL
7777 Overback Lane
Suite 104
Cruz Heights, CA 95010
Tel: (916) 758-1800
Fax: (916) 758-4888
CIVIL - WATER RESOURCES - SURVEYING

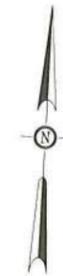
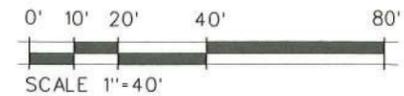
SHEET **IV-2f**

FILE: S:\2719-10 Chestnut St\PLANS\PLAN SHEETS\OPTION A FINAL\1A-06.dgn
DATE: 11/12/2012

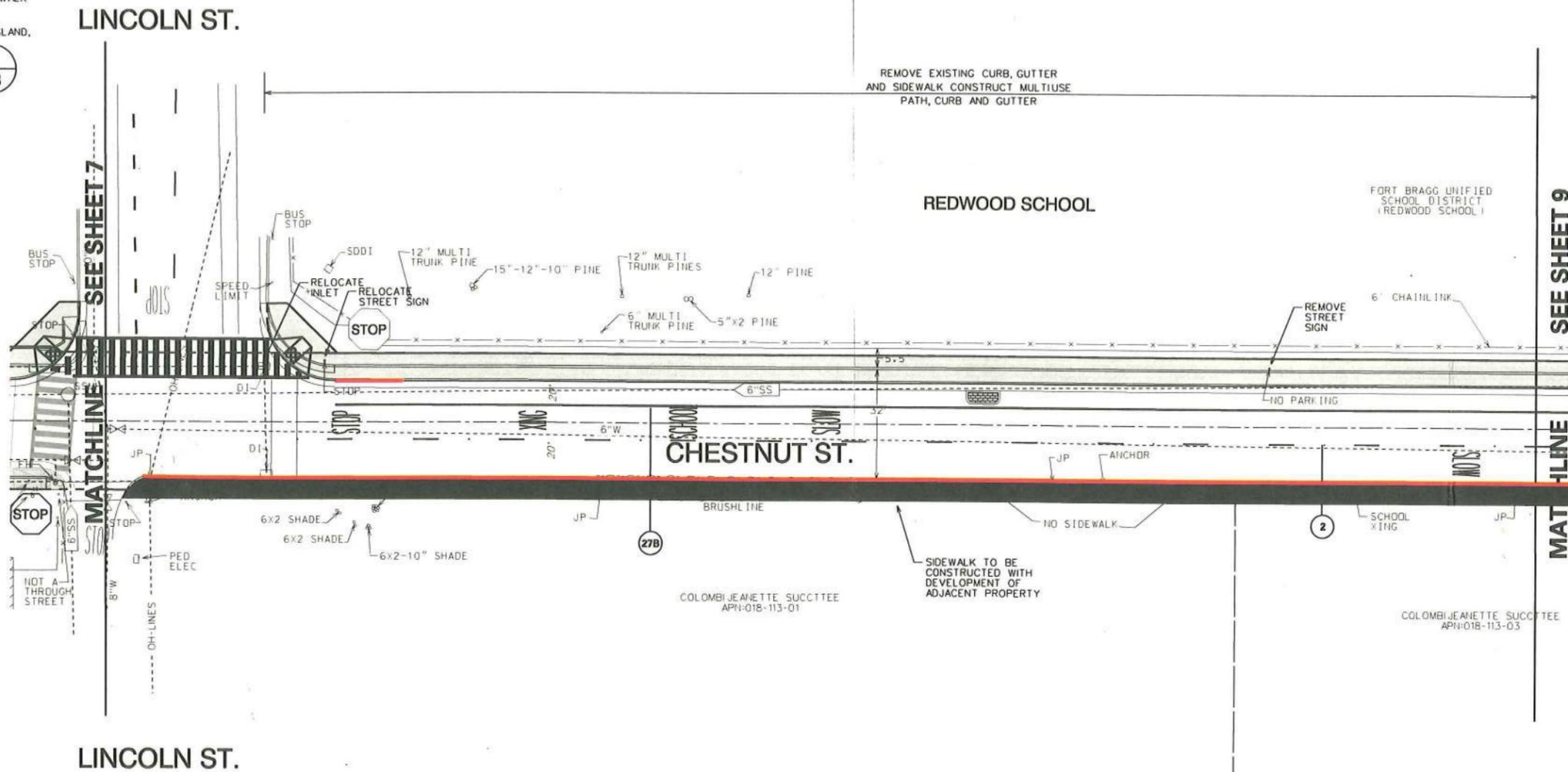
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DATE: 11/12/2012

LEGEND

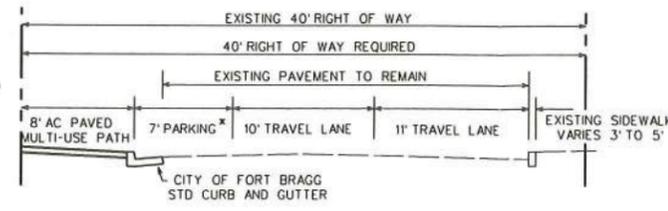
- RIGHT OF WAY
- MULTIUSE PATH
- PEDESTRIAN RAMPS
- HIGH VISIBILITY STRIPED CROSSWALK
- RAISED CROSSWALK TO BE CONSTRUCTED WITH CYCLE III SRTS PROJECT
- CONFORM DRIVEWAY
- EX SIDEWALK TO REMAIN
- FUTURE SIDEWALK
- BUMP OUT ISLAND.
- RIGHT EDGE/PARKING AISLE STRIPING (27B)
- NO PARKING (RED PAINT ON FACE OF CURB)
- RELOCATED JOINT POLE OR SERVICE POLE
- NEW STREET SIGN
- PAVEMENT STRIPING PER CALTRANS STANDARDS
- PAVEMENT MARKINGS PER CALTRANS STANDARDS
- EXISTING SIGN
- PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



SEE 2
13



* NO PARKING WITHIN 10' OF GARAGES ADJACENT TO RIGHT OF WAY. (PARCELS 018-100-04-00 AND 018-100-68-00)



WHIPPLE STREET TO 80' EAST OF SANDERSON WAY

NO.	REVISIONS DESCRIPTION	DATE	BY

OCTOBER 2012
SCALE: 1"=40'
JOB NO. 2719-10



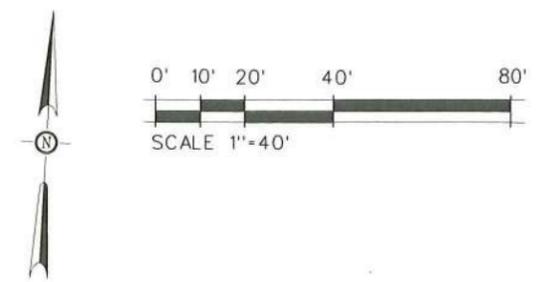
**CHESTNUT STREET IMPROVEMENTS
RECOMMENDED PROJECT**
CITY OF FORT BRAGG, CALIFORNIA
**CHESTNUT STREET
LINCOLN ST TO MID BLOCK**

KASL
7777 Overback Lane
Suite 104
Crestview, CA 95531
Tel: (916) 782-1800
Fax: (916) 782-4888
CIVIL - WATER RESOURCES - SURVEYING

SHEET **IV-2h**

LEGEND

- RIGHT OF WAY
- MULTIUSE PATH
- PEDESTRIAN RAMPS
- HIGH VISIBILITY STRIPED CROSSWALK
- RAISED CROSSWALK TO BE CONSTRUCTED WITH CYCLE STRIPS PROJECT
- CONFORM DRIVEWAY
- EX SIDEWALK TO REMAIN
- FUTURE SIDEWALK
- BUMP OUT ISLAND, SEE 2/13
- RIGHT EDGE/PARKING AISLE STRIPING (27B)
- NO PARKING (RED PAINT ON FACE OF CURB)
- RELOCATED JOINT POLE OR SERVICE POLE
- NEW STREET SIGN
- PAVEMENT STRIPING PER CALTRANS STANDARDS
- PAVEMENT MARKINGS PER CALTRANS STANDARDS
- EXISTING SIGN
- PROPOSED SIGN, CONSTRUCT SIDEWALK WARP PER FORT BRAGG STANDARD 206, AS NEEDED



NO.	REVISIONS DESCRIPTION	DATE	BY

SCALE: 1"=40'	JOB NO. 2719-10
OCTOBER 2012	

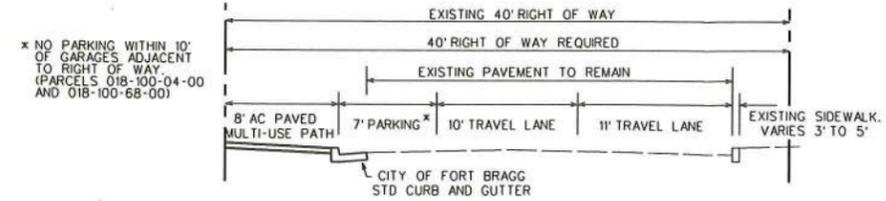
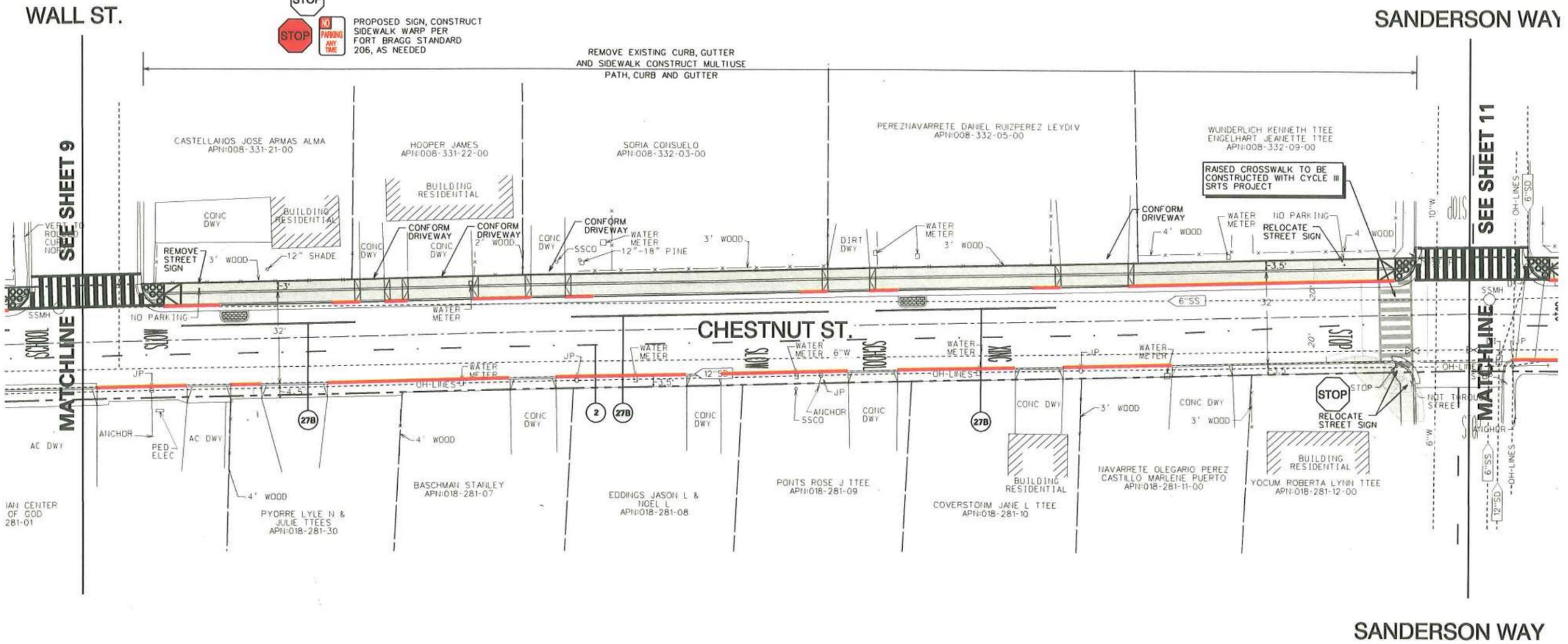


**CHESTNUT STREET IMPROVEMENTS
RECOMMENDED PROJECT**
CITY OF FORT BRAGG, CALIFORNIA

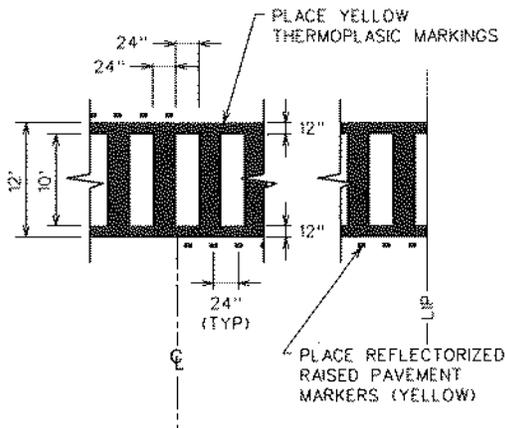
**CHESTNUT STREET
MID BLOCK TO SANDERSON WAY**

KASL
7777 Overlook Lane
Suite 104
Orion Heights, CA 94553
Tel: (916) 723-1800
Fax: (916) 723-4008

CIVIL - WATER RESOURCES - SURVEYING

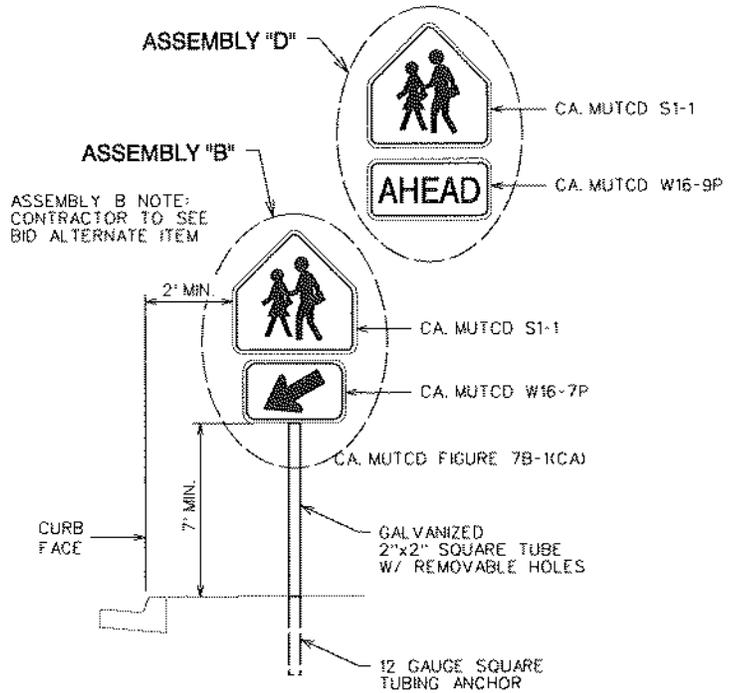


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DATE: 11/12/2012



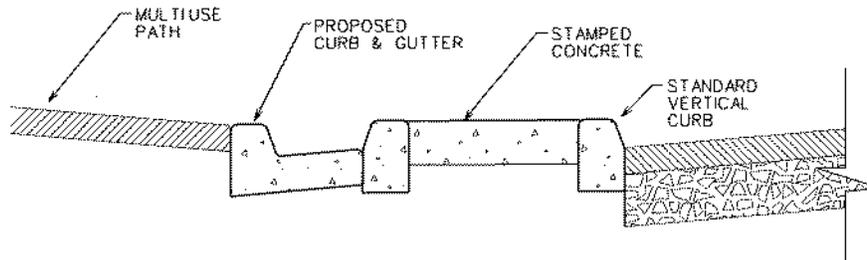
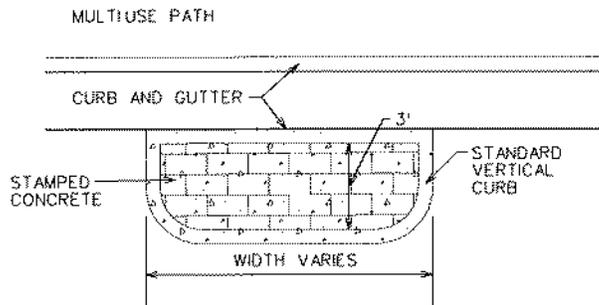
CROSSWALK STRIPING

NO SCALE



SCHOOL AREA SIGNS

NO SCALE



BUMP OUT ISLAND

NO SCALE



FILE: 150278-01_Chestnut_SRTS_Feasibility_Report\Figures\IV-3_Drawing.dwg
DATE: 10/27/2012

TRAFFIC CALMING DETAILS

FIGURE IV-3



**TABLE IV-1
Chestnut Street Corridor
Recommended Project**

Block	Bike Path (1)		New Curb & Gutter		Traffic Calming Features		Driveway Conform		New or Relocate Street Sign		Relocate Inlet (2)		Relocate Hydrant		Right of Way Acquisition		Relocate Joint Pole		Relocate Service Pole		Estimated Cost By Block		
	SF	LF	SF	LF	SF	EA	EA	EA	EA	EA	EA	EA	EA	EA	SF	EA	EA	EA	EA	EA	EA	EA	EA
Franklin to McPherson	2,925	365	50	3	3	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$46,250
McPherson to Harrison	3,680	370	110	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$49,250
Harrison to Whipple	3,645	330	70	2	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$52,675
Whipple to Grove	2,390	380	110	2	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$42,850
Grove to Harold	3,355	525	120	1	1	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$70,600
Harold to Lincoln	5,115	735	120	2	2	4	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$80,150
Lincoln to Minnesota	4,890	655	80	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$66,625
Minnesota to Wall	1,225	185	40	1	1	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$24,150
Wall to Sanderson	3,680	520	80	4	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$42,250
Sanderson to Woodland	480	240	0	1	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$141,900
Woodland to Dana	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Dana to End	1,410	25	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$88,175
Subtotal	33,005	4,350	780	16	26	12	12	0	0	0	0	0	0	0	400	5	10	0	0	0	0	0	\$704,575

25% Contingency \$176,144

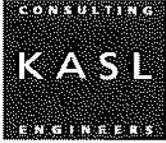
Estimated Total \$880,719

Block	Sidewalk (1)		New Curb & Gutter		Traffic Calming Features		Driveway Conform		New or Relocate Street Sign		Relocate Inlet (2)		Relocate Hydrant		Right of Way Acquisition		Relocate Joint Pole		Relocate Service Pole		Estimated Cost By Block		
	SF	LF	SF	LF	SF	EA	EA	EA	EA	EA	EA	EA	EA	EA	SF	EA	EA	EA	EA	EA	EA	EA	
Franklin to McPherson	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
McPherson to Harrison	170	36	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$2,750
Harrison to Whipple	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Whipple to Grove	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$500
Grove to Harold	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Harold to Lincoln	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Lincoln to Minnesota	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Minnesota to Wall	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$1,000
Wall to Sanderson	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Sanderson to Woodland	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Woodland to Dana	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Dana to End	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Subtotal	170	36	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$4,250

25% Contingency \$1,063

Estimated Total \$5,313

(1) Includes ADA compliant lamps.
(2) Includes drainage lateral and connection to manhole.



**TABLE IV-2
Chestnut Street Corridor**

Summary of Estimated Costs

RECOMMENDED PROJECT	North Side	South Side	Estimated Total
Surface Improvements (1)	\$326,575	\$4,250	\$330,825
Water and Storm Drainage Utilities	\$120,000	\$0	\$120,000
Right of Way Acquisition	\$8,000	\$0	\$8,000
Joint and Service Pole Relocation (2)	<u>\$250,000</u>	<u>\$0</u>	<u>\$250,000</u>
SUBTOTAL	\$704,575	\$4,250	\$708,825
25% Contingency	<u>\$176,144</u>	<u>\$1,063</u>	<u>\$177,206</u>
Estimated Total	<u>\$880,719</u>	<u>\$5,313</u>	<u>\$886,031</u>

(1) Includes bike paths, multi use paths, sidewalks, curbs and gutters, traffic calming features

(2) Portion of costs may be paid by PG&E.

May 8, 2014

Ms. Teresa Spade
Community Development Department
City of Fort Bragg
416 N. Franklin Street
Fort Bragg, CA 95437



Subject: Estimated Design Costs for Chestnut Street Multi-Use Path Improvements

7777 Greenback Lane
Suite 104
Citrus Heights, CA
95610

Tel. 916/ 722-1800
Fax 916/ 722-4595

Principal:
John C. Scroggs

Teresa:

In response to your request the following are land survey and engineering design costs estimated for the Chestnut Street Corridor Improvements. The proposed project includes approximately 4800 lineal feet of multi-use path improvements beginning at Franklin Street on the west and extending to the easterly end of Chestnut Street at the Fort Bragg City limits. Consistent with the recommendations included in the 2013 Chestnut Street Corridor Conceptual Plan, engineering design costs are estimated for multi-use path improvements on the north side of the street only and would be constructed within the existing road right of way. Proposed construction costs have been estimated at \$777,000.

Plan and profile plans sheets would be prepared for the entire corridor. Assuming a 1" = 20' scale a total of 12 plan and profile sheets would be required. The limits of each sheet would be similar to Figures IV-2a throughout IV-2l included in the Corridor Study. We have evaluated the survey and design hours required for each plan and profile sheet and estimate that the cost per street would range from \$3000 to \$5500 depending on the improvements required within each block. In estimating these costs, consideration has been given to detailing the design of new driveways, accessible ramps, bulbouts, traffic calming measures, drainage facilities and utility relocation.

In addition to the plans and profile sheets noted above, the Chestnut Street improvement plans would include cross sections sheets (driveways and intersections), intersection details, drainage details and City standard detail plan sheets. The scope of work will include coordination with PG&E for service pole relocation, coordination with the Fort Bragg School District for improvements along Redwood School, Dana Gray School and Fort Bragg High School and preliminary, intermediate and final plan, specification and estimate submittal and reviews with the City.

We have previously conducted right of way and topographic surveys along the Corridor. Based on our experience with the irregular grading, street and drainage improvements within the Project area, we have estimated 3 days

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of additional field survey time to complete detailed cross sections, especially at existing driveways and intersections.

In consideration of the scope of work presented herein we estimate the land survey and design costs to complete the Chestnut Street Corridor improvements at \$87,500. This design cost estimate is approximately 11.3% of the estimated construction cost.



We hope that this estimate is responsive to your request.

Very Truly Yours,

KASL Consulting Eng., Inc.

A handwritten signature in black ink, appearing to read "John C. Scroggs", is written over the typed name.

John C. Scroggs

Links to Reference Documents

City of Fort Bragg Chestnut Street Corridor Conceptual Plan and Right of Way Feasibility Study

<http://city.fortbragg.com/pdf/ChestnutStreetStudy2012.pdf>

2009 Bicycle Master Plan

<http://city.fortbragg.com/pdf/BikeMasterPlan9-2009FINAL.pdf>

2011 Residential Streets Safety Plan

http://city.fortbragg.com/pdf/2011_RSSP.pdf

III. PROJECT DEVELOPMENT

PRELIMINARY ALTERNATIVES

Preliminary alternatives developed in consideration of Corridor Planning Criteria are presented in **Figure III-1**. Sections with Class II Bike Lanes, both sides, City standard walkways and no on street parking typically require a right of width of 44 feet, similar to the Chestnut Street section included in the 2010 RSSP and presented in Figure I - 1. Alternative sections which would eliminate on street parking and add a Class I bike path or a multi-use trail on one side of the street, similar to Option 2B of Figure III-1, include:

Multi-Use Trail or Class I Bike Path	=	12 feet
Vertical Curb	=	0.5 feet
5 Foot Separation with Landscaping / Barriers	=	5 feet
Vehicle Lane	=	10 feet
Vehicle Lane Adjacent to Curb	=	12 feet
Vertical Curb	=	0.5 feet
Minimum Sidewalk Opposite Side of Street	=	<u>4 feet</u>
Total Width		44 feet

This street section would also require a 44 foot right of way width as itemized above.

With on street parking and a multi-use trail or Class I Bike Path on one side of the street the typical improved section becomes similar to Option 2C of Figure III-1 and includes:

Multi-Use Trail or Class I Bike Path	=	12 feet
Vertical Curb	=	0.5 feet
Parking Lane	=	7 feet
Vehicle Lane	=	10 feet
Vehicle Lane Adjacent to Curb	=	12 feet
Vertical Curb	=	0.5 feet
Minimum Sidewalk Opposite Side of Street	=	<u>4 feet</u>
Total Width		46 feet

This street section requires a 46 foot right of way.

With 60% of the Chestnut Street Corridor now improved with a right of width less than 44 feet, implementation of either of the above two options is impractical.

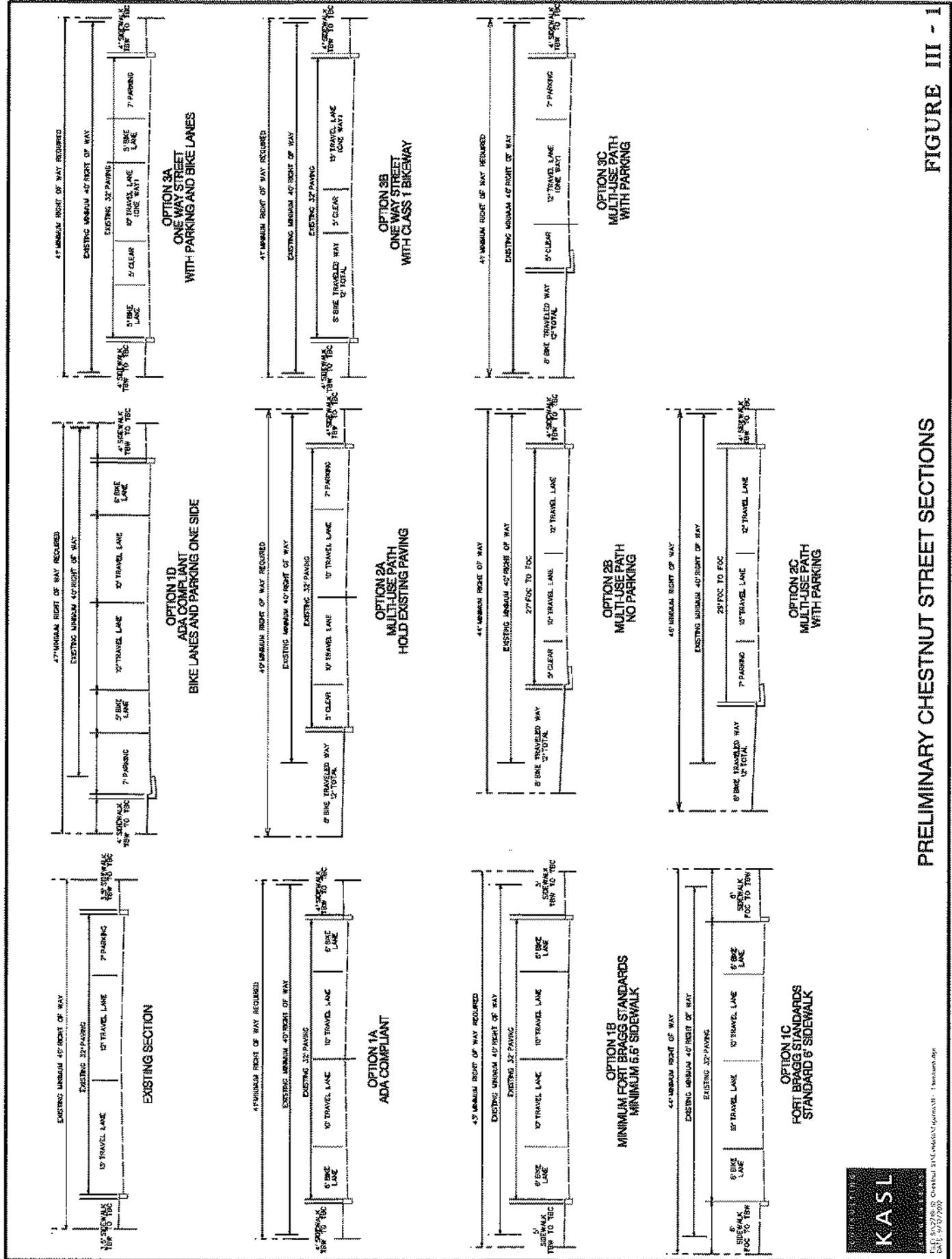


FIGURE III - 1

PRELIMINARY CHESTNUT STREET SECTIONS



To meet the objectives of the Chestnut Street Corridor Study and implement improvements within right of way constraints, one way street alternatives were developed. Alternatives with Chestnut Street converted to one way between Whipple and Lincoln were considered with the following typical sections:

<u>Bike Lane Option</u>		
Minimum 4 Foot Wide Sidewalk	=	4 feet
Vertical Curb	=	0.5 feet
Parking Lane	=	7 feet
Bike Lane	=	5 feet
Vehicle Lane (one way)	=	10 feet
5 foot Separation with Landscaping / Barriers	=	5 feet
Bike Lane	=	5 feet
Vertical Curb	=	0.5 feet
Minimum 4 Foot Wide Sidewalk	=	<u>4 feet</u>
	Total Width	41 feet

or:

<u>Multi-Use Trail or Class I Bike Lane Option</u>		
Minimum 4 Foot Wide Sidewalk	=	4 feet
Vertical Curb	=	0.5 feet
Parking Lane	=	7 feet
Bike Lane	=	5 feet
Vehicle Lane (one way)	=	11 feet
5 foot Separation with Landscaping / Barriers	=	5 feet
Vertical Curb	=	0.5 feet
Multi-Use Trail	=	<u>12 feet</u>
	Total Width	40 feet

Sections similar to the above are presented in Option 3A-3C of Figure III-1.

REVISED ALTERNATIVES

After review with the City Staff it was determined that four of the preliminary alternatives presented in Figures III-1 would be further developed for review with the community at the Community Outreach Meeting.

Alternative 1 – This is the 2010 RSSP alternative with Class II Bike Lanes, City standard sidewalks, both sides, but no on street parking (Minimum R / W width required = 44 feet).

Alternative 1A – Similar to the RSSP proposal except that sidewalks are reduced to 4 feet (4 ½ to top back of curb) to meet ADA requirements. (Minimum R / W width required = 40 feet).

Alternative 2 / 3 – This section includes a Class I Bike Path on the north side. A six foot wide sidewalk would provide the required separation between the Class I path and vehicle lanes. Parking is retained on the south side. Chestnut Street would be converted to one way between Whipple and Lincoln. This section could be constructed within the existing right of way except for the reach between

Redwood School and Dana Grey School. A 42.5 foot right of way would be required in this area to construct a two way street section without on street parking. Right of way would be required from 8 parcels on the north side of the street. Cooperation would also be required from the Fort Bragg School District along the Redwood School frontage.

Alternative 4 – In this alternative, Chestnut Street would be improved with a multi-use path on the north side of the street. Parking would be retained between Franklin and Whipple but shifted from the south to the north side to provide the required separation between the multi-use path and vehicle lanes. East of Whipple on street parking would be eliminated. The multi-use trail on the north side would be separated from the vehicle lanes by an infiltration trench constructed with permeable concrete and delineated by pavers or stamped concrete. The Alternative 4 section could be constructed within the limits of the existing right of way.

Revised Chestnut Street Alternatives developed for review at the Public Outreach Meeting are presented in **Figure III-2**.

PUBLIC PARTICIPATION

Public Outreach Meeting

Approximately 50 Fort Bragg residents attended the Chestnut Street Public Outreach Meeting held on June 8, 2012 at the John Diederich Center. The meeting was monitored by City Staff, Chestnut Street consultants and Sergeant Brandon Lee of the Fort Bragg Police Department.

Mostly in attendance were project area residents. A few of the attendees were parents of school age children who regularly use Chestnut Street as part of their home to school commute.

Participants agreed that Chestnut Street needs improved pedestrian walkways and traffic calming measures. Participants were in favor of:

- Wider sidewalks (or other suitable, wider, pedestrian paths)
- Adding stop signs, enhanced crosswalks and speed “tables” (raised crosswalks) on Chestnut Street to reduce speed.
- Adding ADA compliant features such as ramps

Sgt. Brandon Lee of the Fort Bragg Police Department recommended additional electronic speed advisory signs. Currently, eastbound Chestnut Street vehicles are advised of speeds in the vicinity of the Redwood and Dana Gray Schools. A westbound speed advisory sign was suggested.

Participants recommended that future Chestnut Street sidewalk improvements should be focused on the north side of the street. Along the south side participants encouraged the City to work with PG&E to remove / relocate power poles and guide poles which now block / hinder the pedestrian path of trail.

There were several people that requested wider sidewalks. There was support for a multi-use trail on the north side. One person commented that when they bike to

school with their kids, they avoid all the streets with bike lanes – if the streets have bike lanes they are too wide and cars go too fast. They would rather use a multi use path separated from the vehicle lanes.

Participants agreed that:

- A “safe route to school” currently does not exist on Chestnut Street
- The do-nothing alternative is not acceptable

Mixed responses were obtained regarding on street parking, right of way acquisition and converting Chestnut Street to a one-way street. One resident suggested that reducing Chestnut Street parking would improve safety. A 25 foot red zone at intersections was suggested to increase corner sight distance and visibility. About half of the meeting attendees supported elimination of on street parking. One participant that lives on Chestnut between Olsen and Lincoln would have a difficult time losing on-street parking as not enough parking is available on their property. Another Chestnut Street resident discussed accessibility issues. She does not have a driveway, alley or garage, and parks on the street to get to her house. A third attendee lives on South Harold but has no parking on her property for her guests. They park on Chestnut when they visit. The narrow streets south of Chestnut have on street parking restrictions. Residents of those streets use Chestnut Street for guest parking. One participant suggested that it would be best to just widen the sidewalks and leave the parking as is.

Converting a portion of Chestnut Street to one-way (Whipple to Lincoln) was received with mixed reviews. Many of the Chestnut Street residents were in favor of the one-way street conversion as this measure could reduce Chestnut Street traffic volumes and provide calming. Residents suggested that both one-way westbound as well as one-way east bound conversion be evaluated. Making Chestnut Street one-way eastbound received about 30% support, and others thought it would make more sense to see it one-way westbound.

Residents are reluctant to give up right of way but some are willing to consider limited (1 to 2 foot) right of way acquisition by the City. Residents wanted to be assured that giving up right of way would not result in costs to them nor would it place the travel way closer to their homes. Additional right of way to provide for wider walkways may be acceptable. Additional right of way for wider vehicle lanes or for vehicles travelling closer to their homes is not acceptable.

There was a consensus that people drive too fast on Chestnut Street and it is not safe to walk or bike on this street. The school busses “fly by small children” walking on the street, people speed through at all hours of the night, and after football games. Suggestions to improve conditions included re-routing school busses, additional police surveillance, a stop sign at Corry, specifically, and generally more stop signs. Fifty percent of attendants would like to see high visibility crosswalks, 50% would like to see speed tables. Other discussions included improving Maple Street as the Safe Routes to School alternative and increasing school bus service to improve safety for kids. Only 3% liked bulb outs.

After the public forum portion of the meeting approximately one-half of the meeting participants remained to discuss preliminary Chestnut Street alternatives one-on-one with City staff and consultants and to evaluate how implementation of each alternative would specifically impact their property.

Survey Results

In an effort to reach as many Chestnut area residents as possible, survey forms were mailed and posted on line. Door to door surveys were also conducted. A copy of the Survey Form is presented in **Figure III-3**. A total of 37 surveys were received.

Sidewalks and traffic calming ranked as the most important priority of the survey participants. Bicycle accommodation also ranked as an important priority. Although a few participants viewed retention of on street parking as the most important, significantly more respondents listed retention of on street parking as less important to the least important. Installation of more traffic lights and pavement improvements were neither very important nor the least important items.

Written comments were reviewed and compiled. Results indicate a strong desire for wider sidewalks and safe bicycle access, as well as traffic calming. Many comments reflected a desire to widen sidewalks and remove poles and other structures that block access along the sidewalks. A strong interest was also expressed in providing safe bicycle access and support was given to the multi-use trail option to achieve this goal. Traffic calming was a common topic, although written comments varied on how to achieve calming. Ideas included more stop signs, traffic light, bulb outs, no bulb outs and chicanes rather than bulbouts.

Similar to the feedback received at the Public Outreach Meeting, survey results were mixed with respect to on street parking and converting Chestnut Street to one way. While some responses supported the removal of on street parking to improve safety, other were concerned about accessibility from car to home for disabled persons and how Chestnut Street parking restrictions would impact residents of adjoining streets to the south and other Chestnut Street residents with limited onsite parking options.

There was some interest expressed by survey residents in the one way street option especially by those participants who want to retain on street parking.

City Council Workshop June 25, 2012

On June 25, 2012 a one hour workshop was conducted with the City Council prior to the regular City Council meeting. Right of way survey results and a summary of the physical features present within the Chestnut Street Corridor were reviewed. Project safety and traffic calming goals were discussed and Corridor design criteria were summarized for the council members.

The four project alternatives were presented together with the results of the June 8 Public Workshop and survey findings. At the City Council Workshop it was recommended that Alternative 1, the 2010 RSSP Proposal be eliminated from further consideration because:

- Right of way acquisition would be required from at least 41 of the 71 parcels (\pm 60%) of the Chestnut Street properties. On street parking would be eliminated.
- The Class II Bikeways and the 10 foot vehicle lanes included in this alternative would not achieve the bicycle safety goals of the Project.
- This alternative includes relative high costs for the limited safety and access benefits achieved.



Please complete this brief survey!

Name: _____ Business Name if applicable _____

Address: _____

The goal of the project is to improve pedestrian safety as well as seek input to help define the future of the Chestnut Street corridor. We welcome your input and feedback.

Please rank the following in order of importance (1-7) with 1 being the most important and 7 being the least important

- ___ Bicycle Lanes ___ Traffic calming (speed bumps/bulb outs, roundabouts)
- ___ Sidewalks ___ Bicycle Lanes ___ Traffic Lights
- ___ On-Street Parking ___ Improved Pavement Quality ___ Other (please specify)

For this project to be successful it should be _____ *(Please fill in the blank below)*

Please include other comments, concerns or questions regarding the project here.



For more information on the project or to stay updated please visit our Facebook page at: www.facebook.com/ChestnutStreetProject



It was further recommended that Alternative 1A, be eliminated from further consideration. This alternative would require little right of way acquisition (no more than 5 parcels would be impacted) and would be significantly less costly than Alternative 1A, however, if implemented:

- Neither pedestrian safety nor bicycle safety goals would be adequately achieved.
- Sidewalk improvements would not meet City Standards.
- On street parking would be eliminated.

It was recommended that Alternative 2/3 and Alternative 4 be further refined and developed in response to City Council considerations and directives. Both alternatives include a multi-use path on the north side of the Corridor. Within the constrained right of way zone extending from Whipple to Lincoln conversion of Chestnut Street to a one way street is proposed as part of Alternative 2 /3. Project area traffic circulation impacts associated with modifying Chestnut Street to one way eastbound or to one way westbound were reviewed with the City Council. These one way circulation exhibits are presented in **Figures III-4 and III-5**.

East of Lincoln Street a minimum 42-½ foot right of way would be needed to construct the Alternative 2/3 improvements. Chestnut Street would operate as a 2 way street in this segment. On street parking would be eliminated. Right of way would be needed from 8 residential parcels, all located on the north side of the roadway between Redwood School and Dana Grey School. Based on a preliminary survey of these property owners and the evaluation of existing building setbacks and access it would be difficult to obtain an additional 2 ½ feet of right of way from at least 2 of these properties. East of Lincoln, a total of 43 on street parking spaces would be eliminated with the Alternative 2/3 plan.

The City Council directed staff to further develop Alternative 2/3 and extend the one way portion of Chestnut to begin at Harrison Street on the west and terminate at Dana Street on the east.

With respect to Alternative 4, the City Council suggested that additional evaluation is needed to identify potential impacts of the infiltration trench on existing underground utilities on the north side of the street. The trench section would need to be modified at each driveway, alley and street crossing. Consideration will be given to modifying the infiltration trench to a raised median to provide the necessary separation between the travel lane and the multi-use path. Similar to Alternative 1 and 1A, implementation of Alternative 4 would eliminate on street parking east of Whipple.

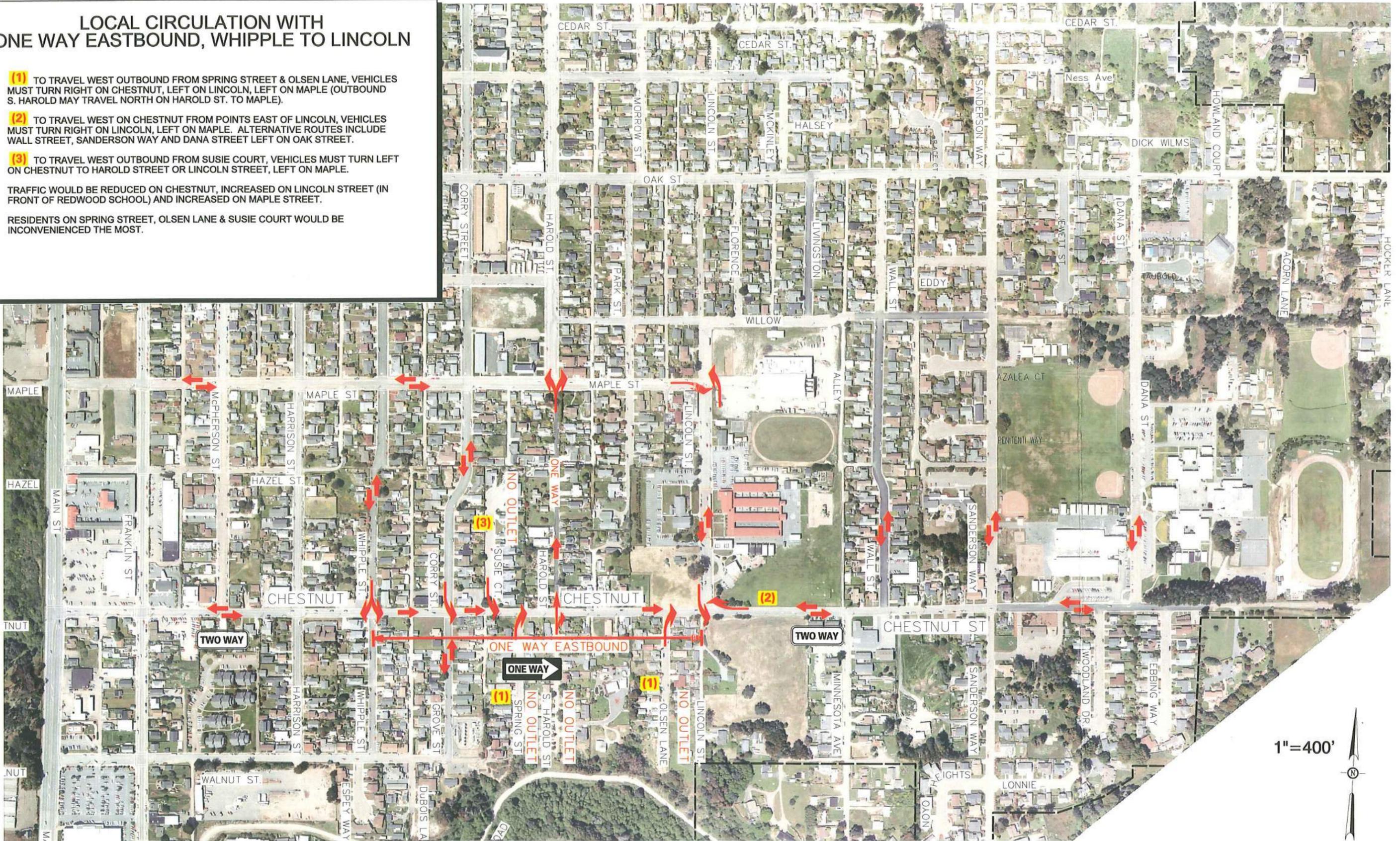
Preliminary cost breakdowns were presented to the City Council for Alternative 2/3 and Alternative 4.

LOCAL CIRCULATION WITH ONE WAY EASTBOUND, WHIPPLE TO LINCOLN

- (1)** TO TRAVEL WEST OUTBOUND FROM SPRING STREET & OLSEN LANE, VEHICLES MUST TURN RIGHT ON CHESTNUT, LEFT ON LINCOLN, LEFT ON MAPLE (OUTBOUND S. HAROLD MAY TRAVEL NORTH ON HAROLD ST. TO MAPLE).
- (2)** TO TRAVEL WEST ON CHESTNUT FROM POINTS EAST OF LINCOLN, VEHICLES MUST TURN RIGHT ON LINCOLN, LEFT ON MAPLE. ALTERNATIVE ROUTES INCLUDE WALL STREET, SANDERSON WAY AND DANA STREET LEFT ON OAK STREET.
- (3)** TO TRAVEL WEST OUTBOUND FROM SUSIE COURT, VEHICLES MUST TURN LEFT ON CHESTNUT TO HAROLD STREET OR LINCOLN STREET, LEFT ON MAPLE.

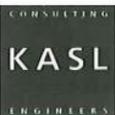
TRAFFIC WOULD BE REDUCED ON CHESTNUT, INCREASED ON LINCOLN STREET (IN FRONT OF REDWOOD SCHOOL) AND INCREASED ON MAPLE STREET.

RESIDENTS ON SPRING STREET, OLSEN LANE & SUSIE COURT WOULD BE INCONVENIENCED THE MOST.



ALTERNATIVE 2/3 PLAN: CIRCULATION WITH ONE WAY EASTBOUND, WHIPPLE TO LINCOLN

FILE: S:\2719-30_Chestnut_S\Exhibits\Figures\III - 4_CIRCULATION.dgn
DATE: 9/12/2012

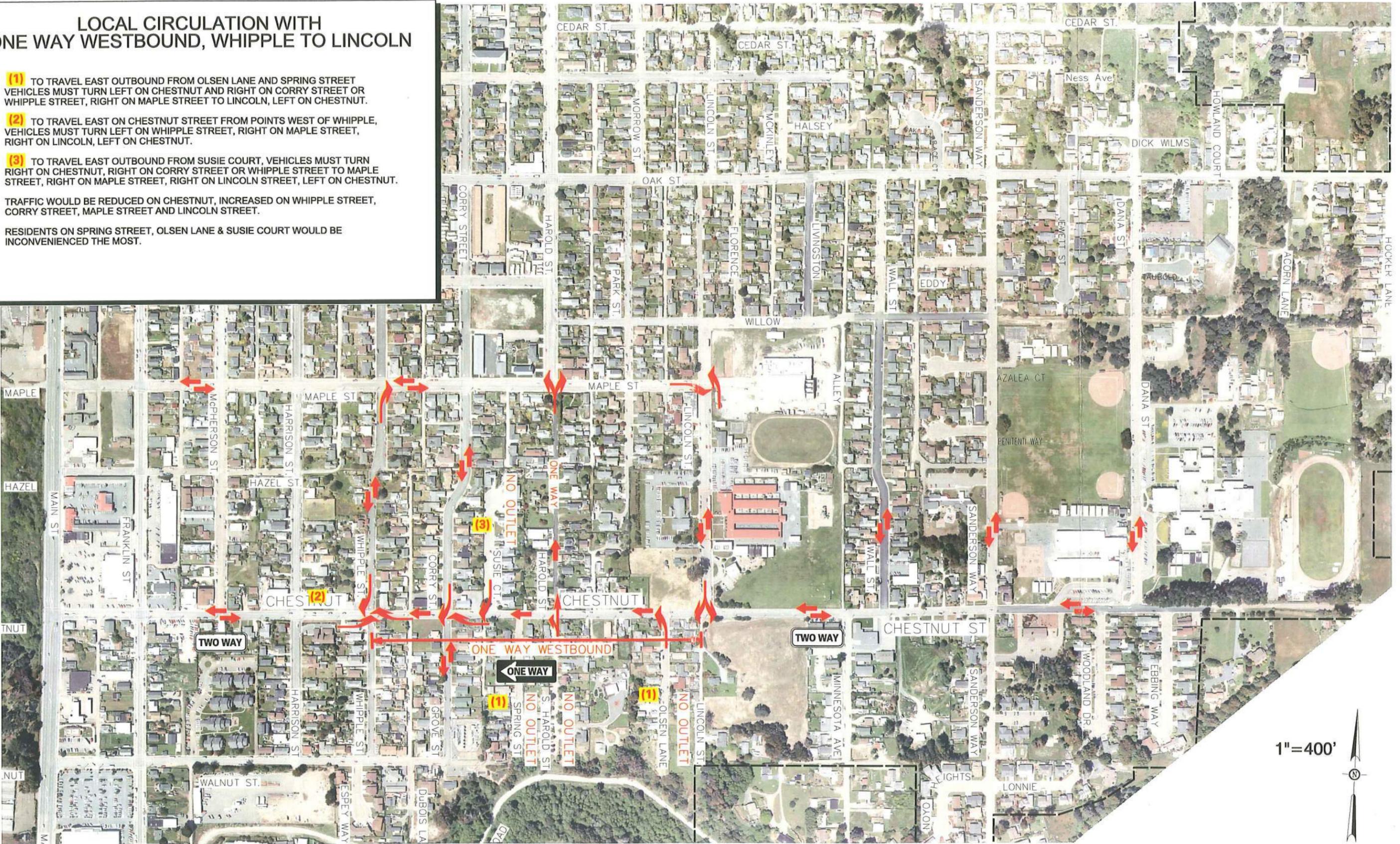


LOCAL CIRCULATION WITH ONE WAY WESTBOUND, WHIPPLE TO LINCOLN

- (1)** TO TRAVEL EAST OUTBOUND FROM OLSEN LANE AND SPRING STREET VEHICLES MUST TURN LEFT ON CHESTNUT AND RIGHT ON CORRY STREET OR WHIPPLE STREET, RIGHT ON MAPLE STREET TO LINCOLN, LEFT ON CHESTNUT.
- (2)** TO TRAVEL EAST ON CHESTNUT STREET FROM POINTS WEST OF WHIPPLE, VEHICLES MUST TURN LEFT ON WHIPPLE STREET, RIGHT ON MAPLE STREET, RIGHT ON LINCOLN, LEFT ON CHESTNUT.
- (3)** TO TRAVEL EAST OUTBOUND FROM SUSIE COURT, VEHICLES MUST TURN RIGHT ON CHESTNUT, RIGHT ON CORRY STREET OR WHIPPLE STREET TO MAPLE STREET, RIGHT ON MAPLE STREET, RIGHT ON LINCOLN STREET, LEFT ON CHESTNUT.

TRAFFIC WOULD BE REDUCED ON CHESTNUT, INCREASED ON WHIPPLE STREET, CORRY STREET, MAPLE STREET AND LINCOLN STREET.

RESIDENTS ON SPRING STREET, OLSEN LANE & SUSIE COURT WOULD BE INCONVENIENCED THE MOST.



ALTERNATIVE 2/3 PLAN: CIRCULATION WITH ONE WAY WESTBOUND, WHIPPLE TO LINCOLN

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DATE: 9/12/2012



ALTERNATIVES DEVELOPED IN RESPONSE TO PUBLIC REVIEW AND CITY COUNCIL COMMENTS

Three Chestnut Street options were developed in response to the comments received from the public outreach, public surveys and City Council workshop activities.

Option A

Option A presented in **Figure III-6** includes a multi-use path on the north side of the street. Within the commercial / Residential Zone, Franklin to Whipple and within a portion of the School / Residential Zone between Sanderson and the east end of the street, a 12 foot wide path is proposed with an 8 foot wide combination pedestrian / bicycle way and 2 foot wide shoulder, each side. Pavers or stamped concrete could be used to identify the shoulders. Existing widened walkways along Dana Grey School would be utilized as part of this Plan. Existing curb, gutter and sidewalk improvements on the south side of the street would remain unimproved. On street parking would be shifted from the south side of the street to the north side.

Option B

The Option B Plan is presented in **Figure III-7**. This Plan shifts the multi-use path to the south side of the street. A raised utility median is proposed to serve as the separation between the multi-use path and the vehicle lanes. Existing utility poles, fire hydrant, street signs and like obstructions now located within and adjacent to the south side sidewalks would be relocated to the raised median area. A six foot wide sidewalk, per Fort Bragg Standards, is proposed on the north side of the street. On street parking is eliminated

Option C

Option C is similar to the Alternative 2/3 Plan reviewed with residents at the Public Outreach Meeting and with the City Council at the June 25 Council Workshop. The one way street limits would begin at Harrison and extend to Dana Street. Option C is presented in **Figure III-8**.

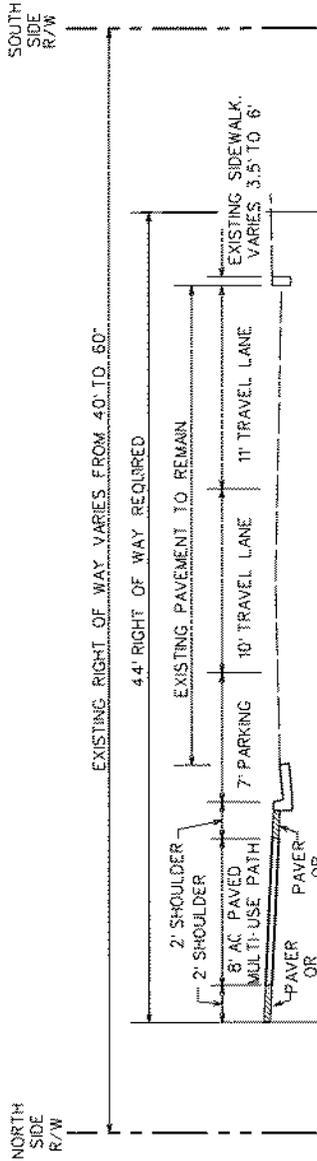
Quantity and Cost Estimates

Block by block quantity and cost estimates for Options A, B & C are presented in **Table III-1A, 1B and 1C**. A summary of estimated Chestnut Street Corridor costs is presented in **Table III-2**.

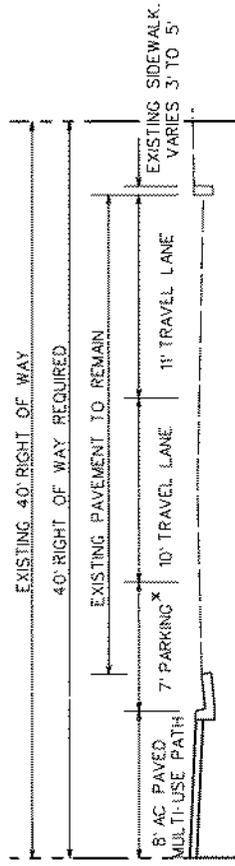
Construction costs were reduced for Option A and B by utilizing the existing widened sidewalk along Dana Grey Street. Further refinements of Option A could include utilizing the existing 5-1/2 foot wide sidewalk along Lincoln Street "as is" and constructing new curb, gutter and sidewalk on the south side of Chestnut west of Spring Street (\pm 160 feet) and east of Lincoln (\pm 450 feet).

City Council Meeting August 27, 2012

City Staff presented Chestnut Street Options A, B and C to the Fort Bragg City Council on August 27. Features of each alternative were summarized. Staff reviewed how each alternative responded to comments and suggestions received from the Public and how these refined options responded to the City Council's concerns and directives developed from the June 25, City Council Workshop. A summary of the current Chestnut Street options, as presented by City Staff for the City Council is included in **Table III-3**.

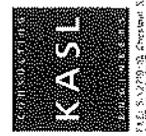


OPTION A
FRANKLIN STREET TO WHIPPLE STREET



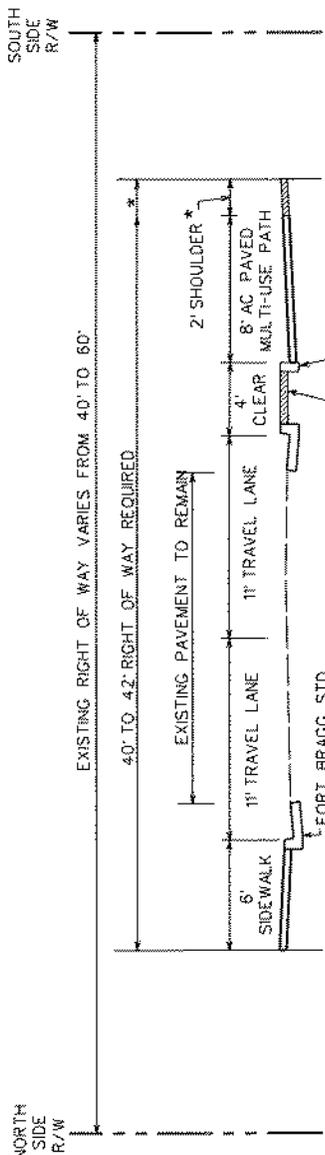
OPTION A
WHIPPLE STREET TO 80' EAST OF SANDERSON WAY

* NO PARKING WITHIN 25' OF GARAGES ADJACENT TO RIGHT OF WAY (PARCELS 018-100-04-00 AND 018-100-68-00)



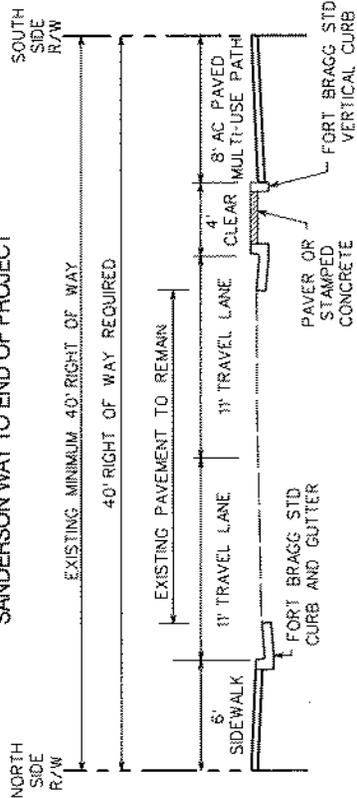
CHESTNUT STREET IMPROVEMENT
OPTION A

FIGURE III - 6



* 2' SHOULDER WHERE RIGHT OF WAY IS AVAILABLE

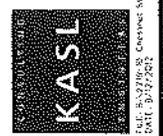
**OPTION B
FRANKLIN STREET TO WHIPPLE STREET
AND
SANDERSON WAY TO END OF PROJECT**

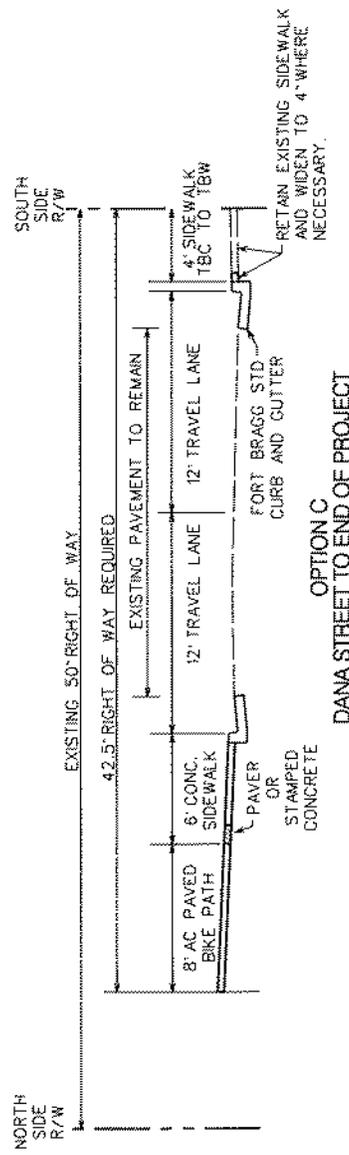
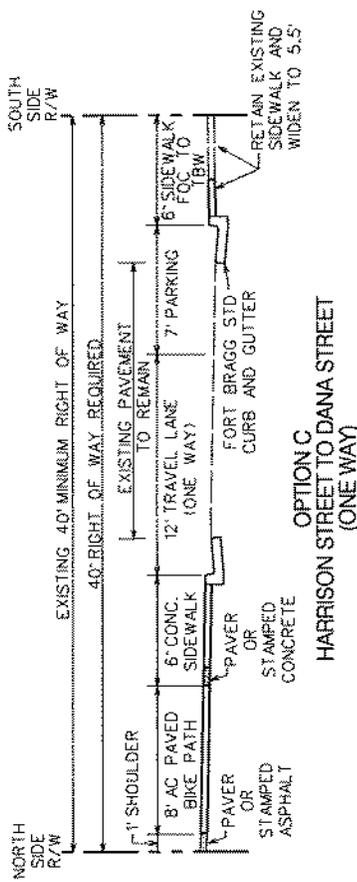
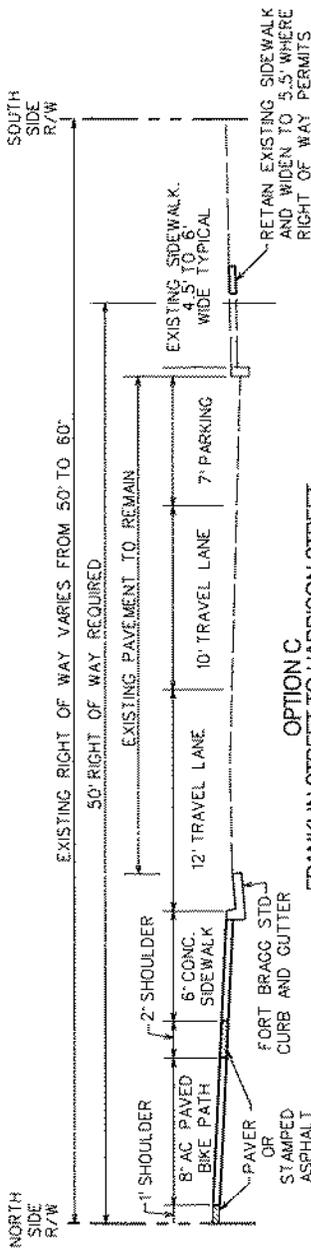


**OPTION B
WHIPPLE STREET TO SANDERSON WAY**

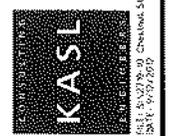
**CHESTNUT STREET IMPROVEMENT
OPTION B**

FIGURE III - 7





CHESTNUT STREET IMPROVEMENT
OPTION C



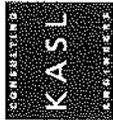


TABLE III - 1A
Chestnut Street Corridor
Option A

Block	Bike Path (1)		New Curb & Gutter	Driveway Conform		Relocate Street Sign		Relocate Inlet (2)		Right of Way Acquisition		Relocate Joint Pole		Relocate Service Pole		Estimated Cost By Block
	SF	EA		EA	EA	EA	EA	EA	SF	EA	EA	EA	EA	EA		
Franklin to McPherson	3,240	2	340	2	1	1	0	0	0	0	0	0	0	0	0	\$44,700
McPherson to Harrison	3,600	1	320	1	1	1	0	0	0	0	0	0	0	0	0	\$48,500
Harrison to Whipple	3,600	2	320	2	2	2	0	0	0	0	0	0	0	0	0	\$51,000
Whipple to Grove	2,320	2	330	2	2	2	0	0	0	0	0	0	0	0	0	\$39,850
Grove to Harold	3,360	1	460	1	4	3	0	0	0	0	0	0	0	0	0	\$67,300
Harold to Lincoln	5,040	2	670	2	4	4	0	0	0	0	0	0	0	0	0	\$72,950
Lincoln to Wall	6,080	1	780	1	4	2	0	0	0	0	0	0	0	0	0	\$83,900
Wall to Sanderson	3,600	3	470	3	3	3	0	0	0	0	0	0	0	0	0	\$37,250
Sanderson to Woodland	840	1	240	1	3	1	0	0	0	0	0	0	0	0	0	\$143,700
Woodland to End	1,260	0	0	0	1	1	0	0	0	0	0	0	0	0	0	\$126,800
Subtotal	32,940	15	3,930	15	26	12	0	0	0	0	6	10	0	0	0	\$715,950

25% Contingency

\$176,988

Estimated Total

\$894,938

South Side

Block	Bike Path (1)		New Curb & gutter	Driveway Conform		Relocate Street Sign		Relocate Inlet (2)		Right of Way Acquisition		Relocate Joint Pole		Relocate Service Pole		Estimated Cost By Block
	SF	EA		EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA		
Franklin to McPherson	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
McPherson to Harrison	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Harrison to Whipple	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Whipple to Grove	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Grove to Harold	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Harold to Lincoln	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Lincoln to Wall	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Wall to Sanderson	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Sanderson to Woodland	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Woodland to End	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0
Subtotal	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	\$0

25% Contingency

\$0

Estimated Total

\$0

- (1) Includes ADA compliant ramps.
- (2) Includes drainage lateral and connection to manhole.



TABLE III - 1B
Chestnut Street Corridor
Option B

Block	Multi-use Path (1) SF	Utility Corridor Paving LF	New Curb & Gutter LF	New Vertical Curb LF	New Sidewalk (2) SF	Driveway Conform EA	Relocate Street Sign EA	Relocate Inlet (2) EA	Relocate Hydrant EA	Right of Way Acquisition SF	Relocate Joint Pole EA	Relocate Service Pole EA	Estimated Cost By Block
Franklin to McPherson	0	0	340	0	1,870	2	2	0	0	0	0	1	\$33,460
McPherson to Harrison	0	0	320	0	1,760	0	1	1	0	0	0	2	\$42,560
Harrison to Whipple	0	0	320	0	1,760	2	3	1	0	0	0	2	\$47,560
Whipple to Grove	0	0	330	0	1,815	2	2	1	0	0	0	1	\$42,770
Grove to Harold	0	0	460	0	2,525	1	4	3	0	0	0	1	\$70,700
Harold to Lincoln	0	0	680	0	3,700	2	3	2	0	0	0	1	\$77,100
Lincoln to Wall	0	0	780	0	4,290	1	4	2	0	0	0	2	\$87,820
Wall to Sanderson	0	0	460	0	2,530	4	2	0	0	0	0	0	\$40,740
Sanderson to Woodland	0	0	240	0	480	1	1	1	0	0	0	0	\$22,340
Woodland to End	0	0	0	0	560	0	0	0	0	0	0	0	\$4,480
Subtotal	0	0	3,930	0	21,290	15	22	11	0	0	0	10	\$469,570

25% Contingency

\$117,393

Estimated Total

\$586,963

Block	Multi-use Path (1) SF	Utility Corridor Paving LF	New Curb & gutter LF	New Vertical Curb LF	New Sidewalk (2) SF	Driveway Conform EA	Relocate Street Sign EA	Relocate Inlet (2) EA	Relocate Hydrant EA	Right of Way Acquisition SF	Relocate Joint Pole EA	Relocate Service Pole EA	Estimated Cost By Block
Franklin to McPherson	2,690	340	340	340	0	2	0	2	0	0	3	0	\$177,150
McPherson to Harrison	3,200	320	320	320	0	3	1	0	1	0	2	0	\$123,600
Harrison to Whipple	2,930	200	200	200	0	1	1	2	1	0	2	0	\$131,650
Whipple to Grove	2,320	290	290	290	0	0	0	2	1	0	2	0	\$131,050
Grove to Harold	3,360	420	420	420	0	0	1	2	0	0	5	0	\$260,400
Harold to Lincoln	4,880	610	610	610	0	2	2	4	2	0	5	0	\$309,950
Lincoln to Wall	6,320	790	790	790	0	6	2	4	1	0	6	0	\$371,550
Wall to Sanderson	3,680	460	460	460	0	7	1	0	0	0	4	0	\$218,200
Sanderson to Woodland	3,600	450	450	450	0	4	4	2	0	0	1	0	\$112,750
Woodland to End	3,280	410	410	410	0	1	1	2	0	0	0	0	\$61,450
Subtotal	36,460	4,290	4,290	4,290	0	26	13	20	6	0	30	0	\$1,897,750

25% Contingency

\$474,436

Estimated Total

\$2,372,188

- (1) Includes ADA compliant ramp.
- (2) Includes drainage lateral and connection to manhole.



TABLE III - 1C
Chestnut Street Corridor
Option C

Block	Bike Path (1)	New Curb & Gutter	Widen Sidewalk	New Sidewalk (1)	Driveway Conform	Relocate Street Sign	Relocate Inlet (2)	Relocate Hydrant	Right of Way Acquisition	Relocate Joint Pole	Relocate Service Pole	Estimated Cost By Block
	SF	LF	SF	SF	EA	EA	EA	EA	SF	EA	EA	
Franklin to McPherson	3,520	260	0	1,760	2	2	0	0	1,150	0	1	\$71,160
McPherson to Harrison	3,300	320	0	1,760	0	1	1	0	0	0	2	\$59,080
Harrison to Whipple	2,700	320	0	1,760	2	3	1	0	0	0	2	\$61,080
Whipple to Grove	2,610	330	0	1,815	2	2	1	0	0	0	1	\$55,820
Grove to Harold	3,780	460	0	2,530	1	4	3	0	0	0	1	\$69,640
Harold to Lincoln	5,760	680	0	3,740	2	3	2	0	0	0	1	\$106,220
Lincoln to Wall	6,840	780	0	4,290	1	4	2	0	0	0	2	\$122,020
Wall to Sanderson	3,960	460	0	2,530	4	2	0	0	0	0	0	\$60,540
Sanderson to Dana	5,040	640	0	3,520	1	2	1	0	0	3	0	\$202,360
Dana to End	2,240	290	0	1,595	0	1	0	0	0	3	0	\$151,710
Subtotal	39,750	4,540	0	25,300	15	24	11	0	1,150	6	10	\$979,650

25% Contingency \$244,913

Estimated Total

\$1,224,563

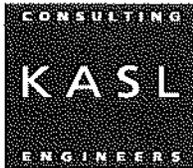
Block	Bike Path (1)	New Curb & Gutter	Widen Sidewalk	New Sidewalk (1)	Driveway Conform	Relocate Street Sign	Relocate Inlet (2)	Relocate Hydrant	Right of Way Acquisition	Relocate Joint Pole	Relocate Service Pole	Estimated Cost By Block
	SF	LF	SF	SF	EA	EA	EA	EA	SF	EA	EA	
Franklin to McPherson	0	0	105	0	1	0	0	0	0	2	0	\$64,100
McPherson to Harrison	0	0	200	0	1	1	0	0	0	2	0	\$86,500
Harrison to Whipple	0	165	300	0	0	1	1	1	0	2	0	\$104,125
Whipple to Grove	0	330	330	0	0	0	1	1	0	2	0	\$108,350
Grove to Harold	0	480	440	770	1	2	1	0	140	5	0	\$242,760
Harold to Lincoln	0	660	700	880	1	2	2	2	300	5	0	\$273,540
Lincoln to Wall	0	780	470	2,585	5	2	1	1	0	6	0	\$314,080
Wall to Sanderson	0	470	470	0	0	2	0	0	0	4	0	\$182,150
Sanderson to Dana	0	560	1,035	0	0	2	1	0	0	1	0	\$85,790
Dana to End	0	0	0	0	0	0	0	0	0	0	0	\$0
Subtotal	0	3,445	4,050	4,235	9	12	7	5	440	29	0	\$1,481,305

25% Contingency \$370,326

Estimated Total

\$1,851,631

(1) Includes ADA compliant ramp.
(2) Includes drainage lateral and connection to manhole.



**TABLE III - 2
Chestnut Street Corridor Options**

Summary of Estimated Costs

OPTION A	North Side	South Side	Estimated Total
Surface Improvements (1)	\$305,950	\$0	\$305,950
Water and Storm Drainage Utilities	\$120,000	\$0	\$120,000
Right of Way Acquisition	\$0	\$0	\$0
Joint and Service Pole Relocation (2)	<u>\$290,000</u>	<u>\$0</u>	<u>\$290,000</u>
SUBTOTAL	\$715,950	\$0	\$715,950
25% Contingency	<u>\$178,988</u>	<u>\$0</u>	<u>\$178,988</u>
Estimated Total	<u>\$894,938</u>	<u>\$0</u>	<u>\$894,938</u>
OPTION B	North Side	South Side	Estimated Total
Surface Improvements (1)	\$309,570	\$476,750	\$786,320
Water and Storm Drainage Utilities	\$110,000	\$221,000	\$331,000
Right of Way Acquisition	\$0	\$0	\$0
Joint and Service Pole Relocation (2)	<u>\$50,000</u>	<u>\$1,200,000</u>	<u>\$1,250,000</u>
SUBTOTAL	\$469,570	\$1,897,750	\$2,367,320
25% Contingency	<u>\$117,393</u>	<u>\$474,438</u>	<u>\$591,830</u>
Estimated Total	<u>\$586,963</u>	<u>\$2,372,188</u>	<u>\$2,959,150</u>
OPTION C	North Side	South Side	Estimated Total
Surface Improvements (1)	\$556,650	\$225,005	\$781,655
Water and Storm Drainage Utilities	\$110,000	\$87,500	\$197,500
Right of Way Acquisition	\$23,000	\$8,800	\$31,800
Joint and Service Pole Relocation (2)	<u>\$290,000</u>	<u>\$1,160,000</u>	<u>\$1,450,000</u>
SUBTOTAL	\$979,650	\$1,481,305	\$2,460,955
25% Contingency	<u>\$244,913</u>	<u>\$370,326</u>	<u>\$615,239</u>
Estimated Total	<u>\$1,224,563</u>	<u>\$1,851,631</u>	<u>\$3,076,194</u>

(1) Includes bike paths, multi use paths, sidewalks, curbs and gutters.

(2) Portion of costs may be paid by PG&E.

August 13, 2012

TABLE III – 3
Summary Chestnut Street Design Options ⁽¹⁾

	Option A	Option B	Option C
Pros	<p>Multi-use path increases pedestrian and bike safety on north side</p> <p>Narrows travel lanes by four feet to calm traffic</p> <p>Retains on-street parking but shifts it to the other side of the street, and allows for two way vehicle traffic</p> <p>Lowest cost option</p> <p>Improvements can be completed within existing R/W</p>	<p>Wide sidewalk on north and multi-use path on south increase bicycle and pedestrian safety on both sides, separating bikes and peds</p> <p>Allows for two way vehicle traffic</p> <p>Eliminates on-street parking and narrows travel lanes to calm traffic (best traffic calming option – narrows roadway by 10 feet)</p> <p>Features a safety separation area between bikes and vehicles, that can also accommodate above ground items, such as fire hydrants, light poles and utility poles (best option to accommodate utilities)</p> <p>Improvements can be completed within existing R / W</p>	<p>Increases pedestrian and bicycle safety on both sides, and separates bikes and peds</p> <p>One way street will reduce traffic on Chestnut Street, and road narrowed three feet will provide traffic calming</p> <p>Retains on-street parking.</p> <p>Retains parking in current configuration on south side of street</p> <p>Improvements can be completed within existing R/W</p>
Cons	<p>Does not improve pedestrian safety on the south side</p> <p>Existing on-street parking would be relocated across the street</p> <p>On-street parking lanes will increase the visual size of the street when parking is not utilized, and this will work against traffic calming</p>	<p>Eliminates on-street parking</p> <p>Higher cost</p>	<p>Inconveniences residents driving to or from home; potentially increases greenhouse gas emissions from additional vehicle travel. May result in increased traffic in other nearby neighborhoods</p> <p>Highest cost</p> <p>Future studies and consultations needed to determine feasibility of one way street</p>
Estimated total project cost	\$894,938	\$2,959,151	\$3,076,194

(1) Presented to City of Fort Bragg City Council, August 27, 20120

Potential Funding Sources

PG&E – The cost of undergrounding overhead utilities versus the cost of utility pole relocation was reviewed with PG&E. Approximately \$995,861 has been set aside as Rule 20A credit for undergrounding PG&E electrical poles within the City of Fort Bragg. City Staff received an estimated cost from PG&E of \$2,784,106 for undergrounding utilities along the Chestnut Street Corridor. The cost estimate includes the cost of 4,670 linear feet of main trench, 3,269 linear feet of service laterals, 49 residential and 21 commercial panel conversions. Since only \$995,861 is available as Rule 20A funds, the City would bear the additional \$1,788,245 needed to underground electrical utilities.

The City Council has considered other areas within the City for utilizing the Rule 20A funds. These include sidewalks within the Central Business District, along Oak Street and along Alder Street. With the cost of local participation estimated for completing the undergrounding of Chestnut Street utilities it was determined that other centrally located areas in the City may be a better candidate for a Rule 20A Project.

PG&E has indicated that at least a portion of the cost to relocate poles outside of the right of way would be paid by them, provided that poles are relocated within the City right of way or a utility easement. A utility easement for the poles and overhead wires would be required for Option C, and possibly for service poles on the north side for Option B. The cost to relocate a PG&E service pole is estimated at approximately \$5,000. The cost to relocate a joint pole (a pole with PG&E service as well as cable TV, telephone, etc.) would be approximately \$40,000, with PG&E only paying a portion of that cost attributable to their facilities. If PG&E pays \$5,000 per pole to be relocated, project costs could potentially be reduced by as much as \$80,000 for Option A, \$200,000 for Option B and \$225,000 for Option C.

State Safe Routes to School – Up to \$450,000 may be available for projects that improve safety for children traveling to school by foot or bicycle. The City has previously been awarded State Safe Routes to School grants. With the Safe Routes to School fund limits, a section of the selected Chestnut Street Corridor option could be designated for implementation with this funding source. The School / Residential zone located between Lincoln and the east end of the project has the highest pedestrian and bicycle traffic. Safe Routes to School improvements for this area could be identified as an early project phase with remaining sections of the Chestnut Street Corridor completed as funds become available.

Federal Safe Routes to School - Up to \$1,000,000 may be available for school related safety projects. The City of Fort Bragg has also received a Federal Safe Routes to School grants. As discussed above, portions of a priority segment or segments of the selected Corridor options could be designated for Federal funding with remaining sections of the selected Corridor Plan completed as funds become available.

Transportation Enhancement Grant – Varying amounts of funding could be available for Chestnut Street pedestrian and bicycle facilities through this Caltrans Administrated Program

Bicycle Transportation Account – Up to 25% of available funds (usual award is \$200,000 to \$300,000) may be available for bikeway improvements. In 2009 / 2010 Fort Bragg received BTA Funds to stripe and sign bicycle routes on North Franklin Street and Oak Street and to provide bicycle racks at downtown locations and along the Pacific Coast Bike Route.

California Office of Traffic Safety – Up to \$500,000 may be available for safety activities.

Community Development Block Grant – Up to \$800,000 may be available for transportation projects.

Transportation Development Act – This Caltrans funding is mainly for transit projects, however some funds may be available for bicycle and pedestrian projects.

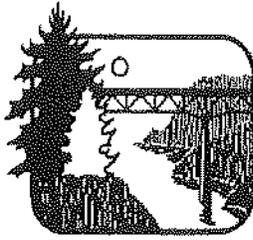
City Sales Tax – An amount estimated at \$750,000 may be available as a local match for state or federal funds.

AB 2766 Funds – These funds come from registered motor vehicle fees. \$30,000 to \$40,000 a year may be available at the discretion of the Air Quality Management District.

Settlement Funds - \$10,000 to \$100,000 may be available as determined by the Air Quality Management District.

City Council Action

The City Council directed staff to proceed with Option A for the Chestnut Street Corridor.



FORT BRAGG UNIFIED SCHOOL DISTRICT

Superintendent
Donald F. Armstrong, Ed.D.

Board of Trustees
Gerald A. Matson
Michelle Norvell, Vice President
Jennifer A. Owen, President
Ryan F. Perkins
Sydney Smith-Tallman

312 South Lincoln Street, Fort Bragg, California 95437-4416

Telephone (707) 961-2850 Fax (707) 964-5002

May 2, 2014

CALTRANS
Division of Local Assistance, MS 1
Attn: Office of Active Transportation and Spec. Prog.
PO Box 942874
Sacramento, CA 94274-0001

Subject: City of Fort Bragg's Active Transportation Program Grant for Chestnut Street Multi-Use Facility

Dear Ms. McWilliam:

On behalf of the Fort Bragg Unified School District, I am pleased to offer my support for the City of Fort Bragg's Active Transportation Program Grant for the Chestnut Street Multi-Use Facility.

Chestnut Street is a busy roadway which provides access to the Fort Bragg High School, Dana Gray Elementary School, and Redwood Elementary School, and to the City's south central neighborhoods. Chestnut Street is a school bus route, and is heavily travelled by pedestrians and bicycles, especially during the early morning and late afternoon hours.

According to a February 2013 Safe Routes to School Survey that teachers at Redwood and Dana Gray Elementary Schools participated in, up to 26% of Dana Gray students (grades three through five) walk or bike to or from school, and up to 12% of Redwood students (grades kindergarten through two) walk or bike to or from school. The main route used by students is along Chestnut Street, which stretches westward nearly a mile, from the schools into the main part of town.

Existing sidewalks along Chestnut Street are narrow and obstructed by utility poles, fire hydrants, and other structures. The proposed project, which includes the creation of a multi-use facility, relocation of utility poles, traffic calming bulb-outs and bump-outs, and new high-visibility sidewalks is a welcome addition. Construction of these improvements will create a safer route to school, which is expected to result in a greater number of students walking and biking to and from school. It is my belief that the actions included in the Active Transportation Grant for the Chestnut Street Multi-Use Facility will significantly improve health and safety for the Fort Bragg Unified School District's students.

Donald F. Armstrong, Ed.D.
Superintendent



FORT BRAGG POLICE DEPARTMENT

250 Cypress Street
Fort Bragg, CA 95437-5437

SCOTT J. MAYBERRY
Chief of Police

Bus: (707) 961-2800
Fax: (707) 961-2806

May 9, 2014

CALTRANS
Division of Local Assistance, MS 1
Attn: Office of Active Transportation and Spec. Prog.
PO Box 942874
Sacramento, CA 94274-0001

Subject: City of Fort Bragg's Active Transportation Program Grant for Chestnut Street Multi-Use Facility

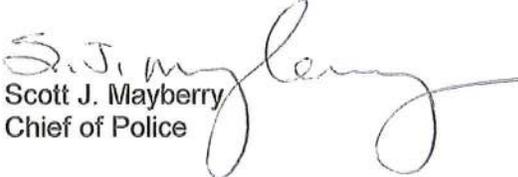
Dear Ms. McWilliam:

As Chief of the Fort Bragg Police Department, I am pleased to offer my support for the City of Fort Bragg's Active Transportation Program Grant for the Chestnut Street Multi-Use Facility.

Chestnut Street is a busy roadway which provides access to the Fort Bragg High School, Dana Gray Elementary School, and Redwood Elementary School, and to the City's south central neighborhoods. Chestnut Street is a school bus route, and is heavily travelled by pedestrians and bicycles, especially during the early morning and late afternoon hours.

According to a February 2013 Safe Routes to School Survey that teachers at Redwood and Dana Gray Elementary Schools participated in, up to 26% of Dana Gray students (grades three through five) walk or bike to or from school, and up to 12% of Redwood students (grades kindergarten through two) walk or bike to or from school. The main route used by students is along Chestnut Street, which stretches westward nearly a mile, from the schools into the main part of town.

Existing sidewalks along Chestnut Street are narrow and obstructed by utility poles, fire hydrants, and other structures. The proposed project, which includes the creation of a multi-use facility, relocation of utility poles, traffic calming bulb-outs and bump-outs, and new high-visibility sidewalks is a welcome addition. Construction of these improvements will create a safer route to school, which is expected to result in a greater number of students walking and biking to and from school. It is my belief that the actions included in the Active Transportation Grant for the Chestnut Street Multi-Use Facility will significantly improve health and safety for walking and biking residents in the Fort Bragg area.


Scott J. Mayberry
Chief of Police

DAN GJERDE
Supervisor
Fourth District



OFFICE PHONE: (707) 463-4221
OFFICE FAX: (707) 463-7237
EMAIL: gjerde@co.mendocino.ca.us

COUNTY OF MENDOCINO
BOARD OF SUPERVISORS
501 Low Gap Road • Room 1010
Ukiah, California 95482

May 8, 2014

CALTRANS
Division of Local Assistance, MS 1
Attn: Office of Active Transportation and Spec. Prog.
PO Box 942874
Sacramento, CA 94274-0001

Subject: City of Fort Bragg's ATP application for the Chestnut Street Multi-Use Facility

Dear Ms. McWilliam:

As a Mendocino County Supervisor, I am pleased to offer my enthusiastic support for the City of Fort Bragg's Active Transportation Program grant application for the Chestnut Street Multi-Use Facility.

Chestnut Street is a busy roadway which provides access to Fort Bragg High School, Dana Gray Elementary School, and Redwood Elementary School, and to the City's southern neighborhoods. As someone who each day walked or rode a bicycle to those schools, I can assure you that the narrow sidewalks (where they exist) on Chestnut Street are heavily travelled by children and adults by foot and bicycles, especially during the early morning and late afternoon hours when parents and bus drivers are traveling on Chestnut.

According to a February 2013 Safe Routes to School Survey, teachers at Redwood and Dana Gray Elementary Schools reported that 26% of Dana Gray students (grades three through five) walk or bike to school, and 12% of Redwood students (grades kindergarten through two) walk or bike to school. The main route used by students is along Chestnut Street, the same route my classmates and I would walk each day to school, back in the 1970s and 1980s.

Existing sidewalks along Chestnut Street are not only narrow, but they are also currently obstructed by utility poles, fire hydrants, and other structures. The proposed project includes the creation of a multi-use facility, the relocation of utility poles, plus the installation of traffic calming bulb-outs and bump-outs, as well as new high-visibility sidewalks. This would be a welcome change. Construction of these improvements will create a safer route to school and should result in even more students walking and riding bicycles. As was apparent by the good attendance at public meetings, it's also clear that the project is supported by the local community. It is my belief that the goals of the Active Transportation Program are successfully achieved by the well-vetted, thoughtfully designed Chestnut Street Multi-Use Facility, and these improvements will make the healthy choice the easy choice for residents of my economically challenged but resilient community.

Thank you for your consideration,

Dan Gjerde
Mendocino County Supervisor, District Four

JARED HUFFMAN
2ND DISTRICT, CALIFORNIA

COMMITTEE ON
NATURAL RESOURCES
COMMITTEE ON THE BUDGET

Congress of the United States
House of Representatives
Washington, DC 20515-0502

WASHINGTON OFFICE
1630 LONGWORTH HOUSE OFFICE BUILDING
WASHINGTON, DC 20515
PHONE: (202) 225-5161
FAX: (202) 225-5163
WEBSITE: huffman.house.gov

May 15, 2014

Teresa McWilliam
Caltrans Division of Local Assistance
Office of Active Transportation and Special Programs (MS 1)
P.O. Box 942874
Sacramento CA 94274-0001

Dear. Ms. McWilliam:

I am writing to offer my support for The City of Fort Bragg's application for an Active Transportation Program (ATP) grant from Caltrans for a multi-use path along the north side of Chestnut Street from Franklin Street to Ebbing Way.

The plan design is outlined in Section IV of the November 2012 City of Fort Bragg Chestnut Street Corridor Conceptual Plan and Right of Way Feasibility Study. The design consists of an 8 to 12 foot wide multi-use pedestrian and bike path along the north side of Chestnut Street, from Franklin Street to the high school at the eastern City Limits. Traffic calming and safety features including bulb-outs and bump outs are also included in the plan.

Currently the City has over \$700,000 in funding for the project and is requesting for the remainder it will take to build, an estimated \$500,000.

I hope that you will give this project your full consideration.

Sincerely,



JARED HUFFMAN
Member of Congress

SAN RAFAEL
999 FIFTH AVENUE, SUITE 290
SAN RAFAEL, CA 94901
PHONE: (415) 258-9657
FAX: (415) 258-9913

PETALUMA
206 G STREET, #3
PETALUMA, CA 94952
PHONE: (707) 981-8967
FAX: (415) 258-9913

UKIAH
559 LOW GAP ROAD
UKIAH, CA 95482
PHONE & FAX: (707) 671-7449

FORT BRAGG
430 NORTH FRANKLIN STREET
P.O. BOX 2208
FORT BRAGG, CA 95437
PHONE: (707) 962-0933
FAX: (707) 962-0905

EUREKA
317 THIRD STREET, SUITE 1
EUREKA, CA 95501
PHONE: (707) 407-3585
FAX: (707) 407-3559

PRINTED ON RECYCLED PAPER



SAFE PASSAGE
Family Resource Center

208 Dana Street · Post Office Box 1718
Fort Bragg, CA 95437

Strengthening our community by strengthening the families that live here.

May 7, 2014

CALTRANS
Division of Local Assistance, MS 1
Attn: Office of Active Transportation and Spec. Prog.
PO Box 942874
Sacramento, CA 94274-0001

Subject: City of Fort Bragg's Active Transportation Program Grant
for Chestnut Street Multi-Use Facility

Dear Ms. McWilliam:

As Executive Director of Safe Passage Family Resource Center, I am pleased to offer my support for the City of Fort Bragg's Active Transportation Program Grant for the Chestnut Street Multi-Use Facility.

I use Chestnut Street every day to get to work. Chestnut is the most often used street for students attending four out of five of our community's schools. Usually I drive, but occasionally I walk or bike. That helps me understand that parents would be more likely to have their children walk or bike to school if this busy thoroughfare was safer for pedestrians and cyclists during the early morning and late afternoon hours.

Existing sidewalks along Chestnut Street are narrow and obstructed by utility poles, fire hydrants, and other structures. The proposed project will make numerous safety improvements to this busy street.

It is my belief that the actions included in the Active Transportation Grant for the Chestnut Street Multi-Use Facility will significantly improve the safety for our community's kids who make the healthy choice to walk or bike to school.

Laura Welter

Laura Welter
Executive Director

Phone: 707-964-3077 email: safepass@mcn.org Fax: 707-964-3087 Tax ID # 68-0430046

City of Fort Bragg

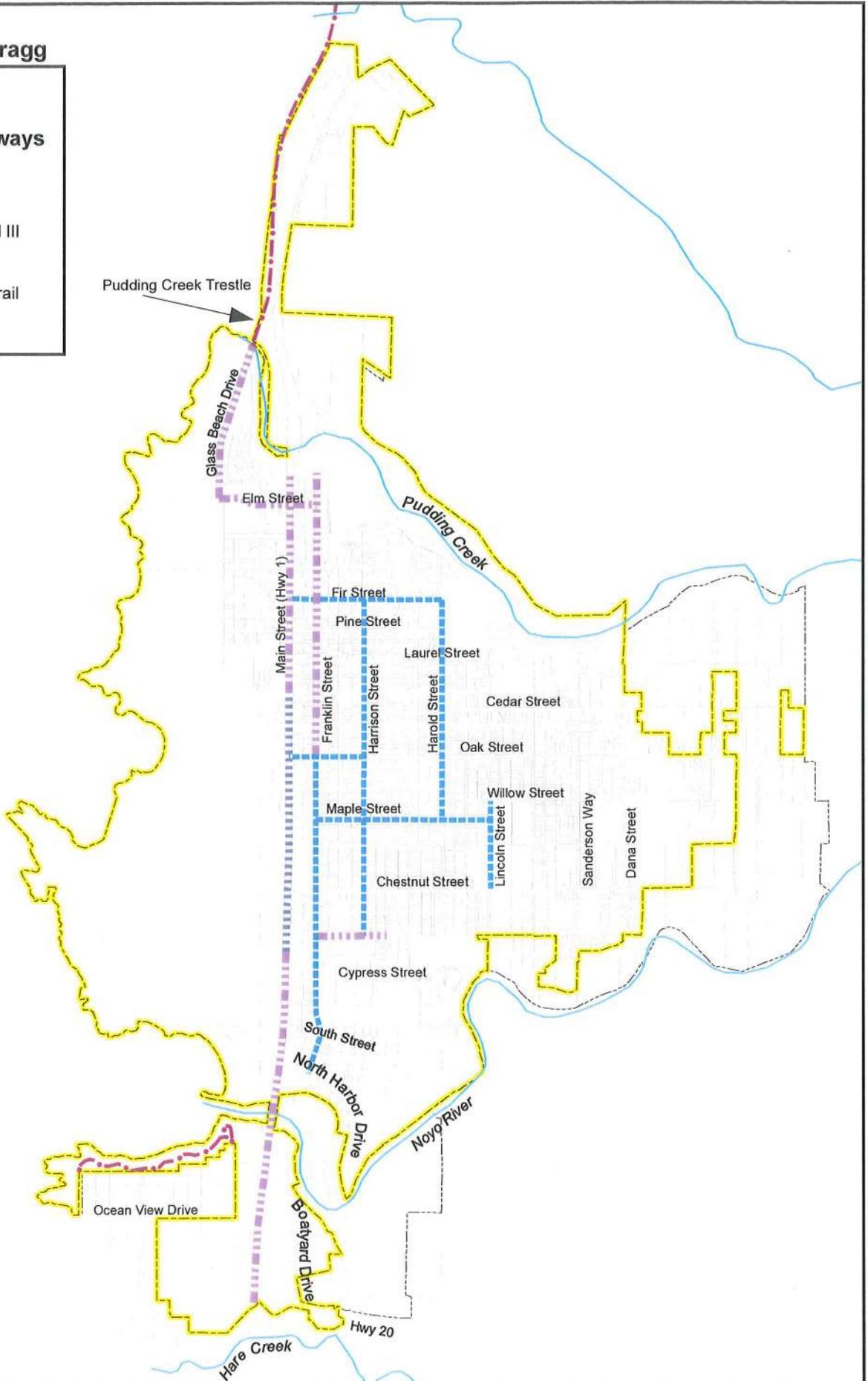
Legend

Existing Bikeways

Class

-  Class II
-  Class II and III
-  Class III
-  Multi Use Trail
-  City Limit

Pudding Creek Trestle



Feature and boundary locations depicted are approximate only.



Map 2: Existing Bikeways, Fort Bragg, 2009



City of Fort Bragg Police Reports
Part 1. All fatal accidents 2000 to 2013

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
CEGAR ST		318 FEET	N SANDERSON WY	02/24/11	1739	THU	Unknown	Other - Public-Fatal	FA1100213
N MAIN ST	X		SPRUCE ST	11/01/08	1931	SAT	21954(A)	Other - Public-Fatal	FA0801348
700 B OAK ST	X		S HAROLD ST	05/18/10	0880	TUE	21950(B)	Other - Public-Fatal	FA1000547
S FRANKLIN ST		191 FEET	OAK ST	08/14/09	1733	FRI		Other - Public-Fatal	FA0900944
SR 1 HY		137 FEET	OCEAN VIEW DR	05/12/12	2349	SAT	21954(A)	Public-H/R - Public-Fatal	FA1200640
SRI		767 FEET	NORTH HARBOR	11/28/02	0214	THU	23153	DUI-Public-Fatal	FA0201232
Record Count									5

City of Fort Bragg Police Reports

Part 2. All fatal accidents involving pedestrians 2000 to 2013

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
N MAIN ST	X		SPRUCE ST	11/01/08	1931	SAT	21954(A)	Other - Public-Fatal	FA0801348
700 B OAK ST	X		S HAROLD ST	05/18/10	0800	TUE	21950(B)	Other - Public-Fatal	FA1000547
SR 1 HY	137	FEET SOUTH	OCEAN VIEW DR	09/12/12	2349	SAT	21954(A)	Public-H/R-Public-Fatal	FA1200540
Record Count									3

City of Fort Bragg Police Reports
Part 3. All injury accidents 2000 to 2013

Traffic Accident Report Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
A&W HAVIL RD		57 FEET	WEST	CYPRESS ST	11/09/06	0220	THU	Fall Asleep	FA0601589
AIRPORT RD		100 FEET	EAST	SR 1	12/12/10	0900	SUN	21954(A)	FA1001390
ALDER ST		200 FEET	EAST	FRANKLIN	10/03/09	1655	SAT	23152(A)	FA0901148
ALDER ST		51.1 FEET	WEST	MCKINLEY ST	06/25/03	1530	WED	21650.1	FA0300747
200 B ALLEY EAST SR 1 SR		129 FEET	SOUTH	E REDWOOD AVENUE	08/16/09	1944	SUN	22107	FA0900852
ALLEY W HARRISON		135 FEET	SOUTH	REDWOOD	09/27/01	1310	THU	Unknown	FA0101093
BOATYARD DR		20 FEET	NORTH	HWY 20	10/28/01	1016	SUN	21804(A)	FA0101207
CHESTNUT		171 FEET	EAST	SR 1	05/13/02	1439	FRI	21804(A)	FA0200459
CHESTNUT		89 FEET	EAST	WALL	07/12/02	0745	WED	22107	FA0200698
CHESTNUT		4 FEET	WEST	WHIPPLE	10/21/04	1921	THU	Unknown	FA0401079
CHESTNUT		8 FEET	WEST	SANDERSON	10/20/04	1315	SAT	22350	FA0401125
CHESTNUT		24 FEET	EAST	MINNESOTA AV	05/16/06	1438	TUE	22350	FA0500088
CHESTNUT		5 FEET	WEST	SUSIE CT	09/21/06	1840	THU	21804(A)	FA0501333
CHESTNUT		38 FEET	WEST	SANDERSON	03/06/07	1812	TUE	Oh Than Driver	FA0700284
CHESTNUT		130 FEET	WEST	S HARRISON ST	03/15/07	1910	THU	21802(A)	FA0700325
CHESTNUT		60 FEET	WEST	S MCPHERSON ST	10/18/10	1559	MON	23103	FA0901226
CHESTNUT		206 FEET	EAST	SR 1	12/04/10	2231	SAT	22350	FA1001172
CHESTNUT		24.8 FEET	EAST	S LINCOLN ST	07/11/13	0844	THU	22350	FA1001349
200 B CHESTNUT ST				S MCPHERSON ST	06/01/09	1300	MON		FA0900654
900 B CHESTNUT ST		150 FEET	WEST	MINNESOTA	03/12/01	1530	MON	22350	FA0100319
CORRY		104 FEET	NORTH	CHESTNUT	09/29/03	1821	TUE	21712B	FA0301210
CYPRESS ST		177 FEET	SOUTH	RIVER DR	09/03/09	1534	THU	22107	FA0901022
DANA ST		80 FEET	NORTH	CHESTNUT	03/19/02	0730	TUE	22350	FA0200277
DANA		34 FEET	SOUTH	TAUBOLD	04/01/02	2341	MON	23152(A)	FA0200331
DANA		189 FEET	SOUTH	OAK ST	10/29/03	1220	WED	26453(A)	FA0301326
100 B DANA ST		241 FEET	SOUTH	OAK	08/23/01	0750	THU		FA0100965
300 B DANA ST		531 FEET	SOUTH	TAUBOLD CT	01/09/02	1530	WED	Oh Improper Driving	FA0200035
DUBOIS		112 FEET	SOUTH	CHESTNUT	10/19/03	1327	SUN	Oh Improper Driving	FA0301282
E LAUREL ST				MORROW ST	10/15/09	1553	THU	21950(C)	FA0901191
E LAUREL ST		74 FEET	WEST	N FRANKLIN ST	04/19/14	1437	SAT	Oh Improper Driving	FA1400475
100 B E OAK ST		172 FEET	EAST	SR-1	03/29/01	1658	THU	21202(A)	FA0100393
E PINE ST		33 FEET	EAST	SR 1	06/26/05	1334	SUN	21954	FA0500712
E REDWOOD AV				HARRISON	12/18/02	0952	TUE		FA0201275
E REDWOOD AV		65 FEET	WEST	N HARRISON ST	07/31/08	1839	THU	21650	FA0800963
E REDWOOD AV		115 FEET	WEST	N HARRISON ST	06/05/10	1520	SAT	Oh Than Driver	FA1000616
300 B E REDWOOD AV		52 FEET	NORTH	REDWOOD	08/18/01	0200	SAT	22350	FA0100951
ELM		140 FEET	EAST	STEWART ST	10/14/01	1228	SUN	Oh Than Driver	FA0101155
FRANKLIN ST				MADROGNE	01/07/01	2029	SUN	22450	FA0100027
FRANKLIN ST				LAUREL	02/17/01	1627	SAT	21802(A)	FA0100221
FRANKLIN		234 FEET	SOUTH	WALNUT	06/08/01	0919	FRI	21804(A)	FA0100673
FRANKLIN				HAZEL	04/24/01	1508	SUN	21802(A)	FA0100729
FRANKLIN		208 FEET	SOUTH	WALNUT	04/18/02	1030	THU	21802(A)	FA0200379
FRANKLIN		255 FEET	SOUTH	WALNUT	05/01/02	1107	WED	22350	FA0200420
FRANKLIN		40 FEET	SOUTH	OAK	08/26/02	1340	MON		FA0200868
FRANKLIN				MAPLE	09/16/02	1547	MON	22350	FA0200946
					10/23/02	1308	FRI	21804(A)	FA0201198

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
FRANKLIN	X		FIR	12/10/02	0835	TUE	22450	Other - Public-Injury	FA0201274
FRANKLIN	X	57 FEET	ALDER	02/04/03	1900	TUE	22517	Other - Public-Injury	FA0300157
FRANKLIN	X	39 FEET	WALNUT ST	03/03/03	1055	MON	22517	Other - Public-Injury	FA0300279
FRANKLIN	X		HAZEL ST	08/09/03	1805	SAT		Other - Public-Injury	FA0300961
FRANKLIN	X	200 FEET	MADRONE	12/29/03	1340	MON	21802(A)	Other - Public-Injury	FA0301562
FRANKLIN	X		CYPRESS ST	01/21/04	1159	WED	21804(A)	Other - Public-Injury	FA0400084
FRANKLIN	X		CHESTNUT	09/28/04	2105	MON	21803(A)	Public-H/R - Public-Injury	FA0400983
FRANKLIN	X		MAPLE	10/07/04	1436	THU	22450(A)	Other - Public-Injury	FA0401039
FRANKLIN	X		BUSH	12/18/04	1535	SAT	21802(A)	Other - Public-Injury	FA0401290
FRANKLIN	X	104 FEET	WALNUT ST	02/03/05	2102	SAT	23152(A)	DUI - Public-Injury	FA0500156
FRANKLIN	X		MADRONE ST	06/04/05	1500	SAT		Other - Public-Injury	FA0500623
FRANKLIN	X		WALNUT	06/22/05	1243	WED	21802(A)	Other - Public-Injury	FA0500697
FRANKLIN	X		WALNUT	08/19/05	0654	FRI	21950A	City Vehicle-Injury	FA0500994
FRANKLIN	X		CHESTNUT	09/15/05	1526	THU	21802(A)	Other - Public-Injury	FA0501142
FRANKLIN	X		MAPLE	11/22/05	1430	TUE	21801(B)	Other - Public-Injury	FA0501478
FRANKLIN	X		FIR ST	12/12/05	1610	MON	22350	Other - Public-Injury	FA0501604
FRANKLIN	X	100 FEET	OAK ST	03/11/06	1430	SAT	22450(A)	Other - Public-Injury	FA0600384
FRANKLIN	X	216 FEET	HAZEL ST	05/03/06	2300	FRI	21208 A	Other - Public-Injury	FA0600636
FRANKLIN	X		WALNUT ST	09/04/06	1115	MON	22450(A)	Other - Public-Injury	FA0600995
FRANKLIN	X		CHESTNUT ST	10/12/06	0756	THU	21803(A)	Public-H/R - Public-Injury	FA0601456
FRANKLIN	X	20 FEET	CYPRESS ST	11/29/06	1640	WED	21803(A)	Other - Public-Injury	FA0601680
FRANKLIN	X	11 FEET	E LAUREL ST	10/15/07	1619	MON	21950(A)	Other - Public-Injury	FA0700125
FRANKLIN	X	6 FEET	WALNUT	11/20/07	1810	TUE	21950(A)	Other - Public-Injury	FA0701374
FRANKLIN	X		WALNUT	02/26/08	0858	TUE	21950(A)	Other - Public-Injury	FA0800257
FRANKLIN	X		CHESTNUT ST	06/06/08	1840	FRI	21802(A)	Other - Public-Injury	FA0800725
FRANKLIN	X		REDWOOD AV	06/22/08	1710	SUN	21650.1	Other - Public-Injury	FA0800791
FRANKLIN	X	64.5 FEET	ALDER ST	01/12/10	1256	TUE	21954(A)	Other - Public-Injury	FA1000044
FRANKLIN	X	16 FEET	OAK ST	11/14/11	1132	MON	21950(A)	Other - Public-Injury	FA1101331
FRANKLIN	X	1166 FEET	STEWART CR	10/13/04	2223	FRI	22350	Public-H/R - Public-Injury	FA0401089
GLASS BEACH		161 FEET	OCEAN VIEW DR	05/14/04	1945	FRI	22106	Other - Public-Injury	FA0400516
HARBOR		32 FEET	WILLOW	06/16/01	1921	SAT	21802(A)	Other - Public-Injury	FA0201048
HAROLD	X	108 FEET	REDWOOD	10/11/02	0808	FRI	22350	Other - Public-Injury	FA0100708
HAROLD	X	26 FEET	OAK ST	01/28/05	0840	TUE		Other - Public-Injury	FA0300121
HAROLD	X		MAPLE ST	07/13/04	2313	TUE	23152(A)	Public-H/R - Public-Injury	FA0400735
HAROLD	X		WILLOW	09/01/04	1739	WED	22350	Other - Public-Injury	FA0400912
HARRISON	X	2 FEET	CEDAR STREET	10/13/08	2219	MON	23152(A)	DUI - Public-Injury	FA0801276
HARRISON	X		FIR	11/01/01	1447	THU	22450	Other - Public-Injury	FA0101228
HARRISON	X		MAPLE	02/24/05	1443	THU	22350	Other - Public-Injury	FA0500212
HARRISON	X		MAPLE ST	03/02/05	1040	WED		Other - Public-Injury	FA0500237
HARRISON	X		OAK ST	05/13/06	2015	SAT	22450(A)	Other - Public-Injury	FA0600670
HARRISON	X		OAK ST	07/07/06	0950	SAT	22450(A)	Other - Public-Injury	FA0600898
HARRISON	X	28 FEET	OAK	08/30/06	1700	WED	21658(A)	Other - Public-Injury	FA0601182
HARRISON	X		REDWOOD	02/03/08	0830	SUN	Unknown	Public-H/R - Public-Injury	FA0600143
HARRISON	X		FIR	12/06/10	1241	MON	22450(A)	Other - Public-Injury	FA1001362
LAUREL		48 FEET	SR 1	09/06/05	1355	SAT	22350	Other - Public-Injury	FA0301103
MADRONE ST		144 FEET	WHIPPLE	01/17/03	2040	FRI	23152(A)	DUI - Public Drivable	FA0300069

Traffic Accident Report

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Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
MAIN ST	X	15	PUDDING CREEK RD	04/03/03	1945	THU		Other - Public-Injury	FA0300410
MAIN ST	X	15	OAK	07/03/03	0711	THU	21950(A)	Other - Public-Injury	FA0300783
MAIN	X	93	MADRONE	09/07/06	2012	THU	21804(A)	Other - Public-Injury	FA0601239
MAIN ST		105	HAZEL	06/22/07	0825	FRI	22107	Other - Public-Injury	FA0700729
MANZANITA		108	E LAUREL ST	07/19/07	1032	THU		Other - Public-Injury	FA0700989
MANZANITA ST		108	SRI	08/03/01	1934	FRI	22350VC		FA0700887
MAPLE ST	X	2	SRI	11/19/04	2242	FRI	23152(AV)	DUI-Public-Injury	FA0401182
MAPLE	X		PARK	03/19/02	1330	TUE	21802(A)	Other - Public-Townway	FA02000278
MAPLE ST	X	3	SRI	08/09/03	1033	SAT	22450(A)	Other - Public-Injury	FA0300983
MAPLE ST	X	3	S MCPHERSON ST	12/07/04	1306	TUE	21950(A)	Other - Public-Injury	FA0401246
MAPLE ST	X	3	S CORRY ST	10/03/11	0755	MON	21802(A)	Other - Public-Injury	FA1101159
MAPLE ST	X	3	S CORRY ST	10/19/12	1657	FRI	21802(A)	Other - Public-Injury	FA1201525
MAPLE ST	X	2	SRI	03/25/13	1740	MON	22450(A)	Other - Public-Injury	FA1300422
MCPHERSON	X		CHESTNUT ST	01/27/03	1790	MON	21801A	Other - Public-Injury	FA0300118
MCPHERSON	X		REDWOOD	03/02/06	1433	THU	22450(A)	Public-H/R - Public-Injury	FA0600340
MCPHERSON ST	X	272	REDWOOD	10/18/01	1248	THU		Other - Public-Injury	FA0101174
MORROW	X		PERN	06/28/02	1715	THU	22350	Other - Public-Injury	FA0200641
N FRANKLIN ST	X	66	E PINE ST	11/15/08	1418	SAT		Other - Public-Injury	FA0801420
N FRANKLIN ST	X	66	SPRUCE ST	07/26/09	1930	SUN	21663	Other - Public-Injury	FA0900869
N FRANKLIN ST	X	130	E ELM ST	06/27/10	1130	SUN	21801(A)	Other - Public-Injury	FA1000704
N FRANKLIN ST	X	141	E REDWOOD AV	08/17/10	2308	TUE	21954(A)	Other - Public-Injury	FA1000928
N FRANKLIN ST	X	141	E FIR ST	12/04/10	1737	SAT	22450(A)	Other - Public-Injury	FA1001348
N FRANKLIN ST	X	141	E ELM ST	02/16/12	1743	THU	22107	Other - Public-Injury	FA1200242
N FRANKLIN ST	X	3	E FIR ST	05/30/12	0738	WED	21751	Other - Public-Injury	FA1300201
N FRANKLIN ST	X	3	E BUSH ST	04/18/13	0753	THU	22450(A)	Other - Public-Injury	FA1300550
N FRANKLIN ST	X	3	MAPLE ST	07/19/13	0914	FRI	21804(A)	Other - Public-Injury	FA1301042
N FRANKLIN ST	X	3	E REDWOOD AV	01/24/14	2117	FRI	22350	Public-H/R - Public-Injury	FA1400114
N FRANKLIN ST	X	3	E REDWOOD AV	01/02/04	1604	FRI	21950(A)	Other - Public-Injury	FA0400006
N FRANKLIN ST	X	3	E REDWOOD AV	02/02/13	0941	SAT	Oh Improper Driving	Other - Public-Injury	FA1300189
N HARBOR DR		3	SRI	12/29/04	0647	THU		Other - Public-Injury	FA0401330
N HARBOR DR		69	TELEPHONE POLE #643	04/12/01	1525	THU	23613(B)	DUI-Public-Injury	EA0100458
N HAROLD		93	OAK	08/06/04	1536	FRI	22106	Other - Public-Injury	FA0400818
N HAROLD ST		32	E LAUREL ST	05/20/10	0633	THU	21954(A)	Other - Public-Injury	FA1000556
N HAROLD ST		69	ALDER	01/19/01	1215	FRI	Unknown	Other - Public-Injury	FA0100082
N HAROLD ST		55	FIR	05/28/09	1238	THU	Oh Improper Driving	Other - Public-Injury	FA0900623
N MAIN ST		274	MPM 63.03 SRI	05/17/01	2000	SUN	Oh Than Driver	Other - Public-Injury	EA0100642
N MAIN ST	X	99	E REDWOOD AV	01/22/10	1419	FRI	21804(A)	Other - Public-Injury	FA1000083
N MAIN ST	X	117	ALDER ST	02/20/10	1456	SAT		Other - Public-Injury	FA10000207
N MAIN ST	X	144	CHESTNUT	01/05/12	1330	THU	21453(B)	Other - Public-Injury	FA1200020
N MAIN ST		12	LAUREL	10/03/03	1148	FRI		Other - Public-Injury	FA03011221
N MAIN ST		189	ELM	12/23/09	1335	MON	21703	Other - Private-Injury	FA0300256
N MAIN ST		629	MAPLE POST MARKER 62.74	09/23/03	1430	THU		Other - Private-Injury	FA0001477
N MAIN ST		41.4	WHIPPLE ST	08/12/01	1721	WED	22350	Other - Public-Injury	FA0100964
N WHIPPLE ST	X	41.4	E LAUREL ST	02/18/02	1247	MON	21703	Other - Public-Injury	FA0200178
N WHIPPLE ST	X	41.4	E LAUREL ST	06/23/10	1315	WED	22106	Other - Public-Injury	FA1000688
N WHIPPLE ST	X	41.4	E LAUREL ST	09/15/10	1145	WED	22450(A)	Other - Public-Injury	FA1001056

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Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
NORTH HARBOR	STATION POINT	39 FEET WEST	MORROW	06/22/06	1930	THU	22850	Other - Public-Injury	FA0600861
OAK		70 FEET EAST	FRANKLIN	01/24/01	1430	WED	21201(C)	Other - Public-Injury	FA0100102
OAK		130 FEET WEST	FRANKLIN	02/13/01	1342	TUE	21950(B)	Other - Public-Injury	FA0100204
OAK		8 FEET EAST	FRANKLIN	05/02/01	1535	WED	21950(B)	Other - Public-Injury	FA0100536
OAK		8 FEET WEST	HAROLD	05/11/01	1525	FRI	21950(B)	Other - Public-Injury	FA0100581
OAK ST			HARRISON	05/14/01	1907	MON	22450(A)	Other - Public-Injury	FA0100582
OAK			HARRISON	04/19/02	0851	FRI	22850 CVC	Other - Public-Injury	FA0200384
OAK		144 FEET EAST	MORROW	07/19/02	2200	FRI	Unknown	Other - Public-Injury	FA0200723
OAK		3 FEET EAST	MCPHERSON	09/19/02	0730	THU	21950(A)	Other - Public-Injury	FA0200963
OAK ST			HARRISON	10/31/02	1500	THU	21804(A)	Other - Public-Injury	FA0201131
OAK			SR 1	07/21/03	0822	MON		Other - Public-Injury	FA0300875
OAK		46 FEET WEST	HOCKER	08/28/03	2005	THU	21801(B)	Public-H/R, Public-Injury	FA0301062
OAK			MCKINLEY	11/01/04	1040	MON	22350	Other - Public-Injury	FA0401128
OAK ST		8 FEET WEST	FRANKLIN	12/16/04	1912	THU	23153(A)	DUI - Public-Injury	FA0401287
B OAK		141 FEET WEST	HAROLD	10/14/05	1600	FRI	22517	Other - Public-Injury	FA0600996
OAK			N HAROLD ST	07/21/06	1030	FRI	22350	Other - Public-Injury	FA0601094
OAK			CORRY	08/11/06	1749	FRI	21200.5	DUI - Public-Injury	FA0701083
OAK ST			S HARRISON ST	09/10/07	1400	MON	22350	Other - Public-Injury	FA0801543
OAK ST			N MCPHERSON ST	10/01/09	0900	THU		Other - Public-Injury	FA0901140
OAK ST		28 FEET EAST	S HAROLD ST	05/01/10	1509	SAT	Ohn Than Driver	Other - Public-Injury	FA1000479
OAK ST		47.7 FEET EAST	N FRANKLIN ST	10/01/10	0800	THU		Other - Public-Injury	FA1001045
OAK ST			JEWETT ST	09/12/10	3030	SUN	21863	Public-H/R, Public-Injury	FA1001406
OAK ST			S FRANKLIN ST	12/15/10	2052	WED	22450(A)	Other - Public-Injury	FA1100940
OAK ST			N HARRISON ST	08/14/11	1251	SUN	22450(A)	Other - Public-Injury	FA1101430
OAK ST			HARRISON STREET	12/13/11	1207	TUE	21801(A)	Other - Public-Injury	FA1200307
OAK ST			N HAROLD ST	02/28/12	1500	TUE	22450(A)	Other - Public-Injury	FA1200984
OAK ST		12 FEET WEST	HAROLD STREET	07/20/12	1849	FRI	21804(A)	Other - Public-Injury	FA0200896
OAK ST		152 FEET WEST	FRANKLIN	09/01/02	1920	SUN	21950(A)	Other - Public-Injury	FA0200886
200 B OAK ST		6 FEET EAST	S HAROLD ST	05/18/10	0800	TUE	21950(B)	Other - Public-Injury	FA1000547
700 B OAK ST			MCPHERSON	08/06/02	1301	TUE	21802(A)	Other - Public-Injury	FA0200781
PINE			N FRANKLIN ST	01/19/05	0853	WED	21800(A)	Other - Public-Injury	FA0500080
PINE ST			SR 1	02/21/05	2040	MON	21950(A)	Other - Public-Injury	FA0500207
PINE		6 FEET EAST	MCPHERSON	08/07/12	1545	TUE	21802A	Other - Public-Injury	FA1201102
PINE ST			334 N MCPHERSON	12/17/00	1900	SUN	Unknown	Other - Public-Injury	FA0010450
PRIVATE PROPERTY			863 N MAIN	01/27/01	1930	SAT	Ohn Improper Driving	Other - Public-Injury	FA0100115
PRIVATE PROPERTY			120 W FIR STREET	02/27/01	1143	TUE	Unknown	Other - Public-Injury	FA0100257
PRIVATE PROPERTY			189 BOATYARD	07/26/01	1115	THU	Ohn Improper Driving	Other - Public-Injury	FA0100853
PRIVATE PROPERTY			930 STEWART	10/15/01	0080	MON	Ohn Improper Driving	Other - Public-Injury	FA0101161
PRIVATE PROPERTY			CHESTNUT ST	06/03/03	1545	TUE	Ohn Improper Driving	Other - Private-Injury	FA0300664
PRIVATE PROPERTY			660 S MAIN	07/22/03	1630	TUE	Ohn Improper Driving	Other - Private-Injury	FA0300884
PRIVATE PROPERTY			171 BOATYARD	01/24/04	1545	SAT	Ohn Improper Driving	Other - Private-Injury	FA0400098
PRIVATE PROPERTY			330 N MAIN STREET	06/25/04	1620	FRI	Unknown	Other - Private-Injury	FA0400865
PRIVATE PROPERTY			125 S MAIN STREET	09/17/04	1456	FRI	Ohn Improper Driving	Other - Private-Injury	FA0400970
PRIVATE PROPERTY			124 S MAIN	11/28/04	1900	SUN	Ohn Improper Driving	Other - Private-Injury	FA0401211
PRIVATE PROPERTY			490 S MAIN	08/01/05	1329	MON	Unknown	Other - Private-Injury	FA0500899
PRIVATE PROPERTY			700 RIVER DRIVE	01/06/06	0830	FRI	Ohn Improper Driving	Other - Private-Injury	FA0600032
PRIVATE PROPERTY			171 BOATYARD DR	08/01/07	1332	WED	22350	Other - Private-Injury	FA0700923
PRIVATE PROPERTY			353 S LINCOLN STREET	09/03/09	0805	THU	Ohn Improper Driving	City Vehicle Injury	FA0901019

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PRIVATE PROPERTY			CHESTNUT ST	07/25/10	1815	SUN	23103(9)	Private-H/R-Injury	FA1009822
PRIVATE PROPERTY			171 BOATYARD DRIVE	12/11/10	1435	SAT	0th Improper Driving	Other - Private-Injury	FA1001388
PRIVATE PROPERTY			171 BOATYARD DRIVE	07/18/11	1303	MON	0th Improper Driving	Other - Private-Injury	FA1100729
PRIVATE PROPERTY			100 BLOCK S MAIN	09/07/12	1345	MON	0th Improper Driving	Other - Private-Injury	FA1200612
PRIVATE PROPERTY			740 S FRANKLIN STREET	08/06/12	1003	MON	0th Improper Driving	Other - Private-Injury	FA1201096
PRIVATE PROPERTY			1021 S MAIN STREET	08/18/12	1215	SAT	0th Improper Driving	Other - Private-Injury	FA1201718
PRIVATE PROPERTY			418 N MAIN STREET	11/30/12	1159	FRI	0th Improper Driving	Other - Private-Injury	FA1201706
PRIVATE PROPERTY			171 BOATYARD DR	10/12/13	1028	SAT	0th Improper Driving	Other - Private-Injury	FA1301531
PRIVATE PROPERTY			130 S MAIN STREET	05/01/14	1038	THU	0th Improper Driving	Private-H/R-Injury	FA1400335
PUDDING CREEK			SR1	10/05/01	2317	FRI	23152	DUI-Public-Injury	FA0101125
PUDDING CREEK			SR 1	06/05/03	0836	THU	21650	DUI-Public-Injury	FA0300653
PUDDING CREEK RD			SR 1	12/08/09	1000	TUE	22107	Other - Public-Injury	FA0901383
REDWOOD			CORRY	10/17/01	1133	WED	22450	Other - Public-Injury	FA0101169
REDWOOD AV			FRANKLIN	12/24/02	0041	TUE	23152	Public-H/R-Public - Towaway	FA0201317
REDWOOD			SR 1	12/24/02	1407	TUE	22850	Other - Public-Injury	FA0201318
REDWOOD			MCPHERSON	06/24/03	1758	TUE	22350	Other - Public-Injury	FA0300740
REDWOOD AV			WHIPPLE	06/13/04	0100	SUN	Unknown	Other - Public-Injury	FA0400827
REDWOOD			MCPHERSON	06/08/06	0200	THU	23153(9)	DUI-Public-Injury	FA0600796
200 B REDWOOD AV			FRANKLIN	08/25/01	1030	SAT	21804(A)	Other - Public-Injury	FA0100973
RIVER DR			CYPRESS ST	12/29/13	0041	SUN	22350	Other - Public-Injury	FA1301804
S FRANKLIN ST			WALNUT ST	11/14/03	1126	FRI	21950(A)	Public-H/R-Public-Injury	FA0301387
S FRANKLIN ST			OAK ST	08/14/09	1733	FRI		Other - Public-Fatal	FA0900944
S FRANKLIN ST			OAK ST	09/07/10	1330	TUE	21950(A)	Public-H/R-Public-Injury	FA1001023
S FRANKLIN ST			CYPRESS ST	02/24/11	1836	THU	22450(A)	Other - Public-Injury	FA1100215
S FRANKLIN ST			CYPRESS ST	02/29/11	1604	FRI	22350	Other - Public-Injury	FA1100220
S FRANKLIN ST			CYPRESS ST	02/08/12	0846	WED	20150(A)	Other - Public-Injury	FA1200193
S FRANKLIN ST			WALNUT STREET	13/02/13	1129	MON	21801(A)	Public-H/R-Public-Injury	FA1301775
S FRANKLIN ST			WALNUT ST	01/05/14	1916	SUN	21950(A)	Public-H/R-Public-Injury	FA1400020
400 B S FRANKLIN ST			HAZEL ST	10/26/13	0936	SAT	21650.1	Other - Public-Injury	FA0400431
800 B S FRANKLIN ST			OAK ST	12/19/03	1615	FRI		Other - Public-Injury	FA1301589
S HARRISON ST			OCEAN VIEW DR	07/08/09	0840	WED	22350	Other - Public-Injury	FA0900775
S MAIN ST			OAK ST	08/19/09	1231	WED	21804(A)	Other - Public-Injury	FA0900959
S MAIN ST			CYPRESS ST	10/06/09	1846	TUE	23152(A)	Other - Public-Injury	FA0901159
S MAIN ST			CYPRESS ST	06/03/10	1730	THU	21453(A)	Other - Public-Injury	FA1000610
S MAIN ST			CHESTNUT ST	06/17/10	2110	THU	21453(A)	Other - Public-Injury	FA1000670
S MAIN ST			OAK ST	09/28/10	2015	THU	22100(A)	Public-H/R-Public-Injury	FA1001408
S MAIN ST			CHESTNUT ST	02/24/11	1647	THU	21453(A)	Other - Public-Injury	FA1100209
S MAIN ST			MEM 60.00	04/30/11	2118	SAT	21954	Other - Public-Injury	FA1100448
S MAIN ST			CHESTNUT ST	11/26/11	1738	SAT	21801(A)	Other - Public-Injury	FA1101370
S MAIN ST			BOATYARD DR	02/09/13	1114	SAT	21453(A)	Other - Public-Injury	FA1300205
100 B S MAIN ST			MADRONE ST	12/06/08	1508	SAT	22107	Other - Public-Injury	FA0801485
300 B S MAIN ST			E REDWOOD AV	02/06/03	1456	THU	21801(A)	Other - Public-Injury	FA0300172
500 B S MAIN ST			CHESTNUT	02/28/01	1150	WED	22350	Other - Public-Injury	FA0100259
660 S MAIN ST			WALNUT ST	05/30/10	1730	SUN	0th Improper Driving	Other - Private-Injury	FA1000586
700 B S MAIN ST			CYPRESS ST	08/03/09	0917	WED	21804(A)	Other - Public-Injury	FA0900903

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700 B S MAIN ST	221	FEET SOUTH	CYPRESS ST	10/05/12	1644	FRI	21453(A)	Other - Public Injury	FA1201449
1100 B S MAIN ST	X	FEET SOUTH	OCEAN VIEW DR	07/30/09	2100	THU	21453(A)	Other - Public Injury	FA0900886
SANDERSON	X	FEET NORTH	OAK	02/05/01	1202	MON	22450	Other - Public Injury	FA0100174
SRUCE	X	FEET WEST	CHESTNUT	08/13/04	1715	FRI	21804(A)	Other - Public Injury	FA0501442
SR I	X	FEET WEST	SR I	12/22/00	1309	FRI	22350	Other - Public-Towaway	FA0400842
SR I SR	X	FEET WEST	CYPRESS	04/30/01	1615	MON	Unknown	Other - Public-Towaway	FA0100523
SR I	X	FEET SOUTH	SR20	07/06/01	1345	FRI	22350	Other - Public-Towaway	FA0300779
SR I	168	FEET SOUTH	WALNUT	12/20/01	1349	THU	21855(A)	Other - Public-Towaway	FA0101389
SR I	1/10	FEET NORTH	PUDDING CREEK ROAD	06/12/02	1836	WED	Oh Than Driver	DUI-Public-Injury	FA0200593
SR I	57	FEET SOUTH	BUSH	06/30/02	1653	SUN	23152	DUI-Public-Injury	FA0200646
SR I	X	FEET SOUTH	ALDER	08/08/02	1246	THU	21802(A)	Other - Public-Injury	FA0200784
SR I HY	160	FEET NORTH	REDWOOD	08/09/02	1600	FRI	906E	Other - Public-Injury	FA0200793
SR I SR	1390	FEET SOUTH	MEN 60.50	08/21/02	1205	SAT	22350	Other - Public-Injury	FA0200892
SR I HY	X	FEET SOUTH	LAUREL	09/16/02	1445	WED	23109(C)	Other - Public-Injury	FA0201070
SR I HY	700	FEET NORTH	OCEANVIEW	11/08/02	1420	FRI	22350	DUI-Public-Injury	FA0201167
SR I SR	287	FEET SOUTH	OCEAN VIEW DR	11/29/02	1258	FRI	22350	Other - Public-Injury	FA0201235
SR I	X	FEET SOUTH	OAK	12/16/02	1100	MON		Other - Public-Injury	FA0201266
SR I SR	X	FEET SOUTH	CHESTNUT	05/02/03	1342	FRI	26453A	Other - Public-Injury	FA0300519
SR I	15	FEET SOUTH	CHESTNUT	05/22/03	1825	TUE	22350	Other - Public-Injury	FA0300573
SR I HY	X	FEET NORTH	OCEAN VIEW DR	06/04/03	1545	WED	22350	Other - Public-Injury	FA0300648
SR I SR	161	FEET NORTH	CHESTNUT ST	07/18/03	2230	FRI	21453(C)	Other - Public-Injury	FA0300867
SR I SR	48	FEET NORTH	OAK	07/19/03	1656	SAT	22350	Other - Public-Injury	FA0300899
SR I HY	X	FEET SOUTH	OCEAN VIEW DR	11/22/03	1838	WED	21453A	Other - Public-Injury	FA0301393
SR I	3	FEET SOUTH	LAUREL	12/03/03	1830	FRI	21950(A)	Other - Public-Injury	FA0301483
SR I	171	FEET SOUTH	OAK ST	12/18/03	1215	THU	21801(A)	Other - Public-Injury	FA0301531
SR I	29	FEET SOUTH	REDWOOD	03/29/04	1213	MON	23163(A)	Public-H/R-Public-Injury	FA0400356
SR I SR	72	FEET NORTH	SPRUCE ST	08/04/04	1633	FRI	22350	Other - Public-Injury	FA0400819
SR I	X	FEET SOUTH	PINE	09/04/04	1430	SAT	21755	Other - Public-Injury	FA0400921
SR I	50	FEET SOUTH	OAK ST	09/16/04	1030	THU	Unknown	Other - Public-Injury	FA0400976
SR I	25	FEET NORTH	CHESTNUT ST	09/22/04	1844	WED	21658(A)	Other - Public-Injury	FA0400991
SR I	300	FEET NORTH	REDWOOD	11/03/04	1530	WED	22517	Other - Public-Injury	FA0401134
SR I	35	FEET NORTH	BOATYARD DR	12/17/04	1330	FRI	22350	Other - Public-Injury	FA0401284
SR I SR	X	FEET NORTH	ALDER	01/11/05	1530	TUE	21801(A)	Other - Public-Injury	FA0500044
SR I	X	FEET SOUTH	ALDER ST	05/18/05	0759	TUE	21804(A)	Other - Public-Injury	FA0500491
SR I	99	FEET SOUTH	CYPRESS ST	01/29/05	1215	SUN	22350	Other - Private-Injury	FA0500128
SR I SR	X	FEET SOUTH	FIR	05/29/05	1355	SUN	22360	Other - Public-Injury	FA0500292
SR I	99	FEET SOUTH	PUDDING CREEK RD	06/09/05	0801	THU	21802(A)	Other - Public-Injury	FA0500840
SR I	99	FEET SOUTH	OAK	06/23/05	1456	SAT	22350	Other - Public-Injury	FA0500708
SR I	20	FEET NORTH	SR 20	08/12/05	1105	FRI	22350	Other - Public-Injury	FA0500950
SR I	78	FEET SOUTH	SR 20	08/15/05	2153	MON	22350	Other - Public-Injury	FA0500971
SR I	81	FEET SOUTH	CYPRESS	08/19/05	0855	FRI	21804(A)	Other - Public-Injury	FA0500995
SR I	X	FEET SOUTH	PINE	10/18/05	1350	TUE	21950(A)	Other - Public-Injury	FA0501308
SR I	X	FEET SOUTH	OAK	10/21/05	0930	FRI	21453(A)	Other - Public-Injury	FA0501327
SR I	X	FEET SOUTH	BOATYARD DR	11/08/05	1300	TUE	22350	Other - Public-Injury	FA0501415
SR I	87	FEET SOUTH	BUSH	12/09/05	1330	FRI	22350	Other - Public-Injury	FA0501577
SR I	6	FEET NORTH	AIRPORT RD	12/21/05	0833	WED	21202(A)	Other - Public-Injury	FA0501662

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
SR 1	X		SPRUCE ST	02/13/06	1350	MON	21950(A)	Other - Public-Injury	FA0602043
SR 1	X		AIRPORT RD	07/15/06	0346	SAT	22350 CVC	Other - Public-Injury	FA0600968
SR 1	X	21	ELM	07/29/06	1225	SAT	22350	Other - Public-Injury	FA0601027
SR 1	X		SR 20	08/14/06	1340	MON	Unknown	Other - Public-Injury	FA0601108
SR 1	X	35	ELM	09/25/06	1112	SAT		Other - Public-Injury	FA0601347
SR 1	X	835	AIRPORT RD	10/04/06	1544	WED	Oh Improper Driving	Other - Public-Injury	FA0601426
SR 1	X	229	MILE MARKER 62.14	12/27/06	1248	WED	22350	Other - Public-Injury	FA0601821
SR 1	X	21	TRAFFIC LIGHT POLE #5908	01/06/07	1240	SAT	22350	Other - Private-Injury	FA0700029
SR 1	X		BOATYARD	01/20/07	1725	SAT	22350	Other - Public-Injury	FA0700093
SR 1	X	280	AIRPORT	03/20/07	1419	FRI	22350	Other - Public-Injury	FA0700379
SR 1	X	15	PINE	05/20/07	1712	SUN	22350	Other - Public-Injury	FA0700586
SR 1	X		ALDER ST	05/25/07	1310	FRI	21804(A)	Other - Public-Injury	FA0700611
SR 1 SR	X	120	OCEAN VIEW DR	07/12/07	1530	THU	22350	Other - Public-Injury	FA0700843
SR 1 SR	X	168	CHESTNUT ST	08/24/07	0000	FRI	21804(A)	Other - Public-Injury	FA0701008
SR 1	X	720	AIRPORT RD	09/18/07	0841	TUE	26453(A)	Other - Public-Injury	FA0701110
SR 1	X	366	CYPRESS ST	09/24/07	1219	MON	21804(A)	Other - Public-Injury	FA0701126
SR 1 SR	X	22	ELM	11/19/07	0851	MON	23152(A)	DUI - Public-Injury	FA0701366
SR 1 SR	X		W PINE ST	01/28/08	1405	MON		Other - Public-Injury	FA0800117
SR 1 SR	X		OCEAN VIEW DR	01/28/08	1516	MON	22350	Other - Public-Injury	FA0800118
SR 1 SR	X	243	MPM 60.23	03/14/08	0730	FRI	22107	Other - Public-Injury	FA0800335
SR 1	X	5	AIRPORT RD	04/27/08	1238	SUN	22107	Other - Public-Injury	FA0800543
SR 1 SR	X	17.6	CHESTNUT ST	05/03/08	0201	SAT	23152(A)	DUI - Public-Injury	FA0800568
SR 1 SR	X	39	BOATYARD DR	07/08/08	1115	TUE	23152(A)	DUI - Public-Injury	FA0800852
SR 1	X	30	OAK ST	08/16/08	2305	SAT	22350	Other - Public-Injury	FA0801022
SR 1 SR	X		BOATYARD DR	08/24/08	2040	SUN	23153(A)	Private-H/R-Injury	FA0801035
SR 1 SR	X	450	OCEAN VIEW DR	09/27/08	1835	SAT	22107	Public-H/R - Public-Injury	FA0801218
SR 1	X		SR 30	11/04/08	1826	TUE	21804(A)	Other - Public-Injury	FA0801361
SR 1 HY	X		CHESTNUT ST	11/20/08	1840	THU		Other - Public-Injury	FA0801433
SR 1 SR	X	25	OCEAN VIEW DR	01/06/09	1153	TUE	21804(A)	Other - Public-Injury	FA0900023
SR 1 SR	X	30	CHESTNUT ST	01/30/09	2025	FRI	23152(A)	Other - Public-Injury	FA0900132
SR 1 SR	X		MADRONE ST	04/13/09	1216	MON	21804(A)	Other - Public-Injury	FA0900442
SR 1 SR	X	356	MPM 63.03	05/10/09	2358	SUN	23152(A)	DUI - Public-Injury	FA0900560
SR 1	X	4	BUSH ST	03/02/10	1657	TUE	21950(A)	Other - Public-Injury	FA1000246
SR 1	X		ALDER ST	04/07/10	1256	WED	22107	Other - Public-Injury	FA1000391
SR 1	X		E REDWOOD AV	04/23/10	1438	FRI	21453(A)	Other - Public-Injury	FA1000442
SR 1 HY	X	189	BUSH ST	08/31/10	1055	TUE	22107	Other - Public-Injury	FA1000987
SR 1 HY	X	159	SOUTH ST	09/07/10	1659	TUE	22350	Other - Public-Injury	FA1001024
SR 1 SR	X	102	SR 20	09/13/10	1913	MON	21451(A)	Other - Public-Injury	FA1001049
SR 1 SR	X		W PINE ST	10/09/10	1201	SAT	21703	Other - Public-Injury	FA1001142
SR 1 SR	X		OAK ST	11/04/10	1418	THU	22350	Other - Public-Injury	FA1001145
SR 1 HY	X		SR 1 SR	12/08/10	1545	WED	22350	Other - Public-Injury	FA1001374
SR 1 SR	X		AIRPORT RD	06/15/11	0857	WED	22107	Other - Public-Injury	FA1100067
SR 1	X	306	CHESTNUT ST	07/02/11	1429	SAT	22350	Other - Public-Injury	FA1100725
SR 1	X	124	E PINE ST	10/30/11	1824	SUN	21453(A)	Other - Public-Injury	FA1101272
SR 1	X	36	BOATYARD DR	11/09/11	2330	WED	23153(B)	Public-H/R - Public-Injury	FA1101313
SR 1 SR	X		PUDDING CREEK RD	12/15/11	1030	THU	21804(A)	Other - Public-Injury	FA1101438
SR 1 SR	X	4	WALNUT ST	01/02/12	1423	MON	21804(A)	Other - Public-Injury	FA1200006
SR 1 SR	X	87	MPM 60.48	02/02/12	1440	THU	22107	Other - Public-Injury	FA1200152

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
SR 1 SR	X	126 FEET	MANZANITA ST	02/27/12	1343	MON	22350	Other - Public-Injury	FA1200303
SR 1 SR	X	206 FEET	SR 20	04/26/12	1530	THU	22107	Other - Public-Injury	FA1200556
SR 1 HY	X		OCEAN VIEW DR	06/06/12	0730	WED	21453(A)	Other - Public-Injury	FA1200749
SR 1 SR	X		CYPRESS ST	06/07/12	2030	WED	21650.1	Other - Public-Injury	FA1200757
SR 1		668 FEET	BOATYARD DR	07/01/12	1700	SUN	21804(A)	Other - Public-Injury	FA1200892
SR 1 SR		105 FEET	SR 20	07/08/12	1540	SUN	21703	Other - Public-Injury	FA1200927
SR 1		48 FEET	SR 20	09/22/12	1514	SAT	22350	Other - Public-Injury	FA1201396
SR 1		43 FEET	MADRONE ST	09/23/12	1447	SUN	22107	Other - Public-Injury	FA1201400
SR 1 SR		41.1 FEET	N HARBOR DR	12/21/12	1617	FRI	22107	Other - Public-Injury	FA1201796
SR 1 SR		214 FEET	EAST CYPRESS	04/09/13	1657	TUE	22107	Other - Public-Injury	FA1300508
SR 1		147 FEET	W PINE ST	06/03/13	1039	MON	21703	Other - Public-Injury	FA1300804
SR 1 HY	X	109 FEET	SR 20	08/28/13	1125	TUE		Public-HR-Public-Injury	FA1301245
SR 1		6 FEET	OAK ST	09/03/13	1545	TUE		Other - Public-Injury	FA1301328
SR 1 HY		177 FEET	OCEAN VIEW DR	10/22/13	1045	TUE	21950(A)	Public-HR-Public-Injury	FA1301572
SR 1 HY		2 FEET	MANZANITA ST	12/20/13	1506	MON	22350	Other - Public-Injury	FA1301917
SR 1 HY		65 FEET	E LAUREL ST	01/03/14	1355	FRI	21804(A)	Other - Public-Injury	FA1400013
SR 1		34 FEET	CHESTNUT ST	01/26/14	0903	SUN	21453(A)	Other - Public-Injury	FA1400120
SR 1 SR		34 FEET	OAK	03/15/14	1726	SAT	22350	Other - Public-Injury	FA1400328
100 B SR 1 SR		27 FEET	OAK ST	05/22/01	1819	TUE	22350	Other - Public-Injury	FA0100626
100 B SR 1 HY		60 FEET	W PINE ST	08/11/03	1630	MON		Other - Public-Injury	FA0300966
500 B SR 1 SR		24 FEET	FIR	11/01/03	1753	SAT	22350	Other - Public-Injury	FA0301343
500 B SR 1		55 FEET	FIR	09/27/01	1255	THU	22350	Other - Public-PDO	FA0101092
700 B SR 1 SR		73 FEET	CYPRESS ST	05/25/02	1537	SAT	22350	Other - Public-Injury	FA0200512
700 SR 1 SR		33 FEET	SOUTH ST	01/21/04	1635	SAT		Other - Public-Injury	FA0400134
1000 B SR 1		129 FEET	OCEANVIEW DRIVE	03/22/04	2033	MON	22350	Other - Public-Drivable	FA0400332
1000 B SR 1 HY		600 FBET	OCEAN VIEW DR	03/07/02	1553	THU	22350 CVC	Other - Public-Injury	FA0200296
1100 SR 1 SR		159 FEET	OCEAN VIEW DR	09/15/09	1538	SUN		Other - Public-Injury	FA0901086
1200 SR 1 HY	X	85 FEET	SR 20	10/23/03	1405	THU	22350	Other - Public-Injury	FA0301294
SR 20		40 FEET	BOATYARD	12/06/08	1700	SAT	21453(A)	Other - Public-Injury	FA0801496
SR 20 SR	X	7.62 MILE	HIGHWAY 1	08/13/04	1630	FRI	22350	Public-HR-Public-Injury	FA0400841
SR 20 HY	X	137 FEET	BOATYARD DR	12/12/00	1555	TUE	22350	Other - Public-Injury	FA0001423
SR 1		100 FEET	OAK	05/11/05	1739	SUN	21804(A)	Other - Public-Injury	FA0500516
SR 1		21 FEET	ALDER	08/20/07	2215	THU	22350	Other - Public-Injury	FA0800071
SR 1	X	165 FEET	ELM	01/17/08	1658	THU	22107	Other - Public-Injury	FA0901408
SR 1		107 FEET	ELM	12/10/09	1719	THU	21804(A)	Other - Public-Injury	FA0101394
SR 1		11 FEET	LAUREL	11/05/01	1650	SAT	22350	Other - Public-Injury	FA0101238
SR 1	X	11 FEET	OCEAN VIEW	11/05/01	1725	MON	21950(B)	Other - Public-Injury	FA0101249
SR 1	X	3 FEET	LAUREL	05/15/02	1536	WED	22350	Other - Public-Injury	FA0200468
SR 1	X	210 FEET	CYPRESS	06/09/02	0200	SUN	23152A8	DUI-Public-Injury	FA0200582
SR 1	X	45 FEET	CHESTNUT	07/05/02	2261	FRI	22350	Other - Public-Injury	FA0200666
SR 1	X	224 FEET	SOUTH	12/04/02	1301	WED	21451(A)	Public-HR-Public-Injury	FA0201081
				03/08/03	1300	SAT	21950(B)	Other - Public-Injury	FA0201251
				03/15/04	1734	MON	21804(A)	Other - Private-Injury	FA03000299
				08/07/04	0917	SAT	23153(A)	DUI-Public-Injury	FA0400822
				08/13/04	1539	FRI	21804(A)	Other - Public-Injury	FA0400840

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
400 B SR-1 ST	X		LAUREL	06/10/01	1805	SUN	22350	Other - Private - Towaway	FA0100885
1100 B SR-1 SR	X	34'	OCEAN VIEW DRIVE	12/14/00	1300	THU		Other - Private-Injury	FA00001436
SR/1	X		OAK	03/03/02	1840	SUN	27360 B	Other - Public-Injury	FA0200222
SR/1	X		SR20	03/10/01	1830	SAT	23152(A)	DUI-Public-Injury	FA0100306
SR/1	X	232	WALNUT	03/10/01	1320	SUN	22350	Other - Private-Injury	FA0100407
SR/1	X	151	CYPRESS	06/10/01	1220	SUN	22102	Other - Public-Injury	FA0100883
SR/1	X	92	HAZEL	09/23/01	2115	SUN	21703	Other - Public-Injury	FA0101063
SR/1	X	10	ALDER	10/17/01	1257	WED	21801A	Other - Public-Injury	FA0101170
SR/1 SR	X	152	SR20	10/20/01	2029	SAT	21950(A)	Other - Public-Injury	FA0101184
SR/1	X	2000	LAUREL	11/07/01	1543	WED	21950B	Other - Public-Injury	FA0101258
SR/1	X	16	OAK	11/22/01	1200	THU	22350	Other - Public-Injury	FA0101283
SR/1 HY	X	18	ELM	12/14/01	1247	FRI	22350	Other - Public-Injury	FA0101308
SR/1	X	160	WALNUT	01/28/02	1825	MON	22350	Other - Public-Injury	FA0200097
SR/1	X	6	REDWOOD	01/29/02	1020	TUE	21650(A)	Other - Public-Injury	FA0200098
SR/1	X	767	NORTH HARBOR	09/08/02	3058	SUN	23152A/B	DUI-Public-Injury	FA0200915
SR/1	X	162	LAUREL	10/08/02	1800	TUE	21950A	Other - Public-Injury	FA0201040
SR/1	X	47	REDWOOD	11/28/02	0214	THU	23153	DUI-Public-Injury	FA0201232
SR/1	X	100	WALNUT	08/23/05	1436	TUE	22350	Other - Public-Injury	FA0501017
SR/1 ST	X		ELM	06/08/06	1724	THU	22350	Other - Public-Injury	FA0600798
SR/1 ST	X		ELM	08/31/07	1330	FRI	22107	Other - Public-Injury	FA0701039
SR/1 ST	X		ELM	10/09/07	2200	TUE	21950(A)	Other - Public-Injury	FA0701193
SR/1 ST	X		HIGHWAY 20	10/09/07	2200	TUE	21950(A)	Other - Public-Injury	FA0701194
SR/1 ST	X		SOUTH ST	05/07/08	0735	WED	21453(A)	Other - Public-Injury	FA0800848
SR/1 ST	X	300	SOUTH ST	07/07/08	1248	MON	21801(A)	Public-Hfr - Public-Injury	FA0800848
600 B SR/1 SR	X	42	HIR	01/26/02	1900	SAT	21801(A)	Other - Public-Injury	FA0200087
700 B SR/1 SR	X	270	CYPRESS	04/23/01	1600	WED	22350	Other - Public-Injury	FA0100501
1111 SR/1 SR	X	261	MEM 62.52 SRI	04/02/01	1530	MON	21801(A)	Other - Public-Injury	FA0100412
900 B STEWART ST	X	38	STEWART	01/29/01	1235	MON	21804(A)	Other - Public-Injury	FA0100123
WALNUT ST	X		S FRANKLIN ST	06/11/13	1558	TUE	22526	DUI-Public-Injury	FA1300846
WHIPPLE	X		PINE	09/03/05	1204	MON	22350	Other - Public-Injury	FA0501089
WILLOW	X	143	EAST WALL ST	03/25/08	1132	TUE	21804(A)	Other - Public-Injury	FA0800390

Record Count 416

City of Fort Bragg Police Reports

Part 4. All injury accidents involving bicycles 2000 to 2013

Traffic Accident Report Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
ALDER ST	FRANKLIN	200 FEET EAST	FRANKLIN	06/25/03	1330	WED	21650.1	Other - Public-Injury	FA03070747
ALDER ST	FRANKLIN	51.1 FEET WEST	MCKINLEY ST	08/16/09	1944	SUN	22107	Other - Public-Injury	FA09009952
BOATYARD DR	HWY 20	20 FEET NORTH	HWY 20	05/13/02	1130	MON	21202A	Other - Public-Injury	FA02004459
CHESTNUT	WALL	89 FEET EAST	WALL	09/15/04	0745	WED	22107	Other - Public-Injury	FA04009960
CHESTNUT ST	WHIPPLE	4 FEET WEST	WHIPPLE	10/21/04	1921	THU	Unknown	Other - Public-Injury	FA0401079
CHESTNUT ST	SUSIE CT	5 FEET WEST	SUSIE CT	09/21/06	1840	THU	21804(A)	Other - Public-Injury	FA0601333
CHESTNUT ST	WHIPPLE	X	WHIPPLE	03/15/07	1910	THU	21802(A)	Other - Public-Injury	FA0700325
200 B CHESTNUT ST	S MCPHERSON ST	X	S MCPHERSON ST	06/01/09	1300	MON		Other - Public-Injury	FA0900634
DUBOIS	CHESTNUT	112 FEET SOUTH	CHESTNUT	10/19/03	1327	SUN	Oh Improper Driving	Other - Public-Injury	FA0301282
100 B E OAK ST	SR 1	172 FEET EAST	SR 1	03/29/01	1658	THU	21202(A)	Other - Public-Injury	FA0100393
ELM	STEWART ST	140 FEET EAST	STEWART ST	10/14/01	1228	SUN	Oh Than Driver	Other - Public-Injury	FA0101155
FRANKLIN	ALDER	57 FEET SOUTH	ALDER	02/04/03	1900	TUE	22517	Other - Public-Injury	FA0300157
FRANKLIN	WALNUT ST	39 FEET NORTH	WALNUT ST	03/03/03	1055	MON	22517	Other - Public-Injury	FA0300279
FRANKLIN ST	HAZEL ST	X	HAZEL ST	08/09/03	1805	SAT		Other - Public-Injury	FA0300981
FRANKLIN ST	MADRONE ST	X	MADRONE ST	06/04/05	1500	SAT		Other - Public-Injury	FA0500623
FRANKLIN ST	HAZEL ST	216 FEET SOUTH	HAZEL ST	03/05/06	1900	FRI	21208 A	Other - Public-Injury	FA0600636
FRANKLIN ST	SOUTH ST	X	SOUTH ST	07/20/06	2300	THU	22450(A)	Other - Public-Injury	FA0600995
FRANKLIN	CYPRESS ST	X	CYPRESS ST	01/26/07	1355	FRI	22450(A)	Other - Public-Injury	FA0700125
FRANKLIN ST	REDWOOD AV	X	REDWOOD AV	06/22/08	1710	SUN	21650.1	Other - Public-Injury	FA0800791
HARRISON	FIR	2 FEET NORTH	FIR	11/01/01	1447	THU	22450	Other - Public-Injury	FA0101228
MAPLE ST	SR 1	2 FEET EAST	SR 1	03/19/02	1530	TUE	21802(A)	Other - Public-Injury	FA0200278
MAPLE ST	S CORRY ST	X	S CORRY ST	03/25/13	1740	MON	22450(A)	Other - Public-Injury	FA1300422
300 B MCPHERSON ST	REDWOOD	222 FEET NORTH	REDWOOD	10/18/01	1248	THU		Other - Public-Injury	FA0101174
200 B N FRANKLIN ST	E REDWOOD AV	X	E REDWOOD AV	02/02/13	0941	SAT	Oh Improper Driving	Other - Public-Injury	FA01300169
100 B N HARBOR DR	SR 1	3 FEET EAST	SR 1	12/25/00	1325	MON	21703	Other - Private-Injury	FA0401530
100 B N HAROLD ST	ALDER	69 FEET SOUTH	ALDER	01/19/01	1215	FRI	Unknown	Other - Public-Injury	FA0100382
400 B N MAIN SR	CHESTNUT	144 FEET NORTH	CHESTNUT	09/15/10	1145	WED	22450(A)	Other - Public-Injury	FA1001056
N WHIPPLE ST	E LAUREL ST	X	E LAUREL ST	07/19/02	2200	FRI	Unknown	Other - Public-Injury	FA02001723
OAK	MORROW	70 FEET EAST	MORROW	08/11/06	1749	FRI	21200.5	Other - Public-Injury	FA0601094
OAK	CORRY	144 FEET EAST	CORRY	07/20/12	1849	FRI	21804(A)	Other - Public-Injury	FA1200994
OAK ST	HAROLD STREET	152 FEET WEST	HAROLD STREET	02/27/01	1143	TUE	Unknown	Other - Private-Injury	FA0100257
PRIVATE PROPERTY	120 W FIR STREET		120 W FIR STREET	06/25/04	1620	FRI	Unknown	Other - Private-Injury	FA0400665
PRIVATE PROPERTY	S20 N MAIN STREET		S20 N MAIN STREET	10/12/13	1028	SAT	Oh Improper Driving	Other - Public-Injury	FA1301531
PUBING CREEK RD	SR 1	X	SR 1	12/08/09	1000	TUE	22107	Other - Public-Injury	FA0901393
REDWOOD	CORRY	759 FEET EAST	CORRY	10/17/01	1133	WED	22450	Other - Public-Injury	FA0101169
400 B S FRANKLIN ST	HAZEL ST	X	HAZEL ST	10/26/13	0936	SAT	21650.1	Other - Public-Injury	FA1301588
S MAIN ST	OAK ST	183 FEET SOUTH	OAK ST	09/28/10	2015	TUE	22100(A)	Other - Public-Injury	FA1001108
300 B S MAIN ST	E REDWOOD AV	5 FEET SOUTH	E REDWOOD AV	02/06/03	1436	THU	21801(A)	Other - Public-Injury	FA0300172
SPRUCE	SR 1	127 FEET WEST	SR 1	08/11/04	1715	FRI	21804(A)	Other - Public-Injury	FA0400842
SR 1 HY	CHESTNUT ST	161 FEET NORTH	CHESTNUT ST	07/19/05	1656	SAT	22350	Other - Public-Injury	FA0300959
SR 1	REDWOOD	300 FEET NORTH	REDWOOD	11/03/04	1530	WED	22517	Other - Public-Injury	FA0401134
SR 1	AIRPORT RD	6 FEET NORTH	AIRPORT RD	12/11/05	0833	WED	21202(A)	Other - Public-Injury	FA0501662
SR 1	OCEAN VIEW DR	450 FEET NORTH	OCEAN VIEW DR	09/27/08	1835	SAT	22107	Other - Public-Injury	FA0801218
SR 1 HY	BUSH ST	189 FEET NORTH	BUSH ST	08/21/10	1053	TUE	22107	Other - Public-Injury	FA1000967
SR 1 SR	CHESTNUT ST	306 FEET NORTH	CHESTNUT ST	06/15/11	0857	WED	22107	Other - Public-Injury	FA1100637
SR 1 SR	WALNUT ST	4 FEET WEST	WALNUT ST	01/02/12	1423	MON	21804(A)	Other - Public-Injury	FA1200906

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
SR 1 SR	X		CYPRESS ST	06/07/12	2050	WED	21650.1	Other - Public-Injury	FA1200757
SR-1	X		MAPLE	11/05/01	1735	MON	21950(B)	Other - Public-Injury	FA0101249
									Record Count 50

City of Fort Bragg Police Reports

Part 5. All Injury accidents involving pedestrians 2000 to 2013

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
A&W HAVL RD	CYPRESS ST	57 FEET WEST	CYPRESS ST	12/12/10	0900	SUN	21954(A)	Other - Public-Injury	FA1001390
200 B ALLEY EAST SR I SR	E REDWOOD AVENUE	129 FEET SOUTH	E REDWOOD AVENUE	09/27/01	1510	THU	Unknown	Other - Public-Towaway	FA0101093
CHESTNUT	MINNESOTA AV	24 FEET EAST	MINNESOTA AV	05/16/06	1438	TUE	22350	Other - Public-Injury	FA0600888
CORRY	SANDERSON	38 FEET WEST	SANDERSON	05/06/07	1812	TUE	Other Than Driver	Other - Public-Injury	FA0700284
300 B DANA ST	CHESTNUT	104 FEET NORTH	CHESTNUT	09/5/00/03	1821	TUE	21712B	Other - Public-Injury	FA0301210
E LAUREL ST	TAUBOLD CT	531 FEET SOUTH	TAUBOLD CT	01/09/02	1330	WED	Other Improper Driving	Private-H/R-Injury	FA0500035
E PINE ST	MORROW ST	33 FEET EAST	MORROW ST	10/15/09	1553	THU	21950(C)	Other - Public-Injury	FA0901181
FRANKLIN	SR I	X	SR I	06/26/05	1334	SUN	21954	Other - Public-Injury	FA0500712
FRANKLIN	CHESTNUT	X	CHESTNUT	08/19/05	0654	FRI	21950A	City Vehicle Injury	FA0500994
FRANKLIN	PINE	X	PINE	11/29/06	1640	WED	21803(A)	Other - Public-Injury	FA0601680
FRANKLIN ST	E LAUREL ST	X	E LAUREL ST	10/15/07	1619	MON	21950(A)	Other - Public-Injury	FA0701212
FRANKLIN	WALNUT	6 FEET SOUTH	WALNUT	11/20/07	1810	TUE	21950(A)	Other - Public-Injury	FA0701374
FRANKLIN	WALNUT	X	WALNUT	02/26/08	0858	TUE	21950(A)	Other - Public-Injury	FA0800287
FRANKLIN ST	ALDER ST	64.5 FEET NORTH	ALDER ST	01/12/10	1256	TUE	21954(A)	Other - Public-Injury	FA1000044
FRANKLIN ST	OAK ST	16 FEET SOUTH	OAK ST	11/14/11	1132	MON	21950(A)	Other - Public-Injury	FA1101331
HARBOR	OCEAN VIEW DR	161 FEET NORTH	OCEAN VIEW DR	05/14/04	1945	FRI	22106	Other - Public-Injury	FA0400516
HAROLD	OAK ST	X	OAK ST	01/28/03	0840	TUE		Other - Public-Injury	FA0300121
HARRISON ST	REDWOOD	X	REDWOOD	02/03/08	0830	SUN	Unknown	Public-H/R-Public-Injury	FA0800143
MAPLE ST	SR I	X	SR I	12/07/04	1306	TUE	21950(A)	Other - Public-Injury	FA0401246
N FRANKLIN ST	E REDWOOD AV	130 FEET NORTH	E REDWOOD AV	08/17/10	2308	TUE	21954(A)	Other - Public-Injury	FA1000929
200 B N FRANKLIN ST	E REDWOOD AV	3 FEET SOUTH	E REDWOOD AV	01/02/04	1604	FRI	21950(A)	Other - Public-Injury	FA0400006
N HAROLD ST	E LAUREL ST	32 FEET NORTH	E LAUREL ST	05/20/10	0623	THU	21954(A)	Other - Public-Injury	FA1000556
N MAIN ST	E REDWOOD AV	X	E REDWOOD AV	01/05/12	1350	THU	21453(B)	Other - Public-Injury	FA1200020
863 N MAIN ST	ELM	41.4 FEET SOUTH	E FIR ST	09/23/03	1430	THU		Other - Private-Injury	FA0301186
N WHIPLE ST	E FIR ST	39 FEET WEST	STATION POINT	06/23/10	1315	WED	22106	Other - Public-Injury	FA1000868
NORTH HARBOR	FRANKLIN	X	FRANKLIN	06/22/06	1930	THU	22350	Other - Public-Injury	FA0600861
OAK	FRANKLIN	130 FEET WEST	FRANKLIN	02/13/01	1342	TUE	21950(B)	Other - Public-Injury	FA0100204
OAK	FRANKLIN	8 FEET EAST	FRANKLIN	05/02/01	1335	WED	21950(B)	Other - Public-Injury	FA0100536
OAK	MCPHERSON	3 FEET EAST	MCPHERSON	09/19/02	0730	THU	21950A	Other - Public-Injury	FA0200923
OAK ST	FRANKLIN	8 FEET WEST	FRANKLIN	12/16/04	1912	THU	23153(A)	DUI-Public-Injury	FA0401281
OAK ST	S HAROLD ST	X	S HAROLD ST	10/01/09	0900	THU		Other - Public-Injury	FA0901140
OAK ST	N HAROLD ST	12 FEET WEST	N HAROLD ST	02/28/12	1300	TUE	22450(A)	Other - Public-Injury	FA1200307
200 B OAK ST	FRANKLIN	6 FEET EAST	FRANKLIN	09/01/02	1920	SUN	21950(A)	Other - Public-Injury	FA0200886
700 B OAK ST	S HAROLD ST	X	S HAROLD ST	05/18/10	0800	TUE	21950(B)	Other - Public-Fatal	FA1000547
PINE	SR I	6 FEET EAST	SR I	02/21/05	2040	MON	21950(A)	Other - Public-Injury	FA0500207
PRIVATE PROPERTY	865 N MAIN		865 N MAIN	01/27/01	1930	SAT	Other Improper Driving	Other - Private-Injury	FA0100115
PRIVATE PROPERTY	660 S MAIN		660 S MAIN	07/22/03	1630	TUE	Other Improper Driving	Private-H/R-Injury	FA0300884
PRIVATE PROPERTY	171 BOATYARD	X	171 BOATYARD	01/24/04	1543	SAT	Other Improper Driving	Other - Private-Injury	FA0400098
PRIVATE PROPERTY	124 S MAIN		124 S MAIN	11/28/04	1900	SUN	Other Improper Driving	Other - Private-Injury	FA0401211
PRIVATE PROPERTY	490 S MAIN		490 S MAIN	08/01/05	1329	MON	Unknown	Other - Private-Injury	FA0500989
PRIVATE PROPERTY	171 BOATYARD DR		171 BOATYARD DR	08/01/07	1332	WED	22350	Other - Private-Injury	FA0700923
PRIVATE PROPERTY	171 BOATYARD DRIVE		171 BOATYARD DRIVE	07/18/11	1303	MON	Other Improper Driving	Other - Private-Injury	FA1100739
PRIVATE PROPERTY	740 S FRANKLIN STREET		740 S FRANKLIN STREET	08/06/12	1003	MON	Other Improper Driving	Other - Private-Injury	FA1201086
PRIVATE PROPERTY	418 N MAIN STREET		418 N MAIN STREET	11/20/12	1159	FRI	Other Improper Driving	Other - Private-Injury	FA1201706
PRIVATE PROPERTY	WALNUT ST	X	WALNUT ST	11/14/03	1126	FRI	21950(A)	Public-H/R-Public-Injury	FA0301397
S FRANKLIN ST	OAK ST	X	OAK ST	09/07/10	1330	TUE	21950(A)	Public-H/R-Public-Injury	FA1001023
S FRANKLIN ST	CYPRESS ST	X	CYPRESS ST	02/08/12	0846	WED	29150(A)	Other - Public-Injury	FA1200193
S HARRISON ST	OAK ST	2 FEET SOUTH	OAK ST	01/05/11	1721	WED	21950(A)	Other - Public-Injury	FA1100019

Traffic Accident Report

Grouped and Sorted By Location, Cross Street and Date

Location	Intersection	Distance	Cross Street	Date	Time	Day	PCF	BCS Code	Incident No.
S MAIN ST	70	FEET SOUTH	MPM 60.00	04/30/11	2118	SAT	21954	Other - Public-Injury	FA1100448
1100 B S MAIN ST	X	FEET SOUTH	OCEAN VIEW DR	07/30/09	2100	THU	21453(A)	Other - Public-Injury	FA0900886
SANDERSON	X	FEET NORTH	CHESTNUT	11/14/05	1500	MON	21950(A)	Other - Public-Injury	FA0501442
SR 1	3	FEET SOUTH	LAUREL	12/05/03	1850	FRI	21950(A)	Other - Public-Injury	FA0301483
SR 1	X	FEET SOUTH	PINE	10/18/05	1530	TUE	21950(A)	Other - Public-Injury	FA0501308
SR 1	X	FEET SOUTH	SPRUCE ST	02/13/06	1150	MON	21950(A)	Other - Public-Injury	FA0600243
SR 1	15	FEET NORTH	PINE	05/20/07	1712	SUN	22350	Other - Public-Injury	FA0700586
SR 1	4	FEET SOUTH	BUSH ST	03/02/10	1657	TUE	21950(A)	Other - Public-Injury	FA1000246
SR 1 HY	6	FEET EAST	OAK ST	10/22/13	1045	TUE	21950(A)	Public-H/R - Public-Injury	FA1301572
SR 1	11	FEET NORTH	OCEAN VIEW	12/04/02	1801	WED	21451(A)	Other - Public-Injury	FA0201251
SR 1	3	FEET NORTH	LAUREL	03/08/03	1200	SAT	21950(B)	Other - Public-Injury	FA0300289
SR 1	6	FEET SOUTH	REDWOOD	11/07/01	1545	WED	21950(B)	Other - Public-Injury	FA0101258
SR 1 ST	X	FEET SOUTH	ELM	10/08/02	1800	TUE	21950(A)	Other - Public-Injury	FA0201040
SR 1 ST	X	FEET SOUTH	ELM	10/09/07	2200	TUE	21950(A)	Other - Public-Injury	FA0701193
SR 1 ST	X	FEET SOUTH	ELM	10/09/07	2200	TUE	21950(A)	Other - Public-Injury	FA0701194

Record Count 63

ATTACHMENT 10: COST/BENEFIT ANALYSIS DETAILS, ATTACHMENT FOR SECTION IV NARRATIVE
QUESTION 4B

Increase safety and mobility of non-motorized users: \$ 1,283,700:

An 80% crash reduction factor (CRF) in bicycle/vehicle injuries was used. Five bicycle injuries were recorded with a value of \$79,000 per injury as calculated with the Caltrans "Local Roadway Safety Manual for California Local Road Owners" (Manual), version 1.1, dated April 2013. Generally, most accidents were reported as Crash Severity B, with a small percentage Crash Severity A. The five bicycle injury accidents in a 13 year period on Chestnut Street between 2000 and 2013 are per Fort Bragg Police Reports. A 20 year lifespan for new infrastructure per the Manual was used.

Three pedestrian injury accidents occurred between 2000 and 2013 per Fort Bragg Police reports. An 80% CRF for pedestrian injury accidents was used. A narrow roadway and narrow non-ADA compliant exists. Pedestrians are regularly seen walking in the Vehicle traveled-way. A multi-use trail, separated from the street traveled-way will replace the existing sidewalk. A CRF of Severity A, (\$216,000 per accident), was used. It should be noted that elsewhere in town three pedestrian related deaths occurred between 2000 and 2013, representing a total of 4% of all reported pedestrian injury accidents.

Chestnut Street is a primary access corridor for the full range of transportation modes serving five of the six primary and secondary schools in town. Given the current experience of pedestrians, (including children), walking in the existing roadway, the attractiveness of using a clearly safer separated multi-use path, and the project's education/outreach component, the 80% CRF suggested by the Manual is appropriate.

The calculated benefit for Bicyclists is: $0.80 \times (5 \text{ bicyclists}) \times (\$79,000)/13 \text{ years}$ or \$24,308/year. The calculated benefit for Pedestrians is: $0.80 \times (3 \text{ pedestrians}) \times (\$216,000)/13 \text{ years}$ or \$39,877/year. The total annual benefit is \$64,185/year. Spread over the 20-year project life –span yields a total benefit of approximately \$1,283,700.

Enhance public health: \$578,808:

Active transportation benefits for Cycling \$1.92 per mile, and Walking \$3.70 per mile per (NZTA 2010, Vol. 2 p 8-11, per <http://vtpi.org/nmt-tdm.pdf>, Table 6, Page 21) was used. An estimated, average walking trip of 1200 feet, (.23 mile one-way, or .46 mile round trip) with a benefit value of \$1.70 per round trip was used. An average bicycle trip of 3000 feet (.57 mile one-way, or 1.14 mile round trip) with a benefit value of \$2.19 per round trip was used. The projected increase in walking and bicycling was estimated to be a 30%¹ increase from 306 students/parents to 399 students/parents.

Total benefit: \$1,862,508

¹ A California study showed that schools that received infrastructure improvements through the Safe Routes to School Program yielded walking and bicycling increases that were often in the range of 20% to 200% (http://www.saferoutespartnership.org/sites/default/files/pdf/school_bus_cuts_national_stats_FINAL.pdf)

ATTACHMENT 10: COST/BENEFIT ANALYSIS DETAILS, ATTACHMENT FOR SECTION IV NARRATIVE
QUESTION 4B

The projected lifecycle of facility is estimated at 20 years, (as suggested by the Manual). Currently per 2013 Safe Routes to Schools (SRTS) tallies: Redwood Elementary School 53 children walking, and 1 child biking, and assuming approximately 25 parents walking with kids, (with an enrollment of 450, and up to 12% walking or biking); Dana Gray Elementary School approximately 99 children walking, and 3 children biking, (with an enrollment of 392, and up to 26% walking or biking); Fort Bragg High School no tally data assumed to be approximately the same percentage of students walking or biking as Dana Gray (with an enrollment of 513, and 26% walking or biking), for a total of 129 walking and 4 biking). Approximately 180 days per school year assumed. As noted previously, a 20 year life of the infrastructure was used. Total currently walking is 306. Total currently biking is 8². The increase in walking by 30% results in 92 new walkers. The increase in biking by 30% results in 2 new bicyclists. New health benefits for pedestrians: 92 pedestrians x \$1.70 x 180 days x 20 years equaling \$563,040. New health benefits for bicyclists: 2 bicyclists x \$2.19 x 180 days x 20 years equaling \$15,768. The total health benefit is therefore valued at \$578,808.

² Five bicycle accidents with injuries were reported to the Police Department between 2000 and 2013, which is a total of 10% of all bicycle accidents reported in the City during that period. Based on this information, my estimate of the number of bicyclists is very low, however no current available data (other than percentage of bicycle accident injuries) supports a higher number.

Spade, Teresa

From: Cynthia Vitale <Cynthia@csgcalifornia.com>
Sent: Saturday, May 10, 2014 11:24 AM
To: Spade, Teresa
Cc: 'Calcc Calcc'; Cynthia Vitale; Virginia Clark (Virginia.clark@ccc.ca.gov)
Subject: RE: ATP grant application partnering (Spade, Teresa)

Good morning,

Thank you for contacting CALCC. Unfortunately, no local corps will be able to participate due to the geographic location of this project. This email should serve as confirmation that you have contacted the local corps and that they have declined to participate. Feel free to attach this email to your final application.

Thanks,
Cynthia

Cynthia Vitale
Conservation Strategy Group
1100 11th Street, Suite 200
Sacramento, CA 95814
(916) 558-1516 ext. 126

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From: Calcc Calcc [mailto:callocalcorps@gmail.com]
Sent: Thursday, May 08, 2014 6:44 PM
To: Cynthia Vitale
Subject: Fwd: ATP grant application partnering (Spade, Teresa)

----- Forwarded message -----

From: Spade, Teresa <tspade@fortbragg.com>
Date: Tue, May 6, 2014 at 9:54 AM
Subject: RE: ATP grant application partnering (Spade, Teresa)
To: "Clark, Virginia@CCC" <Virginia.Clark@ccc.ca.gov>, "callocalcorps@gmail.com" <callocalcorps@gmail.com>
Cc: "Galvan, Jimmy@CCC" <Jimmy.Galvan@ccc.ca.gov>, "Malinowski, Chris@CCC" <Chris.Malinowski@ccc.ca.gov>, "Simpson, Trish@CCC" <Trish.Simpson@ccc.ca.gov>, "Owen, Jennifer" <Jowen@fortbragg.com>, "Varga, Tom" <TVarga@fortbragg.com>, "Prairie, Crystal" <cprairie@fortbragg.com>

Dear Ms. Vitale and Ms. Clark:

Attached is a letter requesting California Conservation Corps participation in our ATP Grant project, Chestnut Street Multi Use Facility and Safe Routes to School Program. Please let me know if you have any questions or need any additional information.

Teresa R Spade, AICP

From: Clark, Virginia@CCC [mailto:Virginia.Clark@CCC.CA.GOV]
Sent: Monday, May 05, 2014 2:39 PM
To: Spade, Teresa
Cc: Galvan, Jimmy@CCC; Malinowski, Chris@CCC; Simpson, Trish@CCC
Subject: FW: ATP grant application partnering (Spade, Teresa)
Importance: High

Teresa,

We are interested in participating in this ATP project. See the emails below that outline what we can do and what a few concerns are.

Virginia Clark

Region Deputy, Region 1



California Conservation Corps

[\(916\) 341-3147](tel:(916)341-3147)

[fx\(877\) 834-4177](tel:fx(877)834-4177)

virginia.clark@ccc.ca.gov



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- School bike & walk safety assemblies: modeled similarly to the bicycle rodeo sponsored by PAL, and per the PBIS demonstration method of teaching, CCC members would staff stations to teach & demonstrate safe practices at school parking lots; at school crosswalks; at intersections, wearing helmets, reflectors, etc. This would occur at each elementary school site (RW, DG, FBMS, 3 Rivers) within the first 2 months of the school year.
- Parent education at back to school tables: CCC members staff information tables at each school's back to school event to actively engage & distribute safety info to parents. This would occur within the first 1 monthly of the school year.

3. **Encouragement component:**

- ***Walking school buses/bike trains: CCC members could be used to launch "walking Wednesday"-type programs at each school (K-8). CCC members would lead walking buses and bike trains to and from school in order to launch the program (Fall activity: Sept 1 through Oct 31); CCC staff would be needed 8 to 8:30 am and 2 30 to 3 30 pm.***
- ***Park & Walk Program: On 2 designated days at each school in the first 2 months of the school year, CCC members assist launch of the program by directing drivers to designated parking areas & encouraging parents to walk with their kids for the final 2-3 blocks to school.***

Teresa R Spade, AICP

Community Development Department

City of Fort Bragg

416 North Franklin Street

Fort Bragg, CA 95437

Phone: 707-961-2827 x 107

Fax: 707-961-2802

Spade, Teresa

From: Clark, Virginia@CCC <Virginia.Clark@CCC.CA.GOV>
Sent: Monday, May 12, 2014 4:28 PM
To: Spade, Teresa; calocalcorps@gmail.com
Cc: Galvan, Jimmy@CCC; Malinowski, Chris@CCC; Owen, Jennifer; Varga, Tom; Prairie, Crystal; 'calocalcorps@gmail.com'
Subject: RE: ATP grant application partnering (Spade, Teresa)

Yes thank you as previously stated we are interested in participating in this ATP project.

Virginia Clark
Region Deputy, Region 1



California Conservation Corps
(916) 341-3147
fx(877) 834-4177
virginia.clark@ccc.ca.gov



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Visit our web site at www.ccc.ca.gov for more information about the California Conservation Corps
Visit our web site at www.WatershedStewards.com for more information about the Watershed Stewards Program

From: Spade, Teresa [mailto:tspade@fortbragg.com]
Sent: Tuesday, May 06, 2014 9:55 AM
To: Clark, Virginia@CCC; calocalcorps@gmail.com
Cc: Galvan, Jimmy@CCC; Malinowski, Chris@CCC; Simpson, Trish@CCC; Owen, Jennifer; Varga, Tom; Prairie, Crystal@FORTBRAGG@DOT
Subject: RE: ATP grant application partnering (Spade, Teresa)

Dear Ms. Vitale and Ms. Clark:

Attached is a letter requesting California Conservation Corps participation in our ATP Grant project, Chestnut Street Multi Use Facility and Safe Routes to School Program. Please let me know if you have any questions or need any additional information.

Teresa R Spade, AICP

From: Clark, Virginia@CCC [mailto:Virginia.Clark@CCC.CA.GOV]
Sent: Monday, May 05, 2014 2:39 PM
To: Spade, Teresa
Cc: Galvan, Jimmy@CCC; Malinowski, Chris@CCC; Simpson, Trish@CCC
Subject: FW: ATP grant application partnering (Spade, Teresa)
Importance: High

Teresa,

We are interested in participating in this ATP project. See the emails below that outline what we can do and what a few concerns are.

Virginia Clark
Region Deputy, Region 1



California Conservation Corps
(916) 341-3147
fx(877) 834-4177
virginia.clark@ccc.ca.gov



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Visit our web site at www.ccc.ca.gov for more information about the California Conservation Corps

Visit our web site at www.WatershedStewards.com for more information about the Watershed Stewards Program

From: Galvan, Jimmy@CCC
Sent: Monday, May 05, 2014 12:02 PM
To: Clark, Virginia@CCC
Cc: Malinowski, Chris@CCC
Subject: ATP grant application partnering (Spade, Teresa)
Importance: High

Good afternoon,

After looking at the scope of work I feel confident in committing to entertain the following areas;

1. Construction/Demolition:

- Removal of debris associated with sidewalk/curb/gutter demolition.
- Installation, monitoring, and maintenance of Best Management Practices (BMPs) materials including straw wattles or other materials used to prevent sedimentation or other materials from entering storm drains during construction, as outlined in the erosion control plan.

2. Education component:

- School bike& walk safety assemblies: modeled similarly to the bicycle rodeo sponsored by PAL, and per the PBIS demonstration method of teaching, CCC members would staff stations to teach & demonstrate safe practices at school parking lots; at school crosswalks; at intersections, wearing helmets, reflectors, etc. This would occur at each elementary school site (RW, DG, FBMS, 3 Rivers) within the first 2 months of the school year.
- Parent education at back to school tables: CCC members staff information tables at each school's back to school event to actively engage & distribute safety info to parents. This would occur within the first 1 monthly of the school year.

FYI, we have not responded to Teresa. Lastly, the areas below that I color coded in red are concerning because of the split shifts and risks associated with Traffic Control. We are certainly amendable with your approval.

Jimmy Galvan
California Conservation Corps
Ukiah & Napa Center Director
(707) 463-2822 Office
(707) 272-4102 or (530) 209-7675 Mobile

From: Spade, Teresa [mailto:tspade@fortbragg.com]
Sent: Monday, May 05, 2014 10:38 AM
To: Malinowski, Chris@CCC
Cc: Galvan, Jimmy@CCC
Subject: ATP grant application partnering

Hi Chris –

Mr. Galvan suggested I contact you regarding development of a partnership with the City of Fort Bragg on our Chestnut Street Multi Use Facility/ Safe Routes to School ATP grant application project. The project consists of construction of an 8 to 12 foot wide raised multi-use facility for bicycles and pedestrians on the north side of Chestnut Street. This is a 0.9 mile stretch of road, and the project would include relocation of utilities and other structures currently located in the existing narrow sidewalk area, and other safety improvements including bulb outs and bump outs. We are also applying for a new Safe Routes to School program in conjunction with our construction project. The following items have been identified as potential items we may be able to request your assistance for. Please let me know if any of these items look feasible or if you have any additional suggestions. I will be sending a letter to Virginia Clark and Cynthia Vitale via email within the next couple days.

1. Construction/Demolition:

- Removal of debris associated with sidewalk/curb/gutter demolition.
- Installation, monitoring, and maintenance of Best Management Practices (BMPs) materials including straw wattles or other materials used to prevent sedimentation or other materials from entering storm drains during construction, as outlined in the erosion control plan.
- ***Traffic control activities needed during construction/demolition.***

2. Education component:

- School bike & walk safety assemblies: modeled similarly to the bicycle rodeo sponsored by PAL, and per the PBIS demonstration method of teaching, CCC members would staff stations to teach & demonstrate safe practices at school parking lots; at school crosswalks; at intersections, wearing helmets, reflectors, etc. This would occur at each elementary school site (RW, DG, FBMS, 3 Rivers) within the first 2 months of the school year.
- Parent education at back to school tables: CCC members staff information tables at each school's back to school event to actively engage & distribute safety info to parents. This would occur within the first 1 month of the school year.

3. ***Encouragement component:***

- ***Walking school buses/bike trains: CCC members could be used to launch "walking Wednesday"-type programs at each school (K-8). CCC members would lead walking buses and bike trains to and from school in order to launch the program (Fall activity: Sept 1 through Oct 31); CCC staff would be needed 8 to 8:30 am and 2 30 to 3 30 pm.***
- ***Park & Walk Program: On 2 designated days at each school in the first 2 months of the school year, CCC members assist launch of the program by directing drivers to designated parking areas & encouraging parents to walk with their kids for the final 2-3 blocks to school.***

Teresa R Spade, AICP
Community Development Department
City of Fort Bragg
416 North Franklin Street
Fort Bragg, CA 95437
Phone: 707-961-2827 x 107
Fax: 707-961-2802



CITY OF FORT BRAGG

Incorporated August 5, 1889
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May 6, 2014

Ms. Cynthia Vitale
California Association of Local
Conservation Corps
1100 111th Street, Ste. 200
Sacramento, CA 95816

Ms. Virginia Clark
California Conservation Corps
1719 24th Street
Sacramento, CA 95816

Re: Request for partnership on Chestnut Street Multi-Use Path and Safe Routes to School

Dear Ms. Vitale and Ms. Clark:

The City of Fort Bragg is seeking an Active Transportation Program (ATP) grant from Caltrans for construction of a 0.9 mile long pedestrian and bicycle multi-use facility and an associated Safe Routes to School program for Fiscal Years 2014-15 and 2015-16. The City is interested in partnering with the California Conservation Corps on this project and has identified a number of project components that we believe you may be able to assist us with. Please let me know if you have an interest in partnering with the City on any or all of these project components.

1. Construction/Demolition:
 - Removal of debris associated with sidewalk/curb/gutter demolition.
 - Installation, monitoring, and maintenance of Best Management Practices (BMPs) materials including straw wattles or other materials used to prevent sedimentation or other materials from entering storm drains during construction, as outlined in the erosion control plan.
2. Education component:
 - School bike & walk safety assemblies: modeled similarly to the bicycle rodeo sponsored by PAL, and per the PBIS demonstration method of teaching, CCC members would staff stations to teach & demonstrate safe practices at school parking lots; at school crosswalks; at intersections, wearing helmets, reflectors, etc. This would occur at each elementary school site (Redwood Elementary, Dana Gray Elementary, Fort Bragg Middle School, Three Rivers Charter School) within the first 2 months of the school year.
 - Parent education at back to school tables: CCC members staff information tables at each school's back to school event to actively engage & distribute safety info to parents. This would occur within the first month of the school year.

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Project Description/Preliminary Plan:

Purpose: The project is intended to improve safety and provide for safe bicycle and pedestrian facilities along the main route to Dana Gray Elementary (third through fifth grades), Redwood Elementary (Kindergarten through second grades), and Fort Bragg High School (ninth through twelfth grades) of the Fort Bragg Unified School District.

Need: Currently pedestrian facilities are inadequate and no bicycle facilities exist in the project area. Existing Chestnut Street sidewalks are typically three to 4 ½ feet in width except for limited locations adjacent to school properties and within existing commercial zones. In addition, there are numerous physical obstructions including utility poles, fire hydrants and drainage structures, and existing gaps in the walkways which impede safe, continuous pedestrian access. Improved walkways rank as a high priority for Chestnut Street residents.

Scope of Work:

Construct an eight to twelve foot wide multi-use facility on the north side of Chestnut Street for a distance of approximately 4800 feet from the intersection of Chestnut and Franklin Streets, to the Fort Bragg High School driveway 140 feet east of the intersection of Chestnut Street and Ebbing Way. The plan features:

- Proposed street sections that can be implemented within the limits of existing rights-of-way;
- New curb and gutter and a new multiuse path replacing existing narrow walkways along the north side of the street;
- Permitted on-street parking shifted from the south side of Chestnut to the north side;
- ADA compliant ramps and intersections;
- Relocated joint service utility poles, street signs and drainage inlets; and
- Traffic calming bulbouts and bump outs, electronic speed advisor sign, high visibility crosswalks.

Figures IV-1A and IV-1B, from the Chestnut Street Corridor Conceptual Plan and Right of Way Feasibility Study, show typical road sections for the project.

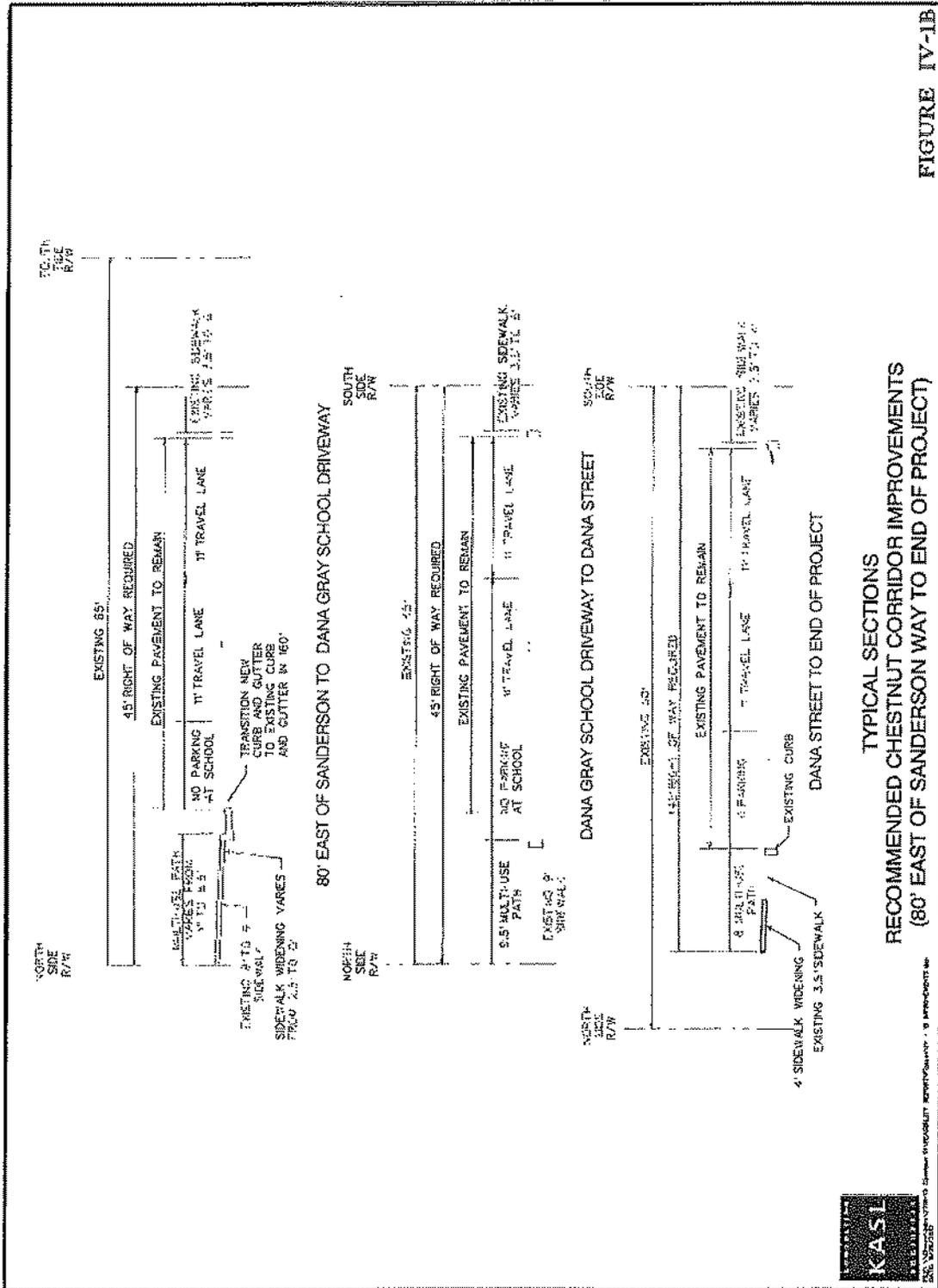


FIGURE IV-1B

Figure 2 Typical roadway sections for the proposed project, east side

The following block by block breakdown describes the project scope in detail. This project description and associated design plans are also viewable online at www.city.fortbragg.com.

Franklin Street to McPherson Street (Figure IV-2a)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Lengthen left turn stacking lane, westbound, at the Franklin Street intersection
- Construct eight foot to twelve foot wide multiuse path, north side
- Construct ADA compliant ramps at Franklin Street and McPherson Street
- Construct bulbout at the McPherson Street intersection
- Relocate joint pole to back of walk or top back of curb
- Conform driveways to new face of curb location
- Relocate drain inlet at Franklin Street and construct new drain lateral
- Relocate street signs
- Place crosswalk, centerline and turn lane striping and markings and red curb painting
- Designate no on-street parking on north and south sides of street

McPherson Street to Harrison Street (Figure IV-2b)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct twelve foot wide multiuse path, north side
- Construct ADA compliant ramps at McPherson Street and Harrison Street
- Construct bulbout at the McPherson Street intersection
- Construct bump out islands
- Relocate joint pole to back of walk, top back of curb or bump out island
- Conform driveway to new face of curb location
- Relocate drain inlet at Harrison Street and construct new drain lateral
- Relocate street signs
- Permit limited on-street parking, north side, as shown
- Place crosswalk and street markings and red curb painting
- Place centerline and right edge striping

Harrison Street to Whipple Street (Figure IV-2c)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct twelve foot wide multiuse path, north side
- Construct ADA compliant ramps at Harrison Street and Whipple Street
- Construct bulbout at Whipple Street intersection
- Construct bump out island
- Relocate joint poles to back of walk, top back of curb or bump out island
- Relocate street signs
- Relocate drain inlet at Harrison Street and construct new drain lateral
- Conform driveway to new face of curb location
- Permit on street parking, north side,
- Place crosswalk and street markings

- Place centerline and right edge striping and red curb painting

Whipple Street to Corry Street (Figure IV-2d)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Whipple Street, Corry Street, and the mid-block alley
- Construct bulbout at Whipple Street
- Construct bump out islands
- Relocate joint pole to back of walk or top back of curb
- Conform driveway to new face of curb location
- Relocate drain inlet at Whipple Street and construct new drain lateral
- Relocate street signs
- Permit on street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and red curb painting
- Place stop markings, stop limit and new stop sign east bound Chestnut Street at Corry Street

Corry Street to Harold Street (Figure IV-2e)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Corry Street, Susie Court and Harold Street
- Construct bump out islands
- Relocate joint pole to back of walk or to top back of curb
- Conform driveways to new face of curb locations
- Relocate drain inlets at Corry Street and Susie Court and construct new drain laterals
- Relocate street signs
- Permit on-street parking, north side, as shown. Prohibit parking adjacent to garage at Parcel 018-100-04
- Place crosswalk, centerline and right edge striping and red curb painting
- Place stop markings, stop limit and new stop sign west bound Chestnut Street at Corry Street

Harold Street to Mid-Block Harold Street (Figure IV-2f)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Harold Street and at mid-block alley
- Construct bump out islands
- Conform driveways to new face of curb locations
- Relocate drain inlet at Harold Street and construct new drain lateral
- Relocate street sign

- Permit on-street parking, north side, as shown. Prohibit parking adjacent to garage at Parcel 018-100-68
- Place crosswalk, centerline and right edge striping and red curb painting

Mid-Block Harold Street to Lincoln Street (Figure IV-2g)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramp at Lincoln Street
- Construct bump out island
- Relocate joint pole to back of walk, top back of curb or bump out
- Conform driveway to new face of curb location
- Relocate drain inlet opposite Olsen Lane and construct new drain lateral
- Relocate street signs
- Permit on-street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and markings and red curb painting

Lincoln Street to Mid-Block Lincoln Street (Figure IV-2h)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramp at Lincoln Street
- Construct bump out island
- Relocate drain inlet at Lincoln Street and construct new drain lateral
- Relocate street signs
- Permit on-street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and markings and red curb painting

Mid-Block Lincoln Street to Wall Street (Figure IV-2i)

- Remove and replace existing curb and gutter to accommodate new street section
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramp at Wall Street
- Construct bump out islands along
- Relocate joint poles to back of walk, top back of curb or bump out
- Conform driveways to new face of curb locations
- Relocate inlet opposite Minnesota Avenue and construct new drain lateral
- Relocate street signs
- Permit on-street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and markings and red curb painting
- Furnish and install electronic, solar powered, speed advisory sign for westbound movement
- Crosswalk improvements shown at Minnesota Avenue were constructed with the 2011 SRTS Project

Wall Street to Sanderson Way (Figure IV-2j)

- Remove and replace existing curb and gutter
- Remove existing sidewalk
- Construct eight foot wide multiuse path, north side
- Construct ADA compliant ramps at Wall Street and Sanderson Way
- Construct bump out islands
- Conform driveways to new face of curb locations
- Relocate street signs
- Permit on street parking, north side, as shown
- Place crosswalk, centerline and right edge striping and markings and red curb painting

Sanderson Way to Woodland Drive (Figure IV-2k)

- Remove and replace existing curb and gutter, west of Dana Gray School to accommodate new street section
- Remove sidewalk along frontage of Parcel 008-332-12
- Construct eight foot wide multiuse path, north side, along frontage of Parcel 008-332-12
- Transition new eight foot wide multiuse path to existing sidewalk along frontage of Dana Gray School
- Existing sidewalk along frontage of Dana Gray School and existing curb and gutter east of school driveway to be retained
- Construct ADA compliant ramp at Sanderson Way
- Relocate joint poles to back of walk or top back of curb
- Conform driveway to new face of curb location
- Relocate drain inlet at Sanderson Way and construct new drain lateral
- Relocate street signs
- Install no parking signs
- Place crosswalk, centerline and right edge striping and markings and red curb painting

Woodland Drive to End of Project (Figure IV-2l)

- Existing curb and gutter and existing eight foot to nine foot wide sidewalk along frontage of Dana Gray School to be retained
- Widen existing sidewalk to eight feet wide east of Dana Street to end of project; existing sidewalk and curb to be retained
- Construct ADA compliant ramp at Dana Street
- Relocate joint poles to back of walk or top back of curb
- Parking allowed from Dana Street to the end of project, north side
- Place crosswalk, centerline and right edge striping and red curb painting
- Relocate chain link fence along Fort Bragg High School to new back of walk

TRAFFIC CALMING FEATURES

A summary of the traffic calming features proposed for the Chestnut Corridor include:

- Electronic Speed Advisory Sign for westbound movement, west of Wall Street.
- New stop signs and stop ahead pavement markings at Corry Street intersection
- Bulbouts at McPherson Street and Whipple Street

- Bump out islands. These medians are proposed at various locations along the Chestnut Street Corridor to reduce the apparent width of the vehicle lanes and to provide protection for parked vehicles.
- Reduced vehicle lane widths throughout.

ACCESSIBILITY AND SAFETY FEATURES

Improved pedestrian accessibility will be provided along the north side of Chestnut with the completion of a continuous, widened multiuse trail, the removal and relocation of obstacles, the construction of ADA compliant ramps, and parking restriction near each street intersection, driveway and alley crossing. Bulbouts at McPherson Street and Whipple Street and bump outs placed adjacent to parking spaces will encourage slower traffic speeds within the Chestnut Street corridor.

NON-INFRASTRUCTURE COMPONENT

The non-infrastructure component of the project involves Education, Encouragement, Enforcement, and Evaluation activities, including classroom curriculum, presentations at assemblies, development of a SRTS webpage or website, development of SRTS maps, "Safety Tip of the Month" at each school site, educational materials to be distributed at community events, annual IWALK days, weekly walk to school program, walking school buses & bicycle trains, park & walk and carpooling programs, presence of school resource officer, "caught you being good" campaign, enhanced patrols, neighborhood watch, and collection and submittal of data using student tally and parent survey forms.

PROJECT LOCATION

The multi-use path project is located along Chestnut Street in the City of Fort Bragg in Mendocino County, California. Chestnut Street begins at State Route One and heads east to the Fort Bragg City Limits, east of Fort Bragg High School. The scope of this project begins at the intersection of Chestnut and Franklin Streets, located one block east of State Route One, and continues east along Chestnut Street, a distance of approximately 4800 feet, to just beyond the intersection of Chestnut Street and Ebbing Way, at Fort Bragg High School. Figure 3, Project Map, shows the location of the multi-use path project.

The non-infrastructure Safe Routes to School Program would occur at schools within the Fort Bragg Unified School District in the City of Fort Bragg, including Redwood Elementary School, Dana Gray Elementary School, Fort Bragg Middle School, and Three Rivers Charter School.



City of Fort Bragg

Figure 3 Project Map for Chestnut Street Multi Use Facility

Detailed Estimate:

Removal of debris associated with sidewalk/curb gutter demolition is expected to take approximately 50 work hours with a crew of approximately 8 workers and is expected to cost approximately \$25,000.

School Bike and Walk Safety Assemblies are expected to take approximately 20 total hours (10 total hours per year, three separate schools, for approx. 3+ hours per assembly). A crew of approximately eight workers is needed. This is expected to cost approximately \$3,000 total. Supplies, coordination and incentives are expected to come out of a different budget and are not included in the estimate, as these will be provided.

Parent education at Back to School night tables is expected to take approximately 32 work hours (16 hours per year, four schools, four separate evenings at four hours each per year), and is expected to require a crew of four people. This is expected to cost a total of \$2,500 in labor. Supplies, coordination and incentives provided at the event are expected to come out of a separate budget and are not included in the estimate as these will be provided.

Project Schedule

The infrastructure project (construction of the multi-use facility along the north side of Chestnut Street) is expected to be constructed in **one construction season between May 1 and October 15, 2015.**

Non-infrastructure components, including the Safe Routes to School Program components, are expected to occur as follows:

- School Bike & Walk and safety assemblies are to occur in **September and October 2014 and September and October 2015.**
- Parent education at back to school tables: to occur **one evening within the first month of the school year (August or September), 2014 and 2015.**

Please let me know if the project is of interest to you. If you have any questions, please contact me at 981-2827, ext. 110.

Sincerely,



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707-961-2827x107

cc: Jennifer Owen, Housing and Economic Development Coordinator
Crystal Prairie, Public Works Project Analyst
Tom Varga, Public Works Director