

City of Roseville Downtown Roseville Class I Trails Project



**Active Transportation Program Application
May 2014**

Submitted by:



City of Roseville

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Vicinity Map

Project Location and Local Activity Centers

Site Photos

Preliminary Plans

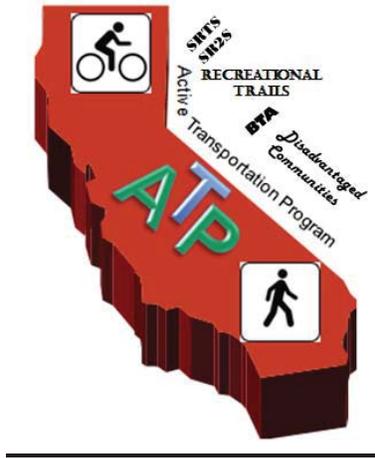
Detailed Engineer’s Estimate

Approved Plan Information

Documentation of Public Participation Process

Letters of Support

CCC Correspondence



ACTIVE TRANSPORTATION PROGRAM CYCLE 1

APPLICATION Part 1 (Includes Sections I, V, VI, VII, VIII & XI)

Please read the Application Instructions at
<http://www.dot.ca.gov/hq/LocalPrograms/atp/index.html>
prior to filling out this application

Project name:

City of Roseville - Downtown Roseville Class I Trail

For Caltrans use only: ___TAP ___STP ___RTP ___SRTS ___SRTS-NI ___SHA
___DAC ___Non-DAC ___Plan

I. GENERAL INFORMATION

Project name: City of Roseville - Downtown Roseville Class I Trail

(fill out all of the fields below)

| | |
|--|---|
| 1. APPLICANT (Agency name, address and zip code) City of Roseville (311 Vernon St, Roseville CA 95678) | 2. PROJECT FUNDING ATP funds Requested \$ _____ 1,236,000.00 Matching Funds \$ _____ 160,000.00 (If Applicable) Other Project funds \$ _____ 1,151,000.00 TOTAL PROJECT COST \$ _____ 2,547,000.00 |
| 3. APPLICANT CONTACT (Name, title, e-mail, phone #) Mike Dour, Alternative Transportation Analyst, mdour@roseville.ca.us, (916) 746-1304 | 5. PROJECT COUNTY(IES): <p style="text-align: center;">Placer County</p> |
| 4. APPLICANT CONTACT (Address & zip code) 401 Vernon Street, Roseville, CA 95678 | 7. Application # <u> 1 </u> of <u> 2 </u> (in order of agency priority) |
| 6. CALTRANS DISTRICT #- Click Drop down menu below District 3 | |

Area Description:

| | |
|--|---|
| 8. Large Metropolitan Planning Organization (MPO)- Select your "MPO" or "Other" from the drop down menu> | SACOG Sacramento Area Council of Government |
| 9. If "Other" was selected for #8- select your MPO or RTPA from the drop down menu> | Placer CTPA |
| 10. Urbanized Area (UZA) population (pop.)- Select your UZA pop. from drop down menu> | Within a Large MPO (Pop > 200,000) |

Master Agreements (MAs):

11. Yes, the applicant has a FEDERAL MA with Caltrans.
12. Yes, the applicant has a STATE MA with Caltrans.
13. If the applicant does not have an MA. Do you meet the Master Agreement requirements? Yes No
 The Applicant MUST be able to enter into MAs with Caltrans

Partner Information:

| | |
|--|--------------------------------|
| 14. Partner Name*: | 15. Partner Type |
| 16. Contact Information (Name, phone # & e-mail) | 17. Contact Address & zip code |

Click here if the project has more than one partner; attach the remaining partner information on a separate page

*If another entity agrees to assume responsibility for the ongoing operations and maintenance of the facility, documentation of the agreement must be submitted with the application, and a copy of the Memorandum of Understanding or Interagency Agreement between the parties must be submitted with the request for allocation.

Project Type: (Select only one)

18. Infrastructure (IF) 19. Non-Infrastructure (NI) 20. Combined (IF & NI)

Project name: City of Roseville - Downtown Roseville Class I Trail

I. GENERAL INFORMATION-continued

Sub-Project Type (Select all that apply)

21. Develop a Plan in a Disadvantaged Community (select the type(s) of plan(s) to be developed)
 Bicycle Plan Safe Routes to School Plan Pedestrian Plan
 Active Transportation Plan

(If applying for an Active Transportation Plan- check any of the following plans that your agency already has):

- Bike plan Pedestrian plan Safe Routes to School plan ATP plan

22. Bicycle and/or Pedestrian infrastructure
Bicycle only: Class I Class II Class III
Ped/Other: Sidewalk Crossing Improvement Multi-use facility

Other:

23. Non-Infrastructure (Non SRTS)
24. Recreational Trails*- Trail Acquisition
- *Please see additional Recreational Trails instructions before proceeding**
25. Safe routes to school- Infrastructure Non-Infrastructure

If SRTS is selected, provide the following information

| |
|-------------------------------------|
| 26. SCHOOL NAME & ADDRESS: |
| 27. SCHOOL DISTRICT NAME & ADDRESS: |

| | | |
|--|--|--|
| 28. County-District-School Code (CDS) | 29. Total Student Enrollment | 30. Percentage of students eligible for free or reduced meal programs ** |
| 31. Percentage of students that currently walk or bike to school | 32. Approximate # of students living along school route proposed for improvement | 33. Project distance from primary or middle school |

**Refer to the California Department of Education website: <http://www.cde.ca.gov/ds/sh/cw/filesafdc.asp>

- Click here if the project involves more than one school; attach the remaining school information including school official signature and person to contact, if different, on a separate page

Project name: City of Roseville - Downtown Roseville Class I Trail

V. PROJECT PROGRAMMING REQUEST

Applicant must complete a Project Programming Request (PPR) and attach it as part of this application. The PPR and can be found at http://www.dot.ca.gov/hq/transprog/allocation/ppr_new_projects_9-12-13.xls

PPR Instructions can be found at <http://www.dot.ca.gov/hq/transprog/ocip/2012stip.htm>

Notes:

- Fund No. 1 must represent ATP funding being requested for program years 2014/2015 and 2015/2016 only.
- Non-infrastructure project funding must be identified as Con and indicated as “Non-infrastructure” in the Notes box of the Proposed Cost and Proposed Funding tables.
- Match funds must be identified as such in the Proposed Funding tables.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

General Instructions

| | | | | | | |
|--|----------------------------|-------------------|---------------|--|------------------|-------------------------------------|
| <input checked="" type="checkbox"/> New Project | | | | | Date: | 5/20/14 |
| District | EA | Project ID | PPNO | MPO ID | TCRP No. | |
| 03 | | | | PLA25469 | | |
| County | Route/Corridor | PM Bk | PM Ahd | Project Sponsor/Lead Agency | | |
| PLA | N/A | | | City Of Roseville | | |
| | | | | MPO | Element | |
| | | | | SACOG | Local Assistance | |
| Project Manager/Contact | | Phone | | E-mail Address | | |
| Mike Dour | | (916) 746-1304 | | mdour@roseville.ca.us | | |
| Project Title | | | | | | |
| City of Roseville Downtown Roseville Class I Trail , From Lincoln Street to Douglas Blvd | | | | | | |
| Location, Project Limits, Description, Scope of Work | | | | | | <input type="checkbox"/> See page 2 |
| The project area runs along the Dry Creek corridor and starts at a point just west of Lincoln Street and terminates At Douglas Blvd.The proposed project includes the following project components: 1. Construction of a Class I bike trail along Dry Creek from just west of Lincoln Street to Royer Park. 2. Removal and relocation of the existing Ice House Bridge. 3. Construction of a new bridge from the Royer Park bike trail to the transit stop in front of the Downtown Roseville Library on Taylor Street (referred to as the "Library Bridge"). | | | | | | |
| <input checked="" type="checkbox"/> Includes ADA Improvements <input checked="" type="checkbox"/> Includes Bike/Ped Improvements | | | | | | |
| Component | Implementing Agency | | | | | |
| PA&ED | City of Roseville | | | | | |
| PS&E | City of Roseville | | | | | |
| Right of Way | City of Roseville | | | | | |
| Construction | City of Roseville | | | | | |
| Purpose and Need | | | | | | <input type="checkbox"/> See page 2 |
| The purpose of the project is to improve conductivity and pedestrian/bicycle access between the Vernon Street commercial area, Royer Park and the upstream Harding to Royer Bike Trail. To accomplish this purpose, the City will extend the bike trail from its terminus at the Oak Street parking lot to the existing trail in Royer Park, relocate the existing Ice House Bridge, and construct a new pedestrian bridge to connect Royer Park to the Downtown Roseville Library. | | | | | | |
| Project Benefits | | | | | | <input type="checkbox"/> See page 2 |
| The project includes a series of safety benefits in order to reduce the rate of pedestrian and bicycle accidents , as well as providing a safe and attractive facility for bicycling, jogging and walking. | | | | | | |
| <input checked="" type="checkbox"/> Supports Sustainable Communities Strategy (SCS) Goals <input checked="" type="checkbox"/> Reduces Greenhouse Gas Emissions | | | | | | |
| Project Milestone | | | | | | Proposed |
| Project Study Report Approved | | | | | | 06/01/06 |
| Begin Environmental (PA&ED) Phase | | | | | | 08/01/06 |
| Circulate Draft Environmental Document | | | | Document Type | EIR | 02/01/09 |
| Draft Project Report | | | | | | 02/01/09 |
| End Environmental Phase (PA&ED Milestone) | | | | | | 06/01/09 |
| Begin Design (PS&E) Phase | | | | | | 04/01/14 |
| End Design Phase (Ready to List for Advertisement Milestone) | | | | | | 04/01/15 |
| Begin Right of Way Phase | | | | | | 02/01/15 |
| End Right of Way Phase (Right of Way Certification Milestone) | | | | | | 04/01/15 |
| Begin Construction Phase (Contract Award Milestone) | | | | | | 07/01/15 |
| End Construction Phase (Construction Contract Acceptance Milestone) | | | | | | 10/01/15 |
| Begin Closeout Phase | | | | | | 10/01/15 |
| End Closeout Phase (Closeout Report) | | | | | | 12/01/15 |

ADA Notice

For individuals with sensory disabilities, this document is available in alternate formats. For information call (916) 654-6410 or TDD (916) 654-3880 or write Records and Forms Management, 1120 N Street, MS-89, Sacramento, CA 95814.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

Date: 5/20/14

| District | County | Route | EA | Project ID | PPNO | TCRP No. |
|--|--------|-------|----|------------|------|----------|
| 03 | PLA | N/A | | | | |
| Project Title: City of Roseville Downtown Roseville Class I Trail , From Lincoln Street to Douglas Blvd | | | | | | |

| Proposed Total Project Cost (\$1,000s) | | | | | | | | | Notes |
|--|-------|--------------|-------|-------|-------|-------|--------|--------------|-------|
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | 2,547 | | | | | | 2,547 | |
| TOTAL | | 2,547 | | | | | | 2,547 | |

| Fund No. 1: | ATP | | | | | | | | Program Code |
|-----------------------------|-------|--------------|-------|-------|-------|-------|--------|--------------|----------------|
| Proposed Funding (\$1,000s) | | | | | | | | | Funding Agency |
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | 1,236 | | | | | | 1,236 | |
| TOTAL | | 1,236 | | | | | | 1,236 | |

| Fund No. 2: | Local (Matching ATP) | | | | | | | | Program Code |
|-----------------------------|----------------------|------------|-------|-------|-------|-------|--------|------------|----------------|
| Proposed Funding (\$1,000s) | | | | | | | | | Funding Agency |
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | 160 | | | | | | 160 | |
| TOTAL | | 160 | | | | | | 160 | |

| Fund No. 3: | CMAQ | | | | | | | | Program Code |
|-----------------------------|-------|------------|-------|-------|-------|-------|--------|------------|----------------|
| Proposed Funding (\$1,000s) | | | | | | | | | Funding Agency |
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | 501 | | | | | | 501 | |
| TOTAL | | 501 | | | | | | 501 | |

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

Date: 5/20/14

| District | County | Route | EA | Project ID | PPNO | TCRP No. |
|--|--------|-------|----|------------|------|----------|
| 03 | PLA | N/A | | | | |
| Project Title: City of Roseville Downtown Roseville Class I Trail , From Lincoln Street to Douglas Blvd | | | | | | |

| Fund No. 4: | | BTA | | | | | | | Program Code |
|-----------------------------|-------|-------|-------|-------|-------|-------|--------|-------|----------------|
| Proposed Funding (\$1,000s) | | | | | | | | | |
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | Funding Agency |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | | 103 | | | | | 103 | |
| TOTAL | | | 103 | | | | | 103 | |

| Fund No. 5: | | TDA | | | | | | | Program Code |
|-----------------------------|-------|-------|-------|-------|-------|-------|--------|-------|----------------|
| Proposed Funding (\$1,000s) | | | | | | | | | |
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | Funding Agency |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | | 24 | | | | | 24 | |
| TOTAL | | | 24 | | | | | 24 | |

| Fund No. 6: | | FTA | | | | | | | Program Code |
|-----------------------------|-------|-------|-------|-------|-------|-------|--------|-------|----------------|
| Proposed Funding (\$1,000s) | | | | | | | | | |
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | Funding Agency |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | | 465 | | | | | 465 | |
| TOTAL | | | 465 | | | | | 465 | |

| Fund No. 7: | | Local (Other) | | | | | | | Program Code |
|-----------------------------|-------|---------------|-------|-------|-------|-------|--------|-------|----------------|
| Proposed Funding (\$1,000s) | | | | | | | | | |
| Component | Prior | 14/15 | 15/16 | 16/17 | 17/18 | 18/19 | 19/20+ | Total | Funding Agency |
| E&P (PA&ED) | | | | | | | | | |
| PS&E | | | | | | | | | |
| R/W SUP (CT) | | | | | | | | | |
| CON SUP (CT) | | | | | | | | | |
| R/W | | | | | | | | | |
| CON | | | 58 | | | | | 58 | |
| TOTAL | | | 58 | | | | | 58 | |

Project name: City of Roseville - Downtown Roseville Class I Trail

VI. ADDITIONAL INFORMATION

Only fill in those fields that are applicable to your project

FUNDING SUMMARY

ATP Funds being requested by Phase (to the nearest \$1000)

Amount

| | | |
|---------------------------------------|-----------|------------------|
| PE Phase (includes PA&ED and PS&E) | \$ | 0 |
| Right-of-Way Phase | \$ | 0 |
| Construction Phase-Infrastructure | \$ | 1,236,000 |
| Construction Phase-Non-infrastructure | \$ | 0 |
| Total for ALL Phases | \$ | 1,236,000 |

All Non-ATP fund types on this project* (to the nearest \$1000)

Amount

| | | |
|----------------------------|----|---------|
| Local (Matching ATP Funds) | \$ | 160,000 |
| CMAQ | \$ | 501,000 |
| FTA | \$ | 465,000 |
| TDA | \$ | 24,000 |
| Local | \$ | 58,000 |
| BTA | \$ | 103,000 |

*Must indicate which funds are matching

| | | |
|-------------------------|-----|-----------|
| Total Project Cost | \$ | 2,547,000 |
| Project is Fully Funded | Yes | |

ATP Work Specific Funding Breakdown (to the nearest \$1000)

Amount

| | | |
|--|----|--|
| Request for funding a Plan | \$ | |
| Request for Safe Routes to Schools Infrastructure work | \$ | |
| Request for Safe Routes to Schools Non-Infrastructure work | \$ | |
| Request for other Non-Infrastructure work (non-SRTS) | \$ | |
| Request for Recreational Trails work | \$ | |

ALLOCATION/AUTHORIZATION REQUESTS SCHEDULE

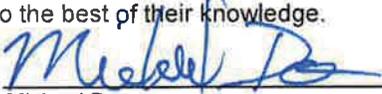
| | Proposed Allocation Date | Proposed Authorization (E-76) Date |
|--------------|--------------------------|------------------------------------|
| PA&ED or E&P | | |
| PS&E | | |
| Right-of-Way | | |
| Construction | 04/29/2015 | 05/26/2015 |

All project costs MUST be accounted for on this form, including elements of the overall project that will be, or have been funded by other sources.

Project name: City of Roseville - Downtown Roseville Class I Trail

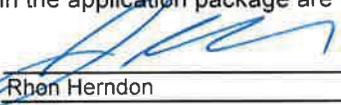
VIII. APPLICATION SIGNATURES

Applicant: The undersigned affirms that the statements contained in the application package are true and complete to the best of their knowledge.

Signature: 
Name: Michael Dour
Title: Alternative Transportation Analyst

Date: 05/20/2014
Phone: 916-746-1304
e-mail: mdour@roseville.ca.us

Local Agency Official (City Engineer or Public Works Director): The undersigned affirms that the statements contained in the application package are true and complete to the best of their knowledge.

Signature: 
Name: Rben Herndon
Title: Public Works Director

Date: 05/20/2014
Phone: 916-774-5331
e-mail: rherndon@roseville.ca.us

School Official: The undersigned affirms that the school(s) benefited by this application is not on a school closure list.

Signature: _____
Name: _____
Title: _____

Date: _____
Phone: _____
e-mail: _____

Person to contact for questions:

Name: _____
Title: _____

Phone: _____
e-mail: _____

Caltrans District Traffic Operations Office Approval*

If the application's project proposes improvements on a freeway or state highway that affects the safety or operations of the facility, it is required that the proposed improvements be reviewed by the district traffic operations office and either a letter of support or acknowledgement from the traffic operations office be attached () or the signature of the traffic personnel be secured below.

Signature: _____
Name: _____
Title: _____

Date: _____
Phone: _____
e-mail: _____

*Contact the District Local Assistance Engineer (DLAE) for the project to get Caltrans Traffic Ops contact information. DLAE contact information can be found at <http://www.dot.ca.gov/hq/LocalPrograms/dlae.htm>

Project name:

City of Roseville - Downtown Roseville Class I Trail

VIII. ADDITIONAL APPLICATION ATTACHMENTS

Check all attachments included with this application.

- Vicinity/Location Map- **REQUIRED for all IF Projects**
 - North Arrow
 - Label street names and highway route numbers
 - Scale

- Photos and/or Video of Existing Location- **REQUIRED for all IF Projects**
 - Minimum of one labeled color photo of the existing project location
 - Minimum photo size 3 x 5 inches
 - Optional video and/or time-lapse

- Preliminary Plans- **REQUIRED for Construction phase only**
 - Must include a north arrow
 - Label the scale of the drawing
 - Typical Cross sections where applicable with property or right-of-way lines
 - Label street names, highway route numbers and easements

- Detailed Engineer's Estimate- **REQUIRED for Construction phase only**
 - Estimate must be true and accurate. Applicant is responsible for verifying costs prior to submittal
 - Must show a breakdown of all bid items by unit and cost. Lump Sum may only be used per industry standards
 - Must identify all items that ATP will be funding
 - Contingency is limited to 10% of funds being requested
 - Evaluation required under the ATP guidelines is not a reimbursable item

- Documentation of the partnering maintenance agreement- Required with the application if an entity, other than the applicant, is going to assume responsibility for the operation and maintenance of the facility

- Documentation of the partnering implementation agreement-Required with the application if an entity, other than the applicant, is going to implement the project.

- Letters of Support from Caltrans (Required for projects on the State Highway System(SHS))

- Digital copy of or an online link to an approved plan (bicycle, pedestrian, safe routes to school, active transportation, general, recreation, trails, city/county or regional master plan(s), technical studies, and/or environmental studies (with environmental commitment record or list of mitigation measures), if applicable. Include/highlight portions that are applicable to the proposed project.

- Documentation of the public participation process (required)

- Letter of Support from impacted school- when the school isn't the applicant or partner on the application (required)

- Additional documentation, letters of support, etc (optional)

II. PROJECT INFORMATION

1. Project Location

The **Downtown Roseville Class I Trail** project is located in the City of Roseville, adjacent to Dry Creek in an area bounded by Douglas Blvd, Oak Street, Lincoln Street and Royer Park.

2. Project Coordinates (Decimal degrees)

Latitude
(Decimal degrees)

Longitude

3. Project Description

Purpose and Need

The proposed project will close a gap in the Dry Creek Regional Trail system and remove barriers to bicycle and pedestrian travel to, from and through Downtown Roseville. Closing the gap in the City's longest and most popular Class 1 multi-use trail will significantly improve pedestrian and bicycle connectivity between businesses, homes, schools, transit and the many other civic uses in Downtown Roseville and the surrounding communities along the Miners Ravine/Dry Creek Trail system. The increased connectivity will enhance safety, comfort and convenience for bicyclists and pedestrians and result in more trips on foot and by bicycle.

As part of a separate project and separate grant application, the City will construct the Downtown Pedestrian Bridge to further enhance connectivity in the Downtown Area and build upon this project's significant active transportation benefits.

Project Scope

The proposed project includes the following project components:

1. Construction of a Class I trail along Dry Creek from just west of Lincoln Street to Royer Park, including a connection to the Miners Ravine section of the greater Dry Creek Trail system.

2. Relocation of and physical improvements to the existing Ice House Bridge to facilitate the Class I trail connection across Dry Creek and into Royer Park, where it will connect with the Royer Park section of the greater Dry Creek Trail system.
3. Construction of a new bridge from the Royer Park bike trail to the transit stop in front of the Downtown Roseville Library on Taylor Street (referred to as the “Library Bridge”).

Project Status

Project level CEQA clearance was obtained for the project as part of the Downtown Specific Plan EIR. Technical studies to support NEPA clearance are in progress. Plans have been prepared to the 65% completion level, and construction is anticipated in 2015.

III. SCREENING CRITERIA

1. Demonstrated Needs of the Applicant

The Downtown Roseville Class I Trail project is a key component of the City of Roseville’s efforts to improve pedestrian and bicycle connectivity to, from and through the downtown core. This is part of the City’s larger vision to make Downtown Roseville a regional live-work destination where travel on foot and by bicycle and transit are emphasized. This project will close a gap in the larger Dry Creek Trail system by connecting the existing Miners Ravine and Royer/Saugstad Park Trail segments, and result in a linear and connected paved trail more than 10-miles in length. As a result, the proposed project will significantly increase opportunities for bicycle and pedestrian activity in this active and growing area of the City. There will also be a tie-in to the Dry Creek Greenway Trail (EIR preparation underway), which includes longer regional Class 1 trail connections to the American River Parkway and Sacramento Northern Trails. The area surrounding the project has many amenities and attractions for students, families and other residents. These include the Civic Center, the Vernon Street Town Square, the Roseville Public Library, the Downtown Roseville Post Office, Fire Station No 1, Veterans Hall,

Royer Park, Saugstad Park which includes a commuter bus park-n-ride lot, the Vernon Street Transfer Station, the Roseville Intermodal Transfer Station which includes local bus service, Capitol Corridor commuter train service, Amtrak and Greyhound bus service, and the Miner's Ravine and Royer/Saugstad Park segments of the larger Dry Creek Trail system. As envisioned in the 2009 Downtown Specific Plan, the City of Roseville is planning tremendous growth in this area. The plan proposes 900,000 square feet of additional civic, entertainment, offices, and pedestrian-oriented retail uses in this area, and 1,020 new residential units in the immediate area.

Dry Creek and the busy Douglas Blvd and Lincoln Streets act as physical barriers that impede safe pedestrian and bicycle access. A pedestrian bridge that had provided direct access from Royer Park to the Library was removed in 2011 due to storm damage. In addition, the existing design and alignment of Ice House Bridge, including its approach ramps and railings, compel cyclists to ride against traffic on one-way Park Drive and require cyclists to walk their bikes across the bridge. Finally, the existing bridge alignment has resulted in a gap between the Miners Ravine and the Royer Park segments of the Dry Creek Trail, which limits connectivity for commuters and recreational riders. This grant application will complete funding needed to close the gap in the trail system and provide bicyclists and pedestrians with a convenient, comfortable and safe connection to the many existing and future uses in the Downtown area. This will be accomplished by extending the bike trail from its terminus at the Oak Street parking lot to the existing trail in Royer Park, relocating and upgrading the existing Ice House Bridge, and constructing a new pedestrian bridge to connect Royer Park and the bike trail to the Downtown Roseville Library.

2. Consistency with Regional Transportation Plan (100 words or less)

The PCTPA RTP incorporates the City's Bicycle Master Plan (2008), and the proposed project is identified as a priority in the BMP. The project helps meet the RTP goals of reducing emissions, reducing the rate of collisions between pedestrians/cyclists and vehicles, increasing the mode share of

non-vehicular trips, and providing facilities to encourage walking and bicycling. Additionally, the project is identified in the Caltrans' I-80 Corridor Management Plan as a bicycle route parallel the I-80 corridor.

IV. NARRATIVE QUESTIONS

1. POTENTIAL FOR INCREASED WALKING AND BICYCLING, ESPECIALLY AMONG STUDENTS, INCLUDING THE IDENTIFICATION OF WALKING AND BICYCLING ROUTES TO AND FROM SCHOOLS, TRANSIT FACILITIES, COMMUNITY CENTERS, EMPLOYMENT CENTERS, AND OTHER DESTINATIONS; AND INCLUDING INCREASING AND IMPROVING CONNECTIVITY AND MOBILITY OF NON-MOTORIZED USERS. (0-30 POINTS)

A. Describe how your project encourages increased walking and bicycling, especially among students.

Downtown Roseville offers a number of amenities, activity centers and attractions for students, families, residents, business owners and employees surrounding the project. These amenities are shown in the "Project Location and Local Activity Centers" exhibit in the attachments. However, Dry Creek and busy nearby roads create a barrier to access and restrict connectivity. The proposed project will close the gap in the Dry Creek Regional Trail system and remove barriers to bicycle and pedestrian travel to, from and through Downtown Roseville. A complete description of how this project will connect bicyclists and pedestrians to their destinations is provided below:

Connectivity to Schools – There are 5 schools located within 1 mile of the project area and along the disconnected Miners Ravine and Royer/Saugstad segments of the Dry Creek trail system. These include :

- ***Adelante High School*** (1/4-mile north): With a total enrollment of 137 students, the total minority enrollment is 50%, and 60% of the students were eligible for reduced meal plans in 2012-2013.
- ***Roseville High School*** (3/4 mile north): With a total enrollment of 1993 students, the total minority enrollment is 42%, and 31% of students were eligible for reduced meal plans in 2012-2013.
- ***Heald College*** (1-mile north)
- ***St. Rose Elementary School*** (0.5-miles south).
- ***Cirby Elementary School*** (1 mile south)

The existing gap in the trail system discourages teachers and students from walking or bicycling to these schools. Closing the gap in the trail system will greatly encourage bicycle and pedestrian travel to school, especially to the high schools whose enrollment boundaries extend on both sides of the project area.

Access to Transit – There are a number of local and commuter transit opportunities available in the project area. These include:

- ***Civic Center Transit Transfer Point*** – Located one block of the project site, the transfer point provides connections to Placer County Transit and Sacramento Regional Transit, which connect to local college campuses.
- ***Intermodal Transfer Station*** – Located 2 blocks from the project site, this transfer station includes local bus service, Capitol Corridor commuter train service, Amtrak and Greyhound bus service.
- ***Route L Transit Stop at Downtown Main Library***
- ***Park-N-Ride lot at Saugstad Park*** – Used for commuter bus service to Downtown Roseville. The City recently installed electronic on-demand bike lockers in this park-n-ride lot to facilitate multi-modal trips.

Access to Attractions – There are many activity destinations near the project location. These include:

- ***Roseville Public Library*** – Located at Taylor Street and Royer Street, the Public Library hosts programs for tots, youths and teens, including story times, playgroups and homework help hours. The library is an important facility for many students in the area. This project will increase student mobility and independence and increase their direct access to the facility without having to use the automobile as their transit.
- ***Vernon Street District*** – Considered to be Roseville’s Civic Core, Vernon Street is a destination for the arts, entertainment, dining and family outings, and includes the Civic Center and Town Square. This project will facilitate access to and through the Vernon Street District.

- ***Civic Center and Town Square*** – The Civic Center and Town Square features an open plaza, water feature, fire pit, and shade features, providing a place for live music, farmers markets, yoga classes, and programs for children and families.
- ***Miners Ravine Trail*** – Currently extending approximately 6 miles from the Sierra College Blvd to Folsom Road, the bike and pedestrian trail will be extended in 2015 below Folsom Road, along the bank of Dry Creek, and below Lincoln Street to the Oak Street parking lot.
- ***Dry Creek Greenway Trail*** – This bike and pedestrian trail runs on the southeast bank of Dry Creek, from the southern limit of Royer Park, underneath Douglas Blvd, through Saugstad Park and south to Darling Way, further along the I-80 Corridor. The northern limit of the trail terminates midway into Royer Park, at Dietrich Drive.
- ***Veterans Hall***– The Veterans Hall is a regional resource and represents a City/County partnership. The Hall provides social space for veteran-related activities such as square dancing, bingo and scouting as well as a place for members to meet and support local/national veterans’ programs,
- ***Royer Park*** – Royer Park is approximately 17 acres in size, and contains a baseball field, soccer field, basketball court, tennis courts, picnic areas, and play areas. During the summer, the popular Music in the Park series draws a crowd on Tuesday nights.
- ***Saugstad Park*** – Saugstad Park has approximately 10 acres of undeveloped open area and 15 acres of developed park, containing baseball diamonds, picnic areas, play areas and an off-leash dog park.

These amenities are surrounded by residences, but Dry Creek and the busy Douglas Blvd and Lincoln Streets act as physical barriers that impede safe pedestrian and bicycle access throughout this area. A pedestrian bridge that had provided direct access from Royer Park to the Library was removed in 2011 due to storm damage. The existing alignment of Ice House Bridge provides access from the north side of Veteran’s Hall across the creek to the Oak Street parking lot, however the alignment of the approach

ramp, railing heights and clearances require cyclists to walk their bikes across the bridge, and there is no direct connection to either bike trail systems.

The proposed project will close the gap between the Miners Ravine Trail and the Dry Creek Greenway Trail, providing a continuous 10-mile long Class I Trail from Sierra College Blvd to Darling Way. The realignment of Ice House Bridge will provide direct access of the trail across the creek, and the bridge will be retrofitted with railing and overhead clearances appropriate for cycling. The proposed Library Bridge will provide direct access between Royer and Saugstad Parks, the adjacent residences, and the bike trail to the Public Library on the northwest side of the creek. Both bridges will provide access between the neighborhoods, parks and downtown attractions along an attractive and safe facility.

B. Describe the number and type of possible users and their destinations, and the anticipated percentage increase in users upon completion of your project. Data collection methods should be described.

Anticipated types of users are assumed to include:

- Students accessing Adelante and Roseville High Schools, Heald College and St. Rose Private School
- Students and employees accessing the Roseville Transit at the Civic Center Transfer Point, the Saugstad Park park-n-ride lot and the Roseville Intermodal Transfer Station
- Families and children accessing the parks, Library, and Town Square by foot or bike
- Commuters travelling between home and workplaces between the Vernon Street District of the Downtown and all other areas linked to existing and planned bike trails to the north and south
- Residents accessing the retail, restaurants and theaters along Vernon Street
- Joggers, walkers, and cyclists for recreational and exercise purposes

The City conducted a significant public outreach program in the most recent Bicycle Master Plan update, which showed that most residents find bicycle travel along arterial roadways uncomfortable due to traffic speeds and volumes. Class I bike paths are preferred by residents, for both recreational

purposes and for commuters to avoid arterial roadways. Bicyclists also expressed a desire to have enhanced connections to multi-modal facilities.

The City of Roseville's Bicycle Master Plan (BMP) (2008) estimated that the mode split of residents who bike to work is 0.4%, and 0.9% of residents who walk to work (Source: Census 2000 Journey to Work). The document estimated that implementation of the measures identified in the BMP would result in a doubling of the cycling rates for commuting to the statewide average of 0.8%.

A Pedestrian Consumer Survey was conducted at a public workshop held in the development of the City of Roseville's Pedestrian Master Plan in 2008. The respondents' average trip time on foot was 31 minutes. Walking for pleasure was the most popular response for trip purpose (50%), followed by health and exercise (37%). Other purposes included walking to school, church or civic activities (16%) and walking to work (16%). Walkways in good condition and reasonable crossing times were cited as the most significant factors for walking.

Conditions cited in the Pedestrian Consumer Survey that discourage walking included a fear of motor vehicles, difficult street crossings, and routes that take too long to reach a destination. The Census 2000 Journey to Work study showed that at 0.4%, the percentage of residents who walk to work is less than a third of the statewide average, which indicates that there is a great potential for increase by providing the safety comfort and convenience of an inter-connected Class I Trail system.

The project will provide a continuous trail and two pedestrian/bicycle crossings over the creek, separate from the busy roadways, which will address the documented concerns of pedestrians and bicyclists and encourage an increase in users. It is the City's policy to conduct data collection to document the effectiveness of the transportation system. The City will collect counts at both ends of the project before and after construction.

- C. Describe how this project improves walking and bicycling routes to and from, connects to, or is part of a school or school facility, transit facility, community center, employment center, state or national trail system, points of interest, and/or park.**

As described previously, the project will connect to the existing trail system which will provide connectivity to many amenities. The “Project Location and Local Activity Centers” exhibit shows the existing trails to which it connects, and key activity centers that are accessible from the trail network.

These include:

- ***Transit Facilities*** - The Civic Center Transit Transfer Point, Intermodal Transfer Station
- ***Public amenities*** - Downtown Library, the Civic Center, Town Square, Post Office, Veteran’s Hall
- ***Vernon Street District*** - including Surewest offices and City of Roseville offices as larger employers in the Downtown
- ***Local parks*** - Royer Park, Saugstad Park, open space along Miners Ravine Trail
- ***Trails*** - Miner’s Ravine Trail, Dry Creek Greenway Trail, Connection to American River Parkway, Sacramento Northern, creating regionally, state and nationally significant looped trail. This trail system is also included in the California Cross State Bicycle Route Study.
- ***Schools*** – St. Rose Elementary School, Adelante High School, Roseville High School

With the project, the connection to the Miner’s Ravine Trail will accommodate commuters, shoppers and diners from the residential areas south of Douglas Blvd and near Vernon Street who wish to ride their bikes to the large shopping and employment district along Lead Hill Blvd.

D. Describe how this project increases and/or improves connectivity, removes a barrier to mobility and/or closes a gap in a non-motorized facility.

As described above, the proposed project will extend a new segment of bike trail to provide connectivity for students and low income neighborhoods services and facilities accessible from the Class I trails system along Miners Ravine and Dry Creek, which have important local and regional connections. It will also improve connectivity to transit (Roseville Transit, Capitol Corridor and

Amtrak) services available in the Downtown, which provide important regional and inter-regional connections to the Sacramento and Bay Areas.

The area has a plethora of amenities for residents to use, but with the existing gap in the bike trail network, Dry Creek and the existing busy roadways create a barrier for pedestrians and bicyclists. As discussed further in Question 2, the project will close the gap in the trail network and provide crossings of the creek that will allow residents to access the local amenities with less need to traverse busy streets and intersections.

2. POTENTIAL FOR REDUCING THE NUMBER AND/OR RATE OF PEDESTRIAN AND BICYCLIST FATALITIES AND INJURIES, INCLUDING THE IDENTIFICATION OF SAFETY HAZARDS FOR PEDESTRIANS AND BICYCLISTS. (0-25 POINTS)

A. Describe the potential of the project to reduce pedestrian and/or bicycle injuries or fatalities.

Although the streets within the downtown core have relatively low speeds, they have proven difficult to negotiate by bicyclists due to higher traffic volumes and the unique configuration and layout of some streets and intersections. Oak Street has an existing Average Daily Traffic (ADT) of 18,000 and projected ADT of 27,000 and Lincoln Street has an ADT of 12,000 existing (13,500 planned). As discussed further in Part C, there were 16 documented collisions involving pedestrians or bicyclists on streets adjacent to the project site between the dates of February 2009 to February 2014. The only option to cross Dry Creek without navigating the busy roadways is to use the existing Ice House Bridge, which has significant limitations for bicycling, including substandard railings, narrow entry with 90-degree turns, and wood decking.

This project will close the gap in the trail network and provide two Class I crossings of Dry Creek, which will reduce the need for pedestrians and bicyclists to use the busy Lincoln Street, Oak Street and Douglas Blvd. The relocation and rehabilitation of Ice House Bridge will address the deficiencies with the existing structure to accommodate cyclists.

B. Describe if/how your project will achieve any or all of the following: (1) Reduces speed or volume of motor vehicles, (2) Improves sight distance and visibility, (3) Improves compliance with local traffic laws, (4)

Eliminates behaviors that lead to collisions, (5) Addresses inadequate traffic control devices, (6) Addresses inadequate bicycle facilities, crosswalks or sidewalks

The project provides a direct connection for pedestrian and cyclists across Dry Creek between residential neighborhoods and the downtown attractions, and between parking structures and park activities. Current circulation patterns that cross Dry Creek on the existing Ice House Bridge are less direct, and may encourage cyclists to use the busy Douglas Blvd and Lincoln St facilities.

As an example of a way in which the proposed project will improve safety and accessibility for pedestrians and bicyclists, the following describes the two primary options that pedestrians and bicyclists currently have for travelling between residential neighborhoods south of Douglas Blvd and the Public Library, and how the project will improve the situation:

Existing Option #1: Douglas Blvd at-grade crossing option– Pedestrians and bicyclists must cross 5 lanes of traffic, and travel along Douglas Blvd to cross Dry Creek. Douglas Blvd currently has no marked bike lanes, so bicyclists either share the lanes with vehicles, or ride along the 5-foot sidewalk with pedestrians. The sidewalk terminates south of the Dry Creek bridge, at which point pedestrians and bicyclists may travel within parking lots to Judah St, and continue on to Royer St. This route is approximately 1340 feet in length.



Figure 1: Pedestrians and cyclists crossing Douglas Blvd at-grade must cross 5 lanes of traffic.

Existing Option #2: Douglas Blvd grade-separated crossing – Cyclists wishing to avoid travelling along Douglas Blvd to cross the creek can use the grade-separated trail crossing below the

roadway to Royer Park. The Class I trail ends at a maintenance road, at which point users have to share the road with vehicles on Dietrich Drive, ride through a parking lot, travel against the flow of the one-way Park Drive and ride over a curb and narrow sidewalk to access the existing Ice House Bridge ramp. Bicycling is not permitted on Ice House Bridge, due to narrow 90-degree turns on the landings, overhead clearance, insufficient railing height, and uneven wood deck. Accordingly, cyclists are asked to walk their bikes across the bridge. Cyclists and pedestrians would then travel along the busy Oak Street to the library. The length of this route is approximately 3020 feet, which is a third of a mile longer than the route along Douglas Blvd.



Figure 2 - Cyclists accessing existing Ice House Bridge must ride against traffic on the one-way Park Drive, and cross over curb and sidewalk.

Proposed Improvements – The proposed Library Bridge will provide direct access from the Class I trail in Royer Park across Dry Creek to the library. The length of this route is approximately 810 feet – 40% shorter than the Douglas Blvd at-grade route. For users wishing to continue towards the downtown area, the project will convert Dietrich Drive to a pedestrian and cyclist path, and the realignment of Ice House Bridge will provide direct access without diverting users to Park Drive. The potential for conflicts between vehicles, pedestrians and bicyclists can be reduced or eliminated along

Dietrich Drive, the Royer Park/Veterans Hall parking lot, Park Drive, and the Douglas Blvd and Lincoln St bridges over Dry Creek.

C. Describe the location’s history of events and the source(s) of data used (e.g. collision reports, community observation, surveys, audits) if data is not available include a description of safety hazard(s) and photos.

A City of Roseville Collision Report Summary and the Statewide Integrated Traffic Record System identify 13 reported collisions between motorized vehicles and bicyclists and 3 collisions between vehicles and pedestrians on streets adjacent to the project site between the dates of February 2009 to February 2014.

Figure 3 shows collisions involving motor vehicles, pedestrians and bicyclists near the project location between 2009 and 2014. Out of the 13 documented incidents between vehicles and bicyclists, the types of collisions were as follows:

- Broadside – 7 (47%)
- Other – 3 (20%)
- Sideswipe – 2 (13%)
- Unknown – 2 (13%)

By providing a Class I trail and bridge crossings, the project will help separate cyclists and pedestrians from vehicular traffic, and will reduce the potential for collisions for users.

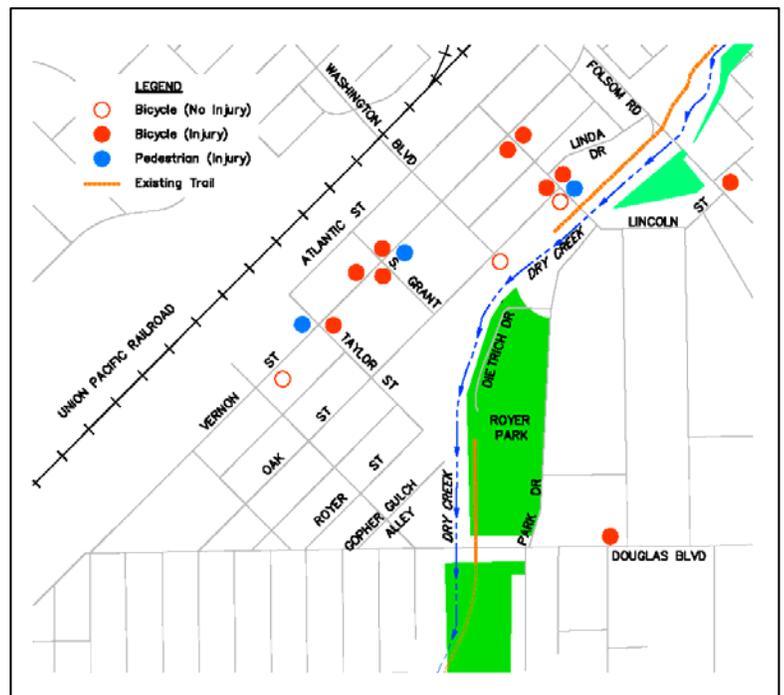


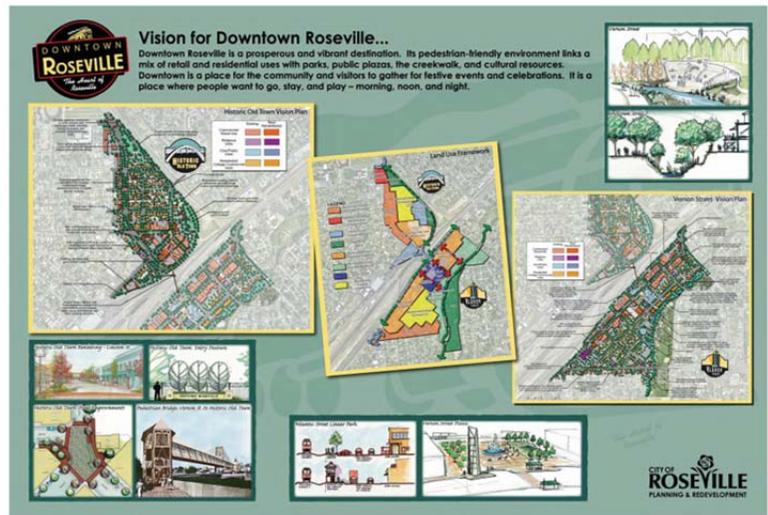
Figure 3 - Collisions involving bicycles and pedestrians

3. PUBLIC PARTICIPATION and PLANNING (0-15 POINTS)

A. Describe the community based public participation process that culminated in the project proposal or plan, such as noticed meetings/public hearings, consultation with stakeholders, etc.

In 2005, the City of Roseville initiated a visioning process for the Downtown area in response to the community's desire to revitalize the area into a vibrant and flourishing destination. The vision was developed through a series of community forums. The first was a **"People and Places" forum**, hosted by

Fred Kent of the nationally-recognized Project for Public Spaces collaborative. At this forum, 70 participants were provided with a presentation about the elements that make for a great park or place, and then were broken into groups to identify existing issues and opportunities for solutions. These opportunities were



consolidated into a conceptual plan. The second was a **walking audit**, which was led by Dan Burden, who is currently the Executive Director of the Walkable and Livable Communities Institute. A key result of the walking audit was the determination that there was great potential for enhancing walkability in Downtown Roseville, and that a more in-depth analysis of these opportunities was warranted.

From the concepts developed during the "People and Places" forum, the project progressed to a more formal and intense visioning project. The effort to involve the community was completed through a **community immersion process**, during which the public was engaged through a series of workshops and downtown walks. Over 900 citizen hours were invested into the community participation efforts to develop a vision for Downtown Roseville.

In a continued effort to involve the community in the Specific Plan development, twenty **one-on-one interviews** were conducted with individual stakeholders. One of the major concepts that were

expressed during the interviews was a desire for the Plan to create safe, attractive pedestrian, bicycle and vehicular connections between Royer Park, the creek and Vernon Street.

In August 2006, a diverse *Steering Committee* was assembled, comprised of residents, property owners, business owners, appointed officials and other dedicated individuals to refine the key goals, strategies and improvements for the Plan. Through a series of nine public meetings, the Steering Committee finalized the Vision Statement, identified and prioritized needed improvements, and worked with the Roseville Revitalization Committee to develop a phasing and implementation program.

As part of the Vision developed from the community participation, connectivity was identified as a crucial element in the Specific Plan, with pedestrian and bicycle paths alongside Dry Creek to connect residential areas, the two parks and the Vernon Street and Historic Old Town districts. The end result of well over 20 public meetings and significant citizen involvement is the Downtown Specific Plan, adopted by the City Council in 2009. The Specific Plan identified the construction of the Library Bridge, relocation of Ice House Bridge and extension of the Class I Trail to provide pedestrian connectivity to the downtown area.

The Downtown Roseville Class I Trail project was identified as a priority component of the Specific Plan by the City Council. Because the project will lay the foundation for future projects along Dry Creek and Royer Park, the City Council requested a robust public participation process for the aesthetic design of the project. To provide opportunity for the community to have significant input to the direction of the design, the City Council decided to implement a *Design Contest* for the consultant selection. As part of this process, the outreach efforts included an initial *Public Workshop* to assess community opinions, a *Focus Meeting* with Veterans Hall members, a 3-week *Public Review* of the design concepts (during which the City received 100 comments), a second *Public Workshop* with a presentation of design concepts from the competing consultants, and a second *Focus Meeting* with Veterans Hall.

B. Describe the local participation process that resulted in the identification and prioritization of the project:

As discussed above, there was rigorous public involvement to identify improvements to enhance connectivity and livability in the community. This project is identified as a top priority projects in the Downtown Specific Plan Public Improvement Program, which was adopted at a noticed public meeting of the City Council on August 17, 2011.

C. Is the project cost over \$1 Million? Yes

If Yes- is the project Prioritized in an adopted city or county bicycle transportation plan, pedestrian plan, safe routes to school plan, active transportation plan, trail plan, circulation element of a general plan, or other publicly approved plan that incorporated elements of an active transportation plan?
Yes

4. COST EFFECTIVENESS (0-10 POINTS)

A. Describe the alternatives that were considered. Discuss the relative costs and benefits of all the alternatives and explain why the nominated one was chosen.

The alternative proposed with this project was developed and thoroughly vetted as part of the extensive public outreach and visioning process as described in the previous section.

B. Calculate the ratio of the benefits of the project relative to both the total project cost and funds requested (i.e., $\frac{Benefit*}{Total\ Project\ Cost}$ and $\frac{Benefit*}{Program\ Funds\ Requested}$).

The cost-benefit analysis accounts for the increased safety and mobility for non-motorized users, improvement in walking conditions, and reduction in automobile travel. Out of the collisions discussed in the response to Question 2, the 8 pedestrian and cyclist collisions along Vernon Street corridor were considered preventable by a parallel Class I trail. Based on the identified collisions, the Cost-Benefit Calculator from UC Berkeley’s Transportation Injury Mapping System (TIMS) estimates that the Life Benefit for the safety improvements is \$1,102,414.

The benefits for improved walking and cycling conditions and reduction in automobile travel were determined following guidelines within the report “Evaluating Active Transport Benefits and Costs” (Todd Litman). The analysis used values for monetary benefits per “person-mile” for various aspects of the facility improvements and reduction in car usage, as described in the report. The 20-year

life cycle benefit for the improved walking and cycling conditions (as shown in the following table) is \$1,560,375, and the benefit for reduced automobile travel is \$509,760.

| | Daily Increase in Users | Average Trip Length (mi) | Average Benefit per person-mile | Daily Benefit | Yearly Benefit | 20-Year Life Cycle Benefit |
|--|-------------------------|--------------------------|---------------------------------|---------------|----------------|----------------------------|
| Improved Walking and Cycling Conditions | | | | | | |
| User Benefits | 150 | 2.5 | \$0.25 | \$94 | \$34,218.75 | \$684,375 |
| Fitness and Health - Walking | 75 | 2 | \$0.50 | \$75 | \$27,375.00 | \$547,500 |
| Fitness and Health - Cycling | 75 | 3 | \$0.20 | \$45 | \$16,425.00 | \$328,500 |
| Subtotal | | | | | | \$1,560,375 |
| Reduced Automobile Travel | | | | | | |
| Vehicle Cost Savings | 30 | 6 | \$0.25 | \$45 | \$10,800.00 | \$216,000 |
| Congestion Reduction | 30 | 6 | \$0.20 | \$36 | \$8,640.00 | \$172,800 |
| Reduced Barrier Effect | 30 | 6 | \$0.01 | \$2 | \$432.00 | \$8,640 |
| Energy Conservation | 30 | 6 | \$0.03 | \$5 | \$1,296.00 | \$25,920 |
| Pollution Reductions | 30 | 6 | \$0.10 | \$18 | \$4,320.00 | \$86,400 |
| Subtotal | | | | | | \$509,760 |
| These calculations assume: (1) an increase in bicycle commuting by 0.4% of the local population, based on projections from the City of Roseville Bicycle Master Plan, (2) an increase in recreational walkers and bicyclists by 1% of the local population, (3) population of 7500 persons in adjacent neighborhoods, (4) 365 days per year for recreational use, 240 days per year for commuting use. | | | | | | |

| |
|--|
| Total Project Benefits: \$1,102,414 + \$1,560,375 + \$509,760 = \$3,172,549 |
| Total Project Cost: \$2,547,259 |
| Program Funds Requested: \$1,236,218 |
| Benefit/Cost (relative to total project cost) = (\$3,172,549)/(2,547,259) = 1.24 |
| Benefit/Cost (relative to programmed cost) = (\$3,172,549)/(1,236,218) = 2.56 |

5. IMPROVED PUBLIC HEALTH (0-10 points)

A. Describe how the project will improve public health, i.e. through the targeting of populations who have a high risk factor for obesity, physical inactivity, asthma, or other health issues.

The proposed project will complete a connection between two trails, which will provide 10 miles of continuous Class I pathway. This will provide a safe and attractive facility for bicycling, jogging and walking. Walking is a low-impact activity in which a variety of ages and fitness levels may participate. Walking produces myriad benefits, including: improved physical fitness and reduced health care costs, reduced stress, reduced health care costs, and improved mental health, reduced auto trips, resulting in reduced consumption and improved air quality. The Veteran’s Hall is generally used by senior citizens,

who are often at risk for physical inactivity. The project will provide convenient and direct routes to walk along or cross the creek to the Civic Center, retail and restaurants in the Vernon Street District.

The CDC’s Behavioral Risk Factor Surveillance System measures obesity and other health factors. As of 2010, the CDC determined that 35.1% of residents in the Sacramento-Roseville metropolitan area are overweight, 24.0% qualify as obese, and 15.3% participate in no physical activity. The new trail connections and bridges will improve access to activities at the Town Square and in Royer Park which provide a plethora of options for activity and play. Town Square offers fitness boot camps, yoga, and tai chi twice per week, and “Wee One Wednesday” play activities. Royer Park hosts sports teams and day camp activities.

The project has the potential to improve public health for senior citizens and children who have risk for obesity and physical activity, by improving accessibility to and from these facilities offering activities and play, and providing an attractive and safe trail to encourage walking.

6. BENEFIT TO DISADVANTAGED COMMUNITIES (0-10 points)

A. I. Is the project located in a disadvantaged community? Y/N

II. Does the project significantly benefit a disadvantaged community? Y/N

a. Which criteria does the project meet? (Answer all that apply)

- o Median household income for the community benefited by the project: \$61,462
- o California Communities Environmental Health Screen Tool (CalEnvironScreen) score for the community benefited by the project: 20.52
- o For projects that benefit public school students, percentage of students eligible for the Free or Reduced Price Meals Programs: Adelante High School – 60.0%

b. Should the community benefitting from the project be considered disadvantaged based on criteria not specified in the program guidelines? If so, provide data for all criteria above and a quantitative assessment of why the community should be considered disadvantaged.

Although the project is not in a disadvantaged community as defined by ATP guidelines, the project is located in one of the oldest, most underserved and economically disadvantaged areas in Roseville. In communities with few areas with over 51% of low or moderate income residents, the US Department of

Housing and Urban Development authorizes an exception criterion. Section 105(c)(2)(A)(ii) of the Housing and Community Development Act of 1974, as amended, states that an activity shall be considered to principally benefit low and moderate income persons when “the area served by such activity is within the highest quartile of all areas within the jurisdiction of such city or county in terms of the degree of concentration of persons of low and moderate income.” The City of Roseville qualifies as an Exception Grantee, with an exception threshold of 41.36%.

B. Describe how the project demonstrates a clear benefit to a disadvantaged community and what percentage of the project funding will benefit that community, for projects using the school based criteria describe specifically the school students and community will benefit.

The majority of Roseville’s bike trail system is located in newer development areas at the edges of the city. Residents in the older, economically disadvantaged downtown area do not enjoy the same recreation and transportation opportunities afforded by the bike trails in the remainder of the city. The Downtown Roseville Class I Trail project will provide a much-needed trail connection for the underserved Enwood, Folsom Road, Cherry Glen and Theiles Manor neighborhoods. According to census figures, Enwood has 42% of households at low or very low income levels, the Folsom Road neighborhood has 41% of residents at low or very low income levels, and the Cherry Glen and Theiles Manor neighborhoods combined have 62% of residents at low or very low income levels. This qualifies these neighborhoods as “areas of benefit” as defined by the US Department of Housing and Urban Development.

This project will connect lower income residents to schools and employment opportunities in Downtown Roseville, retail areas in northeast Roseville, and offices along Douglas Boulevard, Eureka Road and Stone Point, which will greatly increase opportunities for employment by facilitating transportation and access to transit.

7. USE OF CALIFORNIA CONSERVATION CORPS (CCC) OR A CERTIFIED COMMUNITY CONSERVATION CORPS (0 to -5 points)

A. The applicant has coordinated with the CCC to identify how a state conservation corps can be a partner of the project. Yes

a. Name, e-mail, and phone # of the person contacted and the date the information was submitted to them – Virginia Clark, Virginia.Clark@ccc.ca.gov, submitted on 5/13/2014

B. The applicant has coordinated with a representative from the California Association of Local Conservation Corps (CALCC) to identify how a certified community conservation corps can be a partner of the project. Yes

a. Name, e-mail, and phone # of the person contacted and the date the information was submitted to them – Cynthia Vitale, calocalcorps@gmail.com, (916) 558-1516, submitted on 5/13/2014

C. The applicant intends to utilize the CCC or a certified community conservation corps on all items where participation is indicated? Y/N Yes

I have coordinated with a representative of the CCC; and the following are project items that they are qualified to partner on:

The CCC provided a response that there are components of the application that fits within CCC capabilities. Coordination with the CCC will continue.

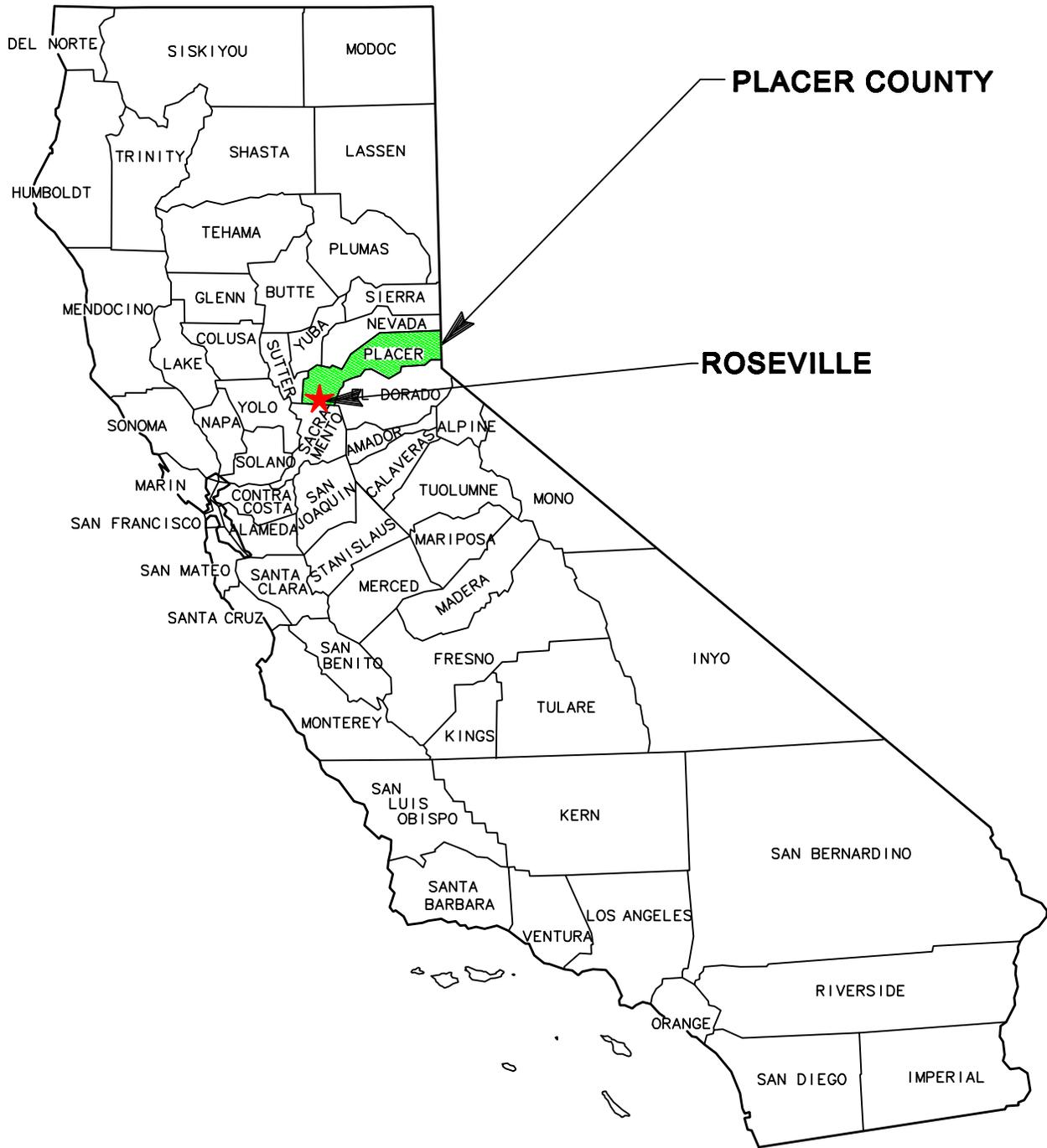
I have coordinated with a representative of the CALCC; and the following are project items that they are qualified to partner on:

The CALCC has not provided a response to the submittal as of this date.

8. APPLICANT'S PERFORMANCE ON PAST GRANTS (0 to -10 points)

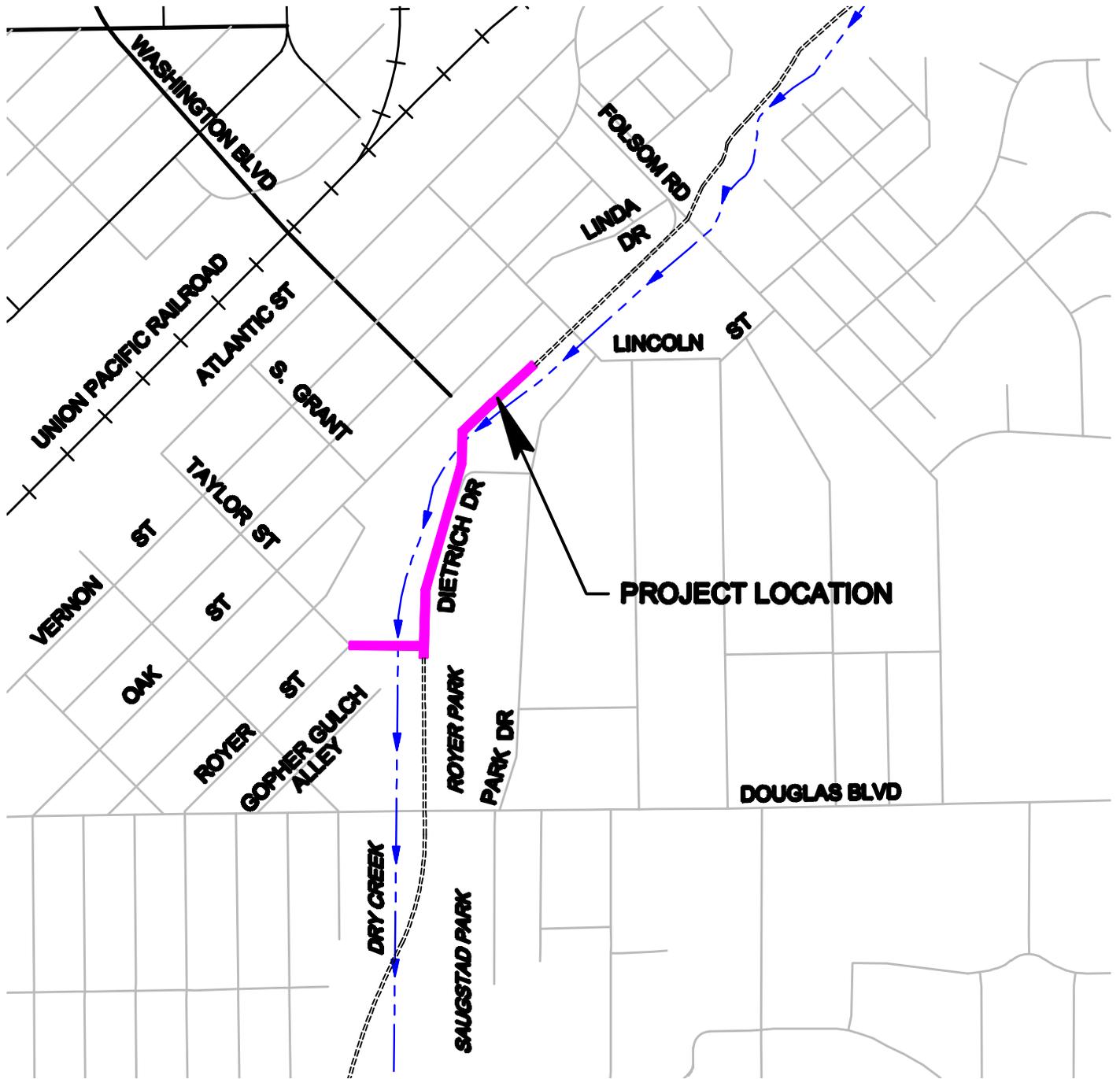
A. Describe any of your agency's ATP type grant failures during the past 5 years, and what changes your agency will take in order to deliver this project.

As noted in the City's single audit reports from Federal Fiscal Years 08/09 through 12/13, over the past 5 years the City of Roseville has submitted federal grant award expenditure requests totaling \$69,188,061. Of this amount, \$22,146,657 has been associated with Caltrans-administered federal grant programs. Other federal grant programs that the City has participated in include: Federal Transit Administration, FEMA, Housing and Urban Development, Homeland Security, and Department of Justice. The City has not experienced any grant failures or any significant audit findings.



DOWNTOWN ROSEVILLE CLASS I TRAIL PROJECT
VICINITY MAP

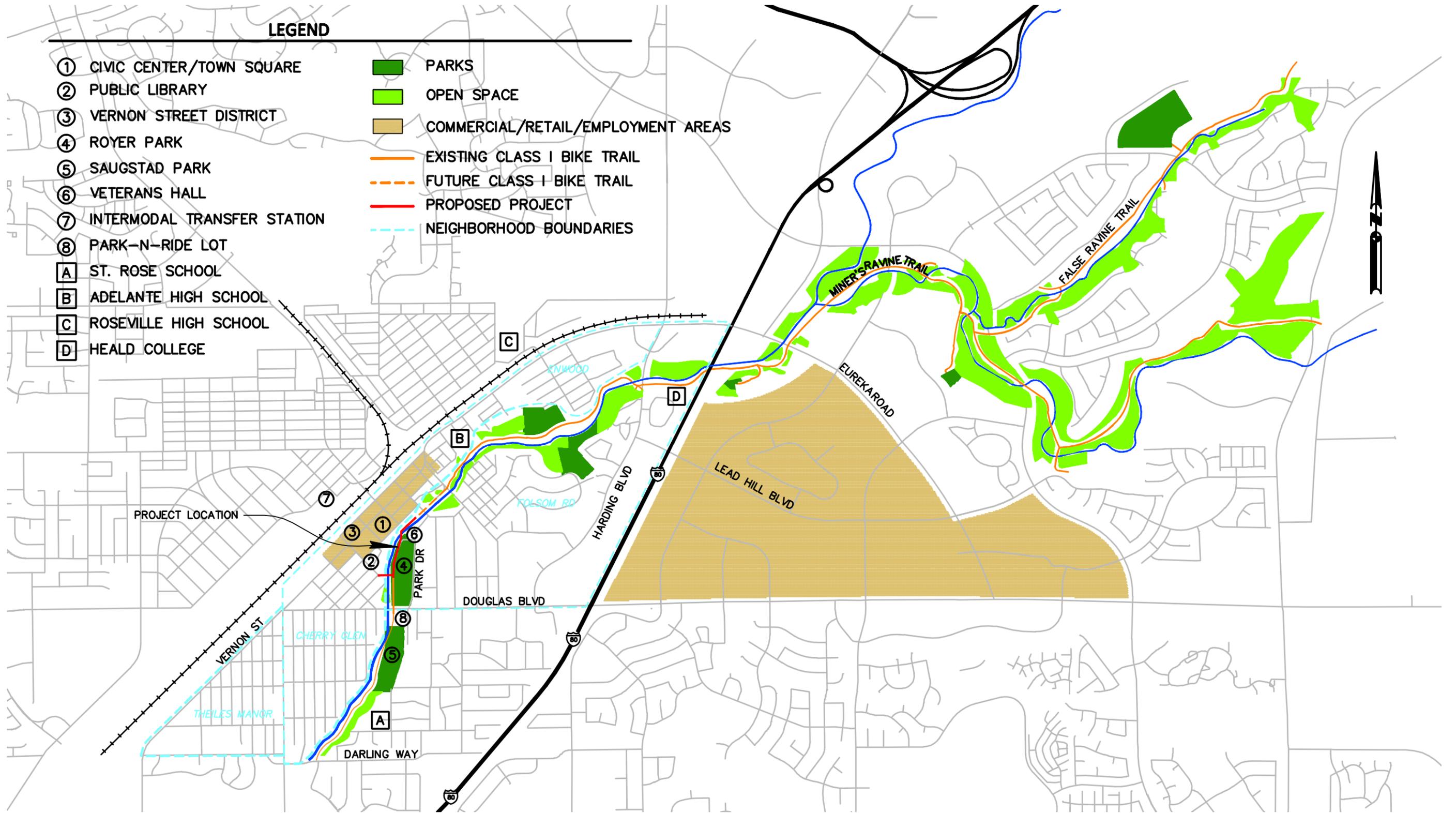




DOWNTOWN ROSEVILLE CLASS I TRAILS PROJECT
VICINITY MAP

LEGEND

- | | |
|--------------------------------|--|
| ① CIVIC CENTER/TOWN SQUARE |  PARKS |
| ② PUBLIC LIBRARY |  OPEN SPACE |
| ③ VERNON STREET DISTRICT |  COMMERCIAL/RETAIL/EMPLOYMENT AREAS |
| ④ ROYER PARK |  EXISTING CLASS I BIKE TRAIL |
| ⑤ SAUGSTAD PARK |  FUTURE CLASS I BIKE TRAIL |
| ⑥ VETERANS HALL |  PROPOSED PROJECT |
| ⑦ INTERMODAL TRANSFER STATION |  NEIGHBORHOOD BOUNDARIES |
| ⑧ PARK-N-RIDE LOT | |
| A ST. ROSE SCHOOL | |
| B ADELANTE HIGH SCHOOL | |
| C ROSEVILLE HIGH SCHOOL | |
| D HEALD COLLEGE | |



SCALE: 1" = 145'



**DOWNTOWN ROSEVILLE CLASS I TRAIL
PROJECT LOCATION AND LOCAL ACTIVITY CENTERS**

 **MARK THOMAS & COMPANY**
7300 FOLSOM BOULEVARD, SUITE 203
SACRAMENTO, CALIFORNIA 95828
(916) 381-9100 FAX: (916) 381-9180



End of the existing Dry Creek Greenway Bike Trail at Royer Park. The project will provide a pedestrian bridge at this location over the creek to the Library, and extend the bike trail to the north.



Proposed location for the Library Bridge.



Children's Art Center, along the proposed bike trail through Royer Park.



Picnic facilities adjacent to proposed bike trail through Royer Park



Bicyclists navigating the Veterans Hall/Royer Park to access the existing Ice House Bridge



The existing Ice House Bridge, which will be realigned with the bike trail

IMPROVEMENT PLANS FOR DOWNTOWN BRIDGES & TRAIL IMPROVEMENT PROJECT CITY JOB # XXXXXX

INDEX OF PLAN SHEETS

| PAGE No. | SHEET NAME | SHEET TITLE |
|----------|------------|------------------|
| 1 | T-1 | TITLE |
| 2-3 | X-1 to X-2 | TYPICAL SECTIONS |
| 4-9 | L-1 to L-6 | LAYOUT |

ABBREVIATIONS:

| | |
|---|--|
| AB - AGGREGATE BASE | JP - JOINT POLE |
| AC - ASPHALT CONCRETE | JT - JOINT TRENCH |
| ARV - AIR RELEASE VALVE | KM - KINDER MORGAN |
| AVE - AVENUE | LIP - LIP OF CUTTER |
| BC - BEGIN CURVE | LOL - LAYOUT LINE |
| BEg - BEGIN | LP - LOW POINT |
| BOC - BACK OF CURB | Lt - LEFT |
| BOF - BOTTOM OF FOOTING | MAX - MAXIMUM |
| BOV - BLOWOFF VALVE | MH - MANHOLE |
| BOW - BACK OF WALK/WALL | MIN - MINIMUM |
| BFP - BACKFLOW PREVENTOR | MUTCD - MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES |
| BLVD - BOULEVARD | No. - NUMBER |
| BVCE - BEGINNING VERTICAL CURVE ELEVATION | OC - ON CENTER |
| BVCS - BEGINNING VERTICAL CURVE STATION | OG - ORIGINAL GROUND |
| CA - CABLE | OH - OVERHEAD UTILITY |
| CB - CATCH BASIN | PCC - PORTLAND CONCRETE CEMENT |
| CG - CURB & GUTTER | PIV - POST INDICATOR VALVE |
| CG&SW - CURB, GUTTER, & SIDEWALK | PG&E - PACIFIC GAS & ELECTRIC |
| CIPP - CURED IN PLACE PIPE | PL - PROPERTY LINE |
| CL - CENTER LINE | PLNTR - PLANTER |
| CLF - CHAIN LINK FENCE | POC - POINT OF CONNECTION |
| CLR - CLEAR/CLEARANCE | PP - POWER POLE |
| CONC - CONCRETE | PUE - PUBLIC UTILITY EASEMENT |
| CST - CITY STANDARDS | PROP - PROPOSED |
| CT - CALTRANS | PVC - POLYVINYL CHLORIDE |
| DFL - DITCH FLOWLINE | PVMT - PAVEMENT |
| DI - DRAIN INLET | R - RADIUS |
| DIP - DUCTILE IRON PIPE | RCP - REINFORCED CONCRETE PIPE |
| DWY - DRIVEWAY | RL - RESTRAINT LENGTH |
| DWG - DRAWING | Rt - RIGHT |
| EB - ELECTRONIC BOX | R/W - RIGHT OF WAY |
| EC - END CURVE | SC - SAWCUT |
| ELEC - ELECTRIC LINE | SD - STORM DRAIN LINE |
| ELL - PIPE ELBOW | SDMH - STORM DRAIN MANHOLE |
| EM - ELECTRIC METER | Shld - SHOULDER |
| EMH - ELECTRIC MANHOLE | SIC - SIGNAL INTERCONNECT |
| EP - EDGE OF PAVEMENT | SL - STREET LIGHT |
| EUD - ENVIRONMENTAL UTILITIES DEPARTMENT | SMT - SURFACE MOUNTED TRANSFORMER |
| EV - ELECTRIC VAULT | SS - SANITARY SEWER LINE |
| EVCE - END VERTICAL CURVE ELEVATION | SSCO - SANITARY SEWER CLEANOUT |
| EVCS - END VERTICAL CURVE STATION | SSMH - SANITARY SEWER MANHOLE |
| EXIST - EXISTING | ST - STREET |
| FC - FACT OF CURB | Std - STANDARD |
| FO - FIBER OPTIC | SW - SIDEWALK |
| FOC - FACE OF CURB | T - TELEPHONE LINE |
| FOD - FACE OF DIKE | TB - TELEPHONE BOX |
| FDC - FIRE DEPARTMENT CONNECTION | TBC - TOP BACK OF CURB |
| FG - FINAL GRADE | TC - TOP OF CURB |
| FH - FIRE HYDRANT | TFC - TOP FACE OF CURB |
| FL - FLOWLINE | TG - TOP OF GRATE |
| FW - FACE OF WALL/FRONT OF WALK | TOF - TOP OF FOOTING |
| G - GAS LINE | TOP - TOP OF PIPE |
| GB - GRADE BREAK | TOW - TOP OF WALL |
| GM - GAS METER | TP - TELEPHONE POLE |
| GP - GUARD POST | TS - TRAFFIC SIGNAL |
| GR - GRATE | TSB - TRAFFIC SIGNAL CONTROL BOX |
| GV - GAS VALVE | TYP - TYPICAL |
| H - HEIGHT | UG - UNDERGROUND UTILITY |
| HMA - HOT MIX ASPHALT | VAR - VARIES |
| HORIZ - HORIZONTAL | VERT - VERTICAL |
| HP - HINGE POINT | VG - VALLEY GUTTER |
| HRC - HANDICAP RAMP | W - WATER MAIN |
| INV - INVERT | WM - WATER METER |
| IRR - IRRIGATION | WMH - WATER MANHOLE |
| | WS - WATER SERVICE |
| | WV - WATER VALVE |
| | YL - YARD LIGHT |



VICINITY MAP
SCALE: NONE

APPROVAL RECOMMENDED BY:

JASON SHYKOWSKI, P.E., DIVISION MANAGER
PUBLIC WORKS - ENGINEERING
CITY OF ROSEVILLE
RCE 58234

APPROVED BY:

KELYE MCKINNEY, ENGINEERING MANAGER
ENVIRONMENTAL UTILITIES
CITY OF ROSEVILLE

APPROVED BY:

RHON L. HERNDON P.E.
PUBLIC WORKS DIRECTOR
CITY OF ROSEVILLE
RCE 50909

SUBMITTED BY:

ROBERT MATTHEW BROGAN RCE 63854
ENGINEER IN RESPONSIBLE CHARGE
MARK THOMAS & COMPANY INC.

| CITY OF ROSEVILLE UTILITY REPRESENTATIVES/CONTACTS | | | |
|---|------------------------|---------------------|--------------|
| UTILITY | REPRESENTATIVES | | PHONE |
| U.S.A. | U.S.A. | | 800-642-2444 |
| TELEPHONE | SUREWEST | TERRY VERSEMAN | 786-1926 |
| FIBER OPTIC | AT&T CALIFORNIA | SCOTT SHELLENBERGER | 435-0031 |
| GAS | PACIFIC GAS & ELECTRIC | ORBIE DAVIS | 530-889-3827 |
| ELECTRIC | ROSEVILLE ELECTRIC | RICK CORRAL | 774-5616 |
| FIRE | ROSEVILLE FIRE DEPT. | PATRICK CHEW | 774-5823 |
| WATER | CITY OF ROSEVILLE | MWAH POLSON | 774-5773 |
| SEWER | CITY OF ROSEVILLE | MWAH POLSON | 774-5773 |
| CABLE T.V. | COMCAST | KIP MILLER | 919-6065 |
| DRAINAGE | CITY OF ROSEVILLE | JERRY DANKBAR | 746-5790 |

T-1

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
| | | | |

| | |
|---|------|
| BENCH MARK | |
| ELEVATION NGVD29 148.78' DATUM | CITY |
| DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 - | |
| A 3 1/4 inch BRASS DISK STAMPED 154796 | |
| - JAN. 1995, SOUTHWEST SIDE OF LINCOLN ST. AT | |
| THE NORTHWEST CORNER OF THE LINCOLN ST. | |
| BRIDGE AT THE ENTRANCE TO THE MUNICIPAL | |
| PARKING LOT AT THE CORNER OF OAK & | |
| LINCOLN. | |

| | |
|--------------|------------|
| DESIGN BY: | MJ |
| DRAWN BY: | MJ/KD |
| CHECKED BY: | ZS/MB |
| SCALE: | NONE |
| DATE: | XX/XX/XXXX |
| PROJECT NO.: | SA-13119 |

| | |
|---------------------|--|
| BY: | |
| 30% SUBMITTAL | |
| PLANS APPROVAL DATE | |



MARK THOMAS & COMPANY, INC.
7300 FOLSOM BOULEVARD, SUITE 203
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CITY OF ROSEVILLE
DEPARTMENT OF PUBLIC WORKS
311 VERNON STREET
ROSEVILLE, CA 95678
(916) 746-1300

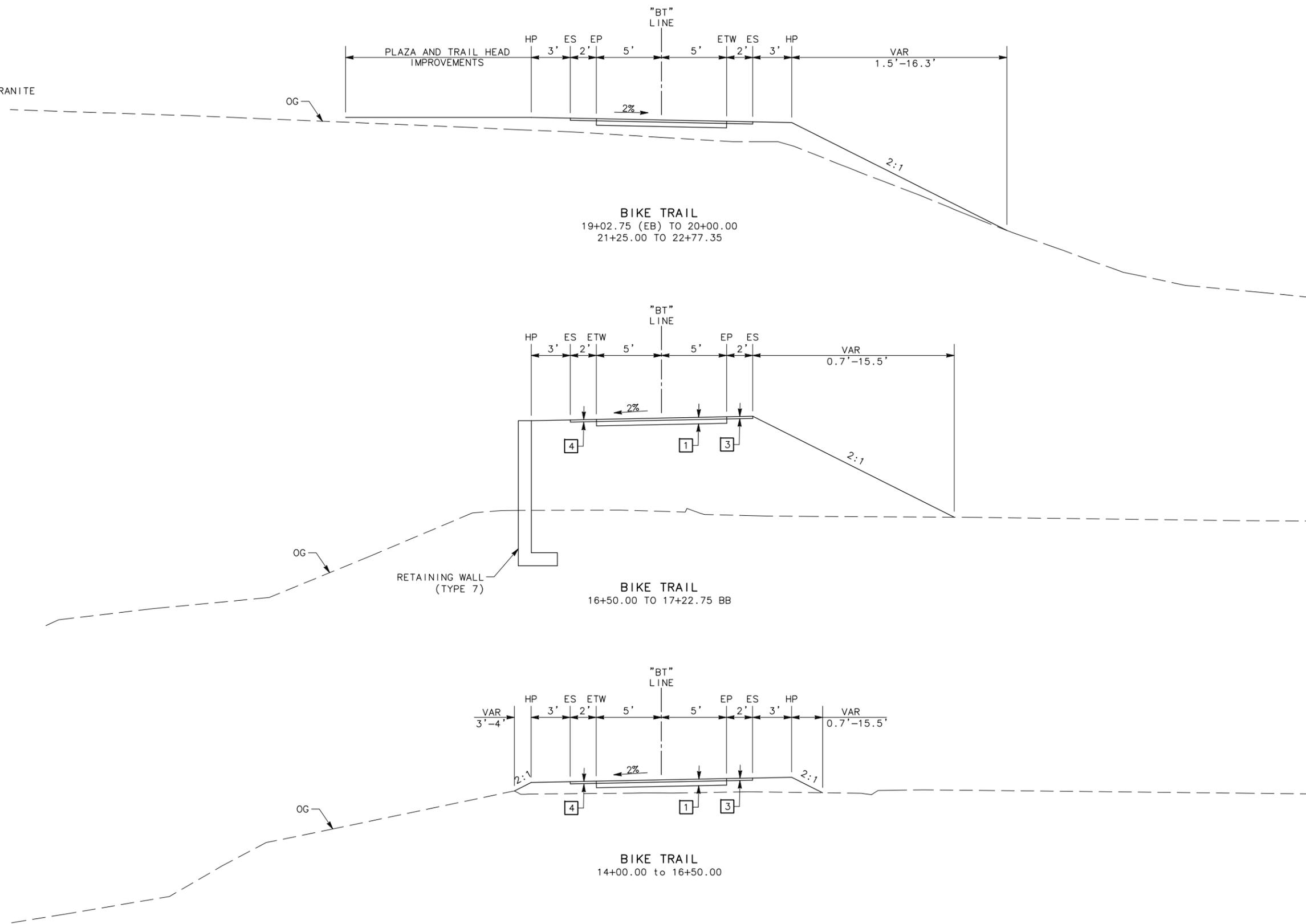


DOWNTOWN BRIDGES AND TRAIL PROJECT
TITLE

| |
|----|
| 1 |
| OF |
| XX |

LEGEND

- 1 2" HMA (TYPE A)
4" AB (CL2)
- 2 6" PCC
6" AB (CL2)
- 3 4" AB (CL2)
- 4 3" DECOMPOSED GRANITE



X-1

| NO. | REVISIONS | BY | DATE |
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BENCH MARK
 ELEVATION NGVD29 148.78' DATUM CITY
 DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 -
 A 3 1/4 inch BRASS DISK STAMPED 154796
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 BRIDGE AT THE ENTRANCE TO THE MUNICIPAL
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DESIGN BY: MJ
 DRAWN BY: MJ/KD
 CHECKED BY: ZS/MB
 SCALE: NONE
 DATE: XX/XX/XXXX
 PROJECT NO: SA-13119

BY:
 30% SUBMITTAL
 PLANS APPROVAL DATE



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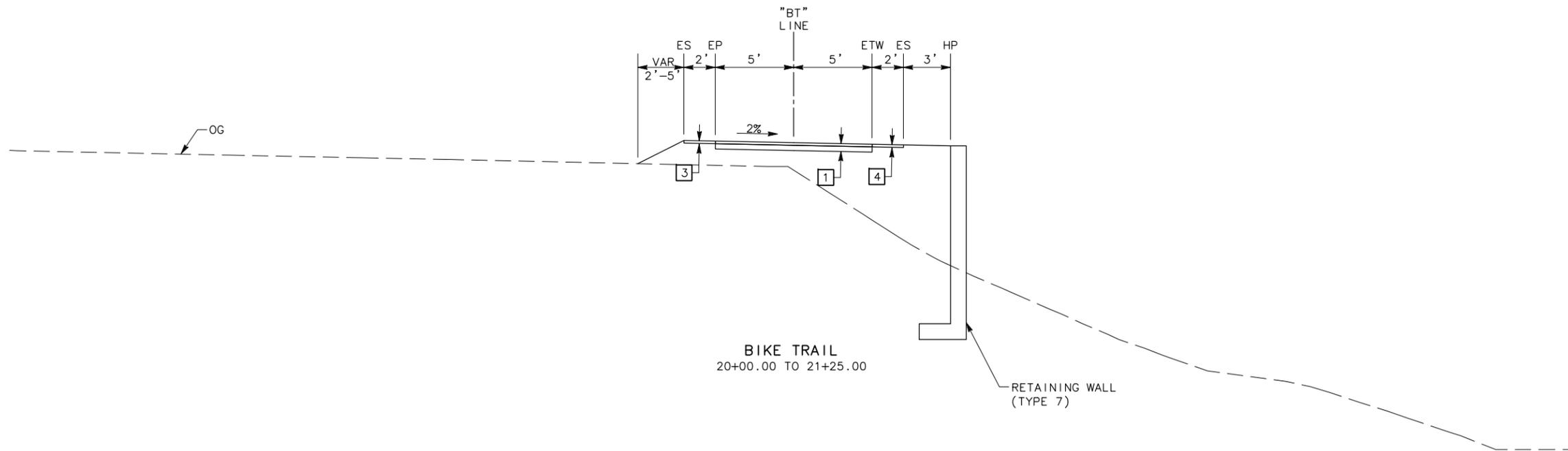
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 DEPARTMENT OF PUBLIC WORKS
 311 VERNON STREET
 ROSEVILLE, CA 95678
 (916) 746-1300



**DOWNTOWN BRIDGES AND
 TRAIL PROJECT
 CROSS SECTION**

LEGEND

- 1 2" HMA (TYPE A)
4" AB (CL2)
- 2 6" PCC
6" AB (CL2)
- 3 4" AB (CL2)
- 4 3" DECOMPOSED GRANITE



X-2

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
| | | | |
| | | | |
| | | | |

BENCH MARK
 ELEVATION NGVD29 148.78' DATUM CITY
 DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 -
 A 3 1/4 inch BRASS DISK STAMPED US4796
 JAN. 1995, SOUTHWEST SIDE OF LINCOLN ST. AT
 THE NORTHWEST CORNER OF THE LINCOLN ST
 BRIDGE AT THE ENTRANCE TO THE MUNICIPAL
 PARKING LOT AT THE CORNER OF OAK &
 LINCOLN.

DESIGN BY: MJ
 DRAWN BY: MJ/KD
 CHECKED BY: ZS/MB
 SCALE: NONE
 DATE: XX/XX/XXXX
 PROJECT NO: SA-13119

BY: _____
 30% SUBMITTAL
 PLANS APPROVAL DATE _____



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CITY OF ROSEVILLE
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 311 VERNON STREET
 ROSEVILLE, CA 95678
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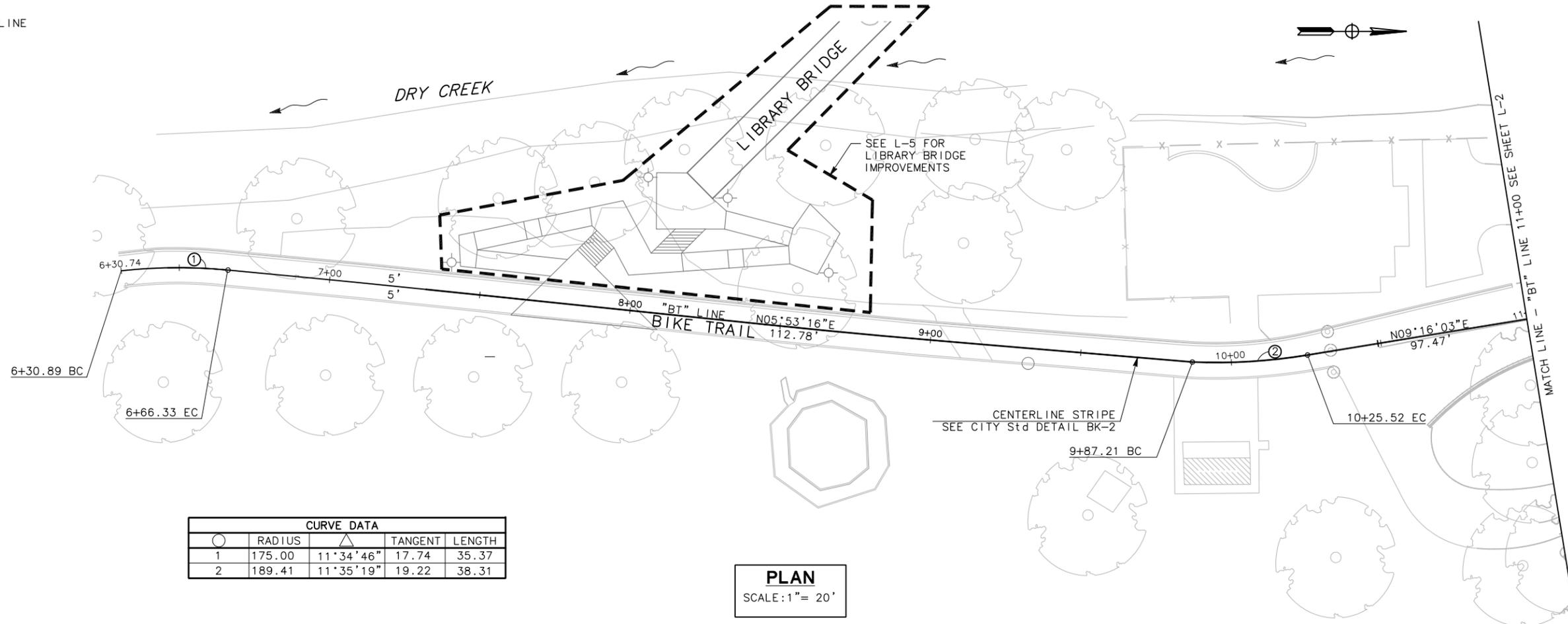


**DOWNTOWN BRIDGES AND
 TRAIL PROJECT
 CROSS SECTION**

3
 OF
 XX

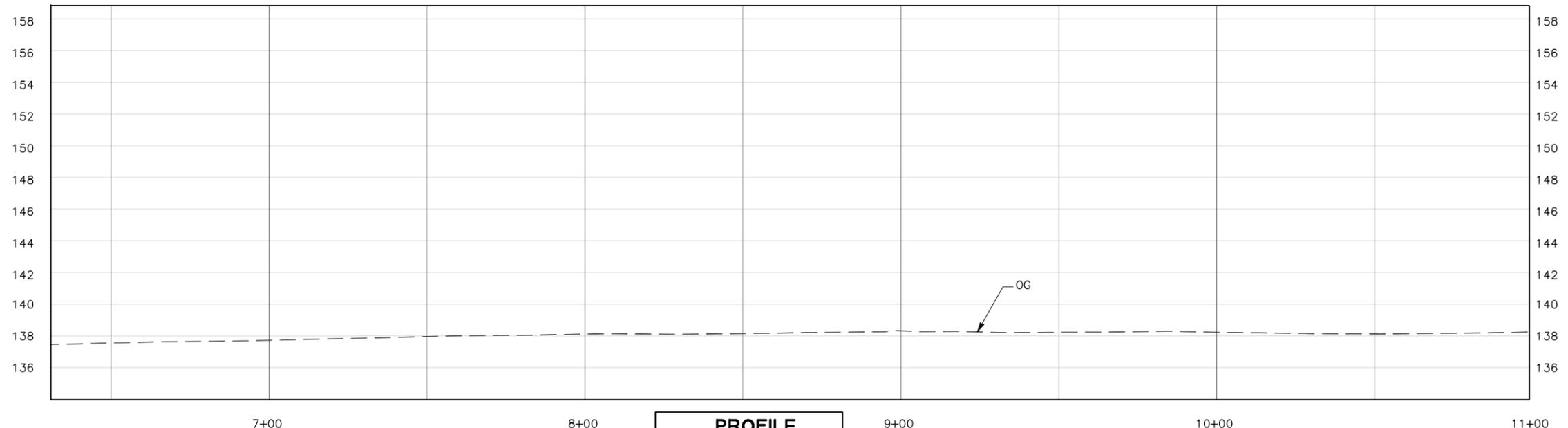
NOTES:

1. ALL CALLOUTS ON THIS SHEET REFERENCE THE "BT" LINE UNLESS OTHERWISE NOTED.



| CURVE DATA | | | | |
|------------|--------|-----------|---------|--------|
| ⊙ | RADIUS | △ | TANGENT | LENGTH |
| 1 | 175.00 | 11°34'46" | 17.74 | 35.37 |
| 2 | 189.41 | 11°35'19" | 19.22 | 38.31 |

PLAN
SCALE: 1" = 20'



PROFILE
SCALE: HORIZ 1" = 20'
VERT 1" = 5'

L-1

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
| | | | |

BENCH MARK
ELEVATION NGVD29 148.78' DATUM CITY
DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 -
A 3 1/4 inch BRASS DISK STAMPED L54796
-JAN. 1995, SOUTHWEST SIDE OF LINCOLN ST. AT
THE NORTHWEST CORNER OF THE LINCOLN ST
BRIDGE AT THE ENTRANCE TO THE MUNICIPAL
PARKING LOT AT THE CORNER OF OAK &
LINCOLN.

DESIGN BY: MJ
DRAWN BY: MJ/KD
CHECKED BY: ZS/MB
SCALE: 1"=20'
DATE: XX/XX/XXXX
PROJECT NO: SA-13119

BY: _____
30% SUBMITTAL
PLANS APPROVAL DATE _____



MARK THOMAS & COMPANY, INC.
7300 FOLSOM BOULEVARD, SUITE 203
SACRAMENTO, CALIFORNIA 95826
(916) 381-9100
FAX (916) 381-9180

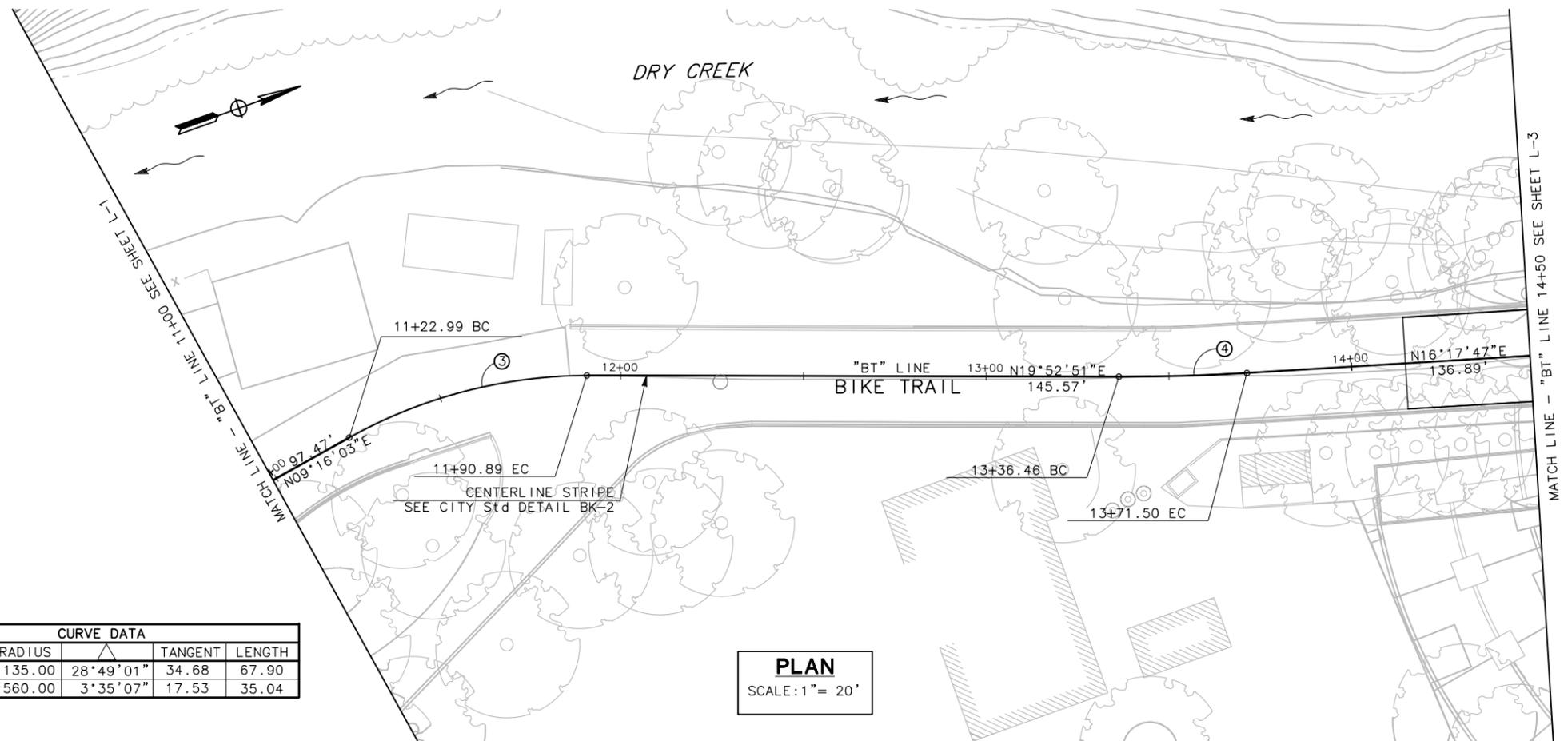
CITY OF ROSEVILLE
DEPARTMENT OF PUBLIC WORKS
311 VERNON STREET
ROSEVILLE, CA 95678
(916) 746-1300



DOWNTOWN BRIDGES AND TRAIL PROJECT LAYOUT

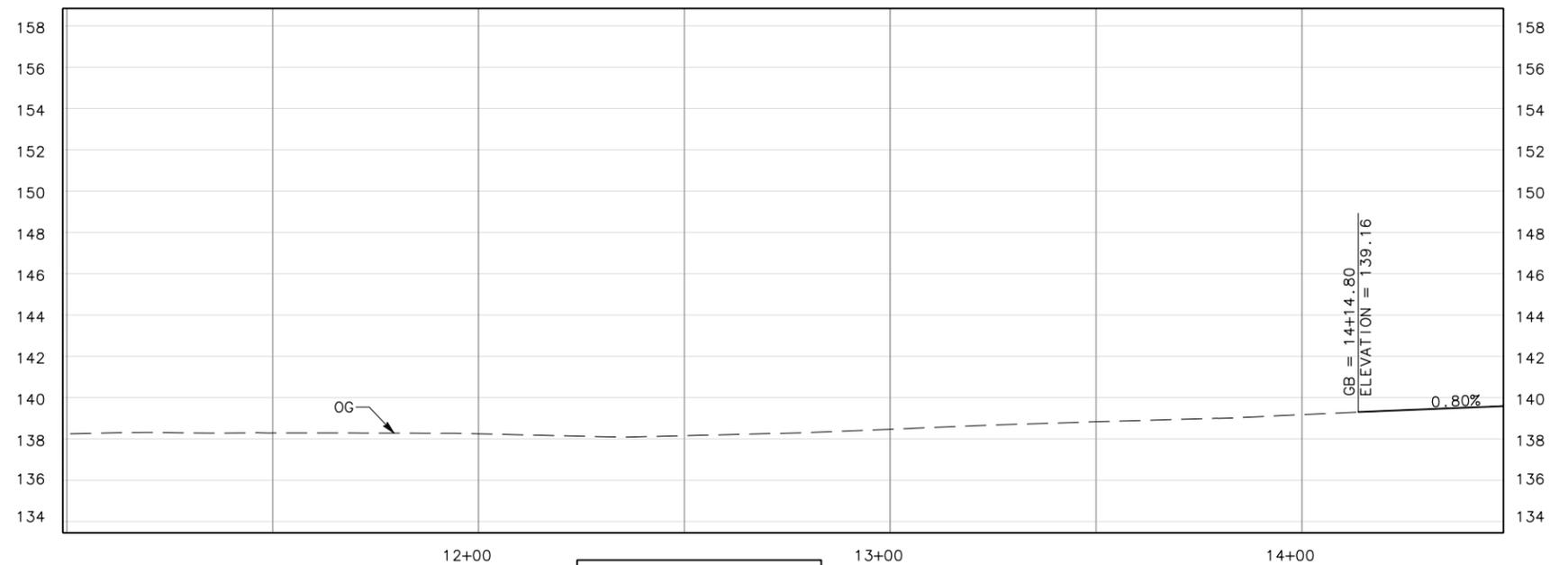
NOTES:

1. ALL CALLOUTS ON THIS SHEET REFERENCE THE "BT" LINE UNLESS OTHERWISE NOTED.



| CURVE DATA | | | | |
|------------|--------|-----------|--------|-------|
| NO. | RADIUS | TANGENT | LENGTH | |
| 3 | 135.00 | 28°49'01" | 34.68 | 67.90 |
| 4 | 560.00 | 3°35'07" | 17.53 | 35.04 |

PLAN
SCALE: 1" = 20'



PROFILE
SCALE: HORIZ 1" = 20'
VERT 1" = 5'

L-2

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
| | | | |
| | | | |
| | | | |

BENCH MARK
ELEVATION NGVD29 148.78' DATUM CITY
DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 -
A 3 1/4 inch BRASS DISK STAMPED 154796
- JAN. 1995, SOUTHWEST SIDE OF LINCOLN ST. AT
THE NORTHWEST CORNER OF THE LINCOLN ST
BRIDGE AT THE ENTRANCE TO THE MUNICIPAL
PARKING LOT AT THE CORNER OF OAK &
LINCOLN.

DESIGN BY: MJ
DRAWN BY: MJ/KD
CHECKED BY: ZS/MB
SCALE: 1"=20'
DATE: XX/XX/XXXX
PROJECT NO: SA-13119

BY: _____
30% SUBMITTAL
PLANS APPROVAL DATE _____



MARK THOMAS & COMPANY, INC.
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CITY OF ROSEVILLE
DEPARTMENT OF PUBLIC WORKS
311 VERNON STREET
ROSEVILLE, CA 95678
(916) 746-1300

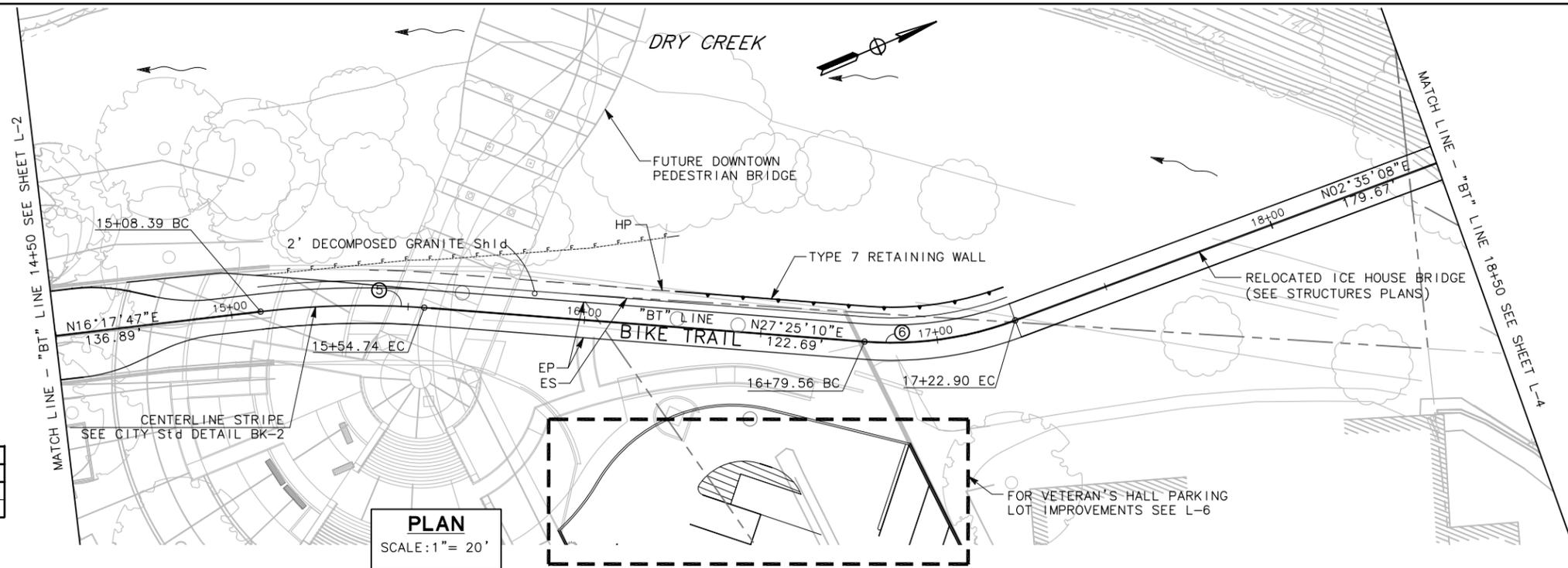


**DOWNTOWN BRIDGES AND
TRAIL PROJECT
LAYOUT**

5
OF
XX

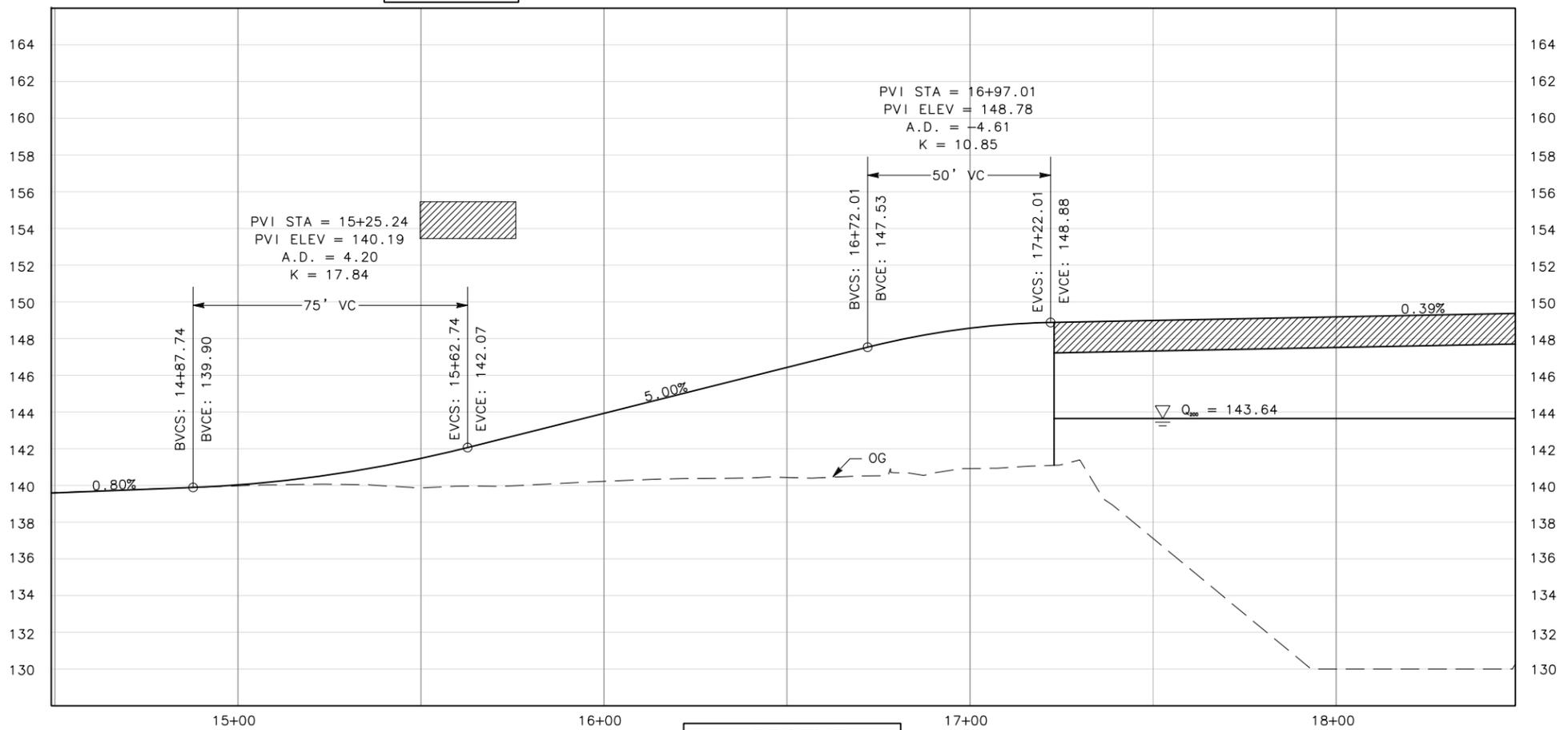
NOTES:

1. ALL CALLOUTS ON THIS SHEET REFERENCE THE "BT" LINE UNLESS OTHERWISE NOTED.



| CURVE DATA | | | | |
|------------|--------|-----------|---------|--------|
| Station | RADIUS | Δ | TANGENT | LENGTH |
| 5 | 249.75 | 11°07'23" | 24.32 | 48.48 |
| 6 | 100.00 | 24°50'02" | 22.02 | 43.34 |

PLAN
SCALE: 1" = 20'



PROFILE
SCALE: HORIZ 1" = 20'
VERT 1" = 5'

L-3

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
| | | | |

BENCH MARK
ELEVATION NGVD29 148.78' DATUM CITY
DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 -
A 3 1/4 inch BRASS DISK STAMPED 154796
- JAN. 1995, SOUTHWEST SIDE OF LINCOLN ST. AT
THE NORTHWEST CORNER OF THE LINCOLN ST
BRIDGE AT THE ENTRANCE TO THE MUNICIPAL
PARKING LOT AT THE CORNER OF OAK &
LINCOLN.

DESIGN BY: MJ
DRAWN BY: MJ/KD
CHECKED BY: ZS/MB
SCALE:
DATE: XX/XX/XXXX
PROJECT NO: SA-13119

BY:
30% SUBMITTAL
PLANS APPROVAL DATE



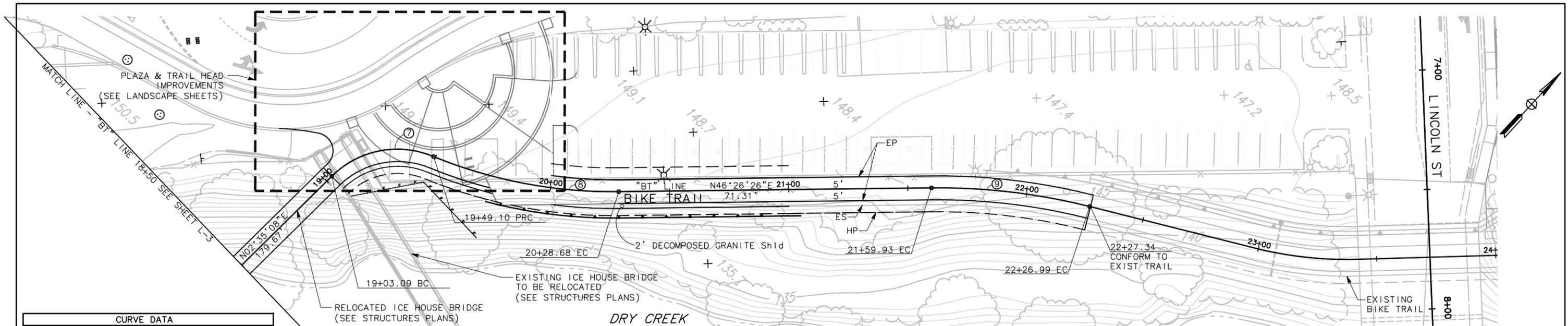
MARK THOMAS & COMPANY, INC.
7300 FOLSOM BOULEVARD, SUITE 203
SACRAMENTO, CALIFORNIA 95826
(916) 381-9100
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CITY OF ROSEVILLE
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311 VERNON STREET
ROSEVILLE, CA 95678
(916) 746-1300



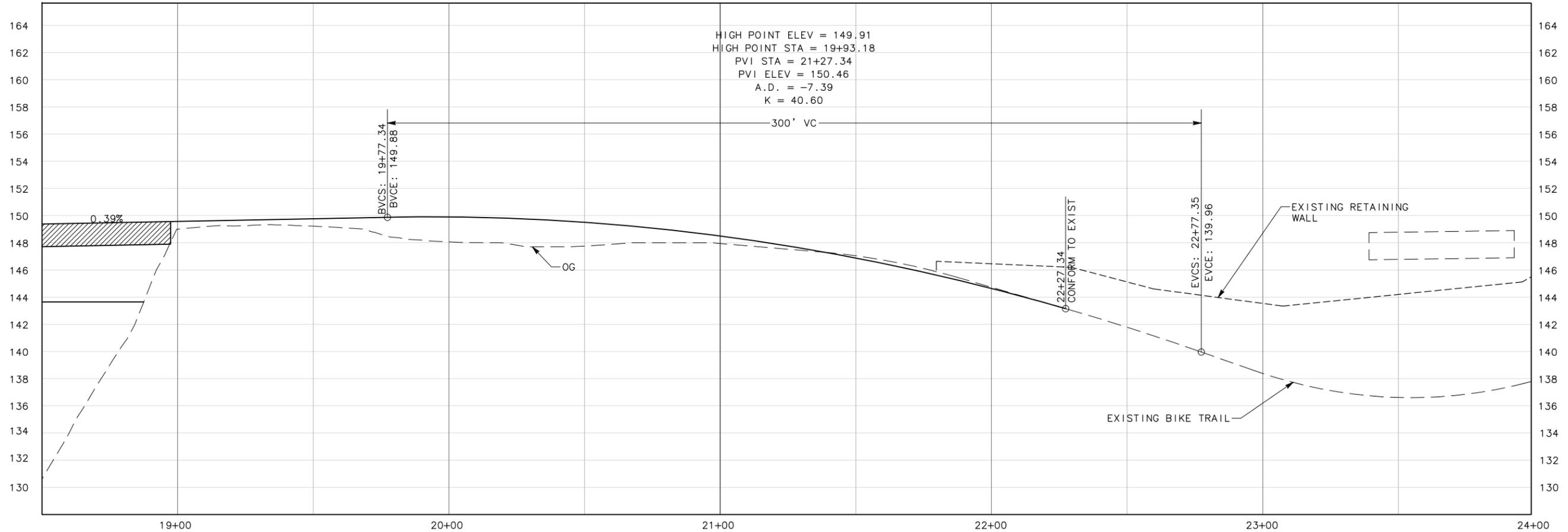
**DOWNTOWN BRIDGES AND
TRAIL PROJECT
LAYOUT**

6
OF
XX



| CURVE DATA | | | | |
|------------|--------|-----------|---------|--------|
| NO. | RADIUS | ANGLE | TANGENT | LENGTH |
| 7 | 40.00 | 66°39'09" | 26.30 | 46.58 |
| 8 | 200.00 | 22°47'51" | 40.32 | 79.58 |
| 9 | 260.00 | 11°45'50" | 26.79 | 53.38 |

PLAN
SCALE: 1" = 20'



PROFILE
SCALE: HORIZ 1" = 20'
VERT 1" = 5'

NOTES:
1. ALL CALLOUTS ON THIS SHEET REFERENCE THE "BT" LINE UNLESS OTHERWISE NOTED.

L-4

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
| | | | |

BENCH MARK
ELEVATION NGVD29 148.78' DATUM CITY
DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 -
A 3 1/4 inch BRASS DISK STAMPED L54796
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THE NORTHWEST CORNER OF THE LINCOLN ST
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PARKING LOT AT THE CORNER OF OAK &
LINCOLN.

DESIGN BY: MJ
DRAWN BY: MJ/KD
CHECKED BY: ZS/MB
SCALE: 1"=20'
DATE: XX/XX/XXXX
PROJECT NO: SA-13119

BY:
30% SUBMITTAL
PLANS APPROVAL DATE



MARK THOMAS & COMPANY, INC.
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(916) 381-9100
FAX (916) 381-9180

CITY OF ROSEVILLE
DEPARTMENT OF PUBLIC WORKS
311 VERNON STREET
ROSEVILLE, CA 95678
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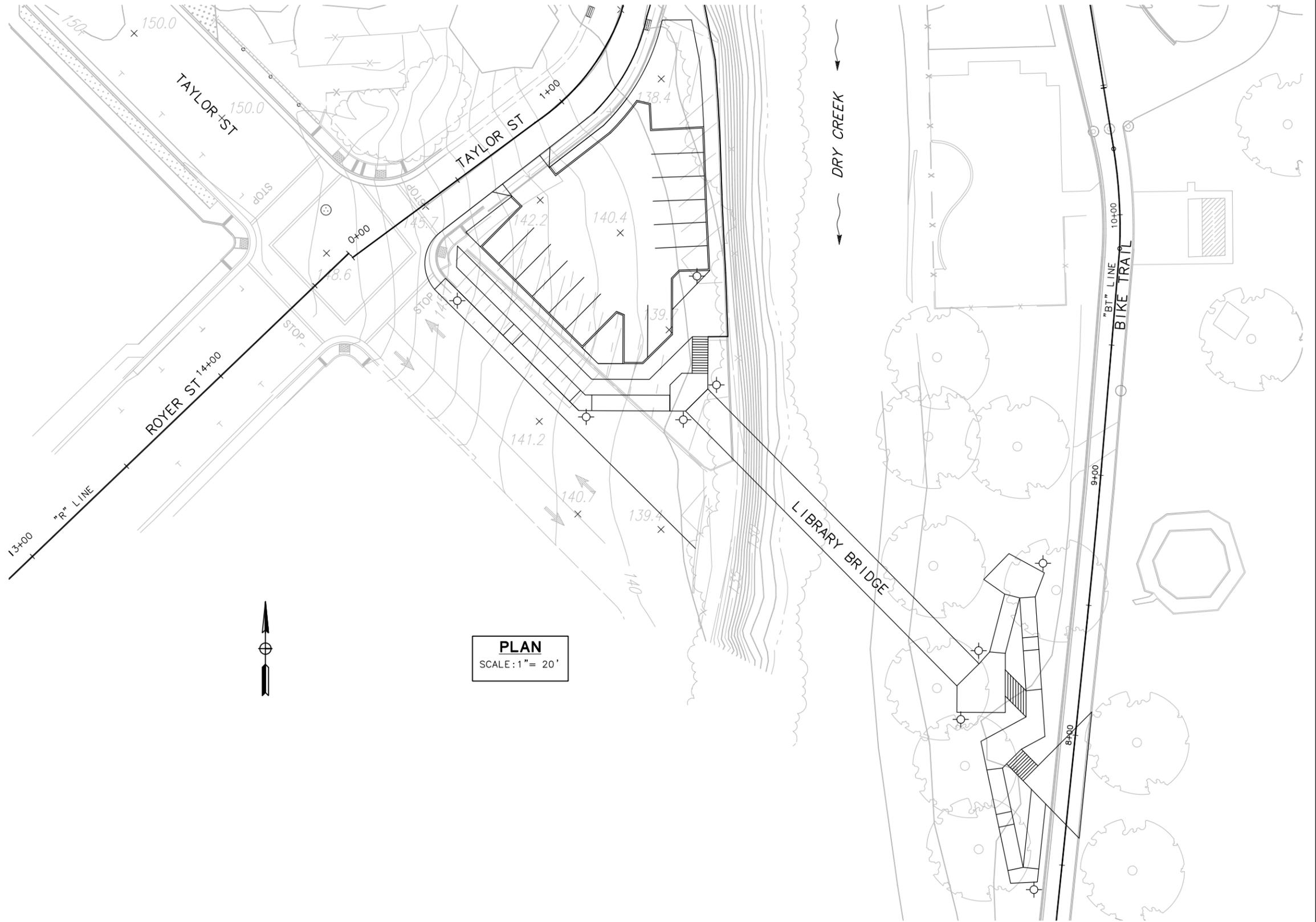


**DOWNTOWN BRIDGES AND
TRAIL PROJECT
LAYOUT**

7
of
XX

NOTES:

1. ALL CALLOUTS ON THIS SHEET REFERENCE THE "BT" LINE UNLESS OTHERWISE NOTED.



L-5

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
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BENCH MARK
 ELEVATION NGVD29 148.78' DATUM CITY
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 A 3 1/4 inch BRASS DISK STAMPED 154796
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DESIGN BY: MJ
 DRAWN BY: MJ/KD
 CHECKED BY: ZS/MB
 SCALE: 1"=20'
 DATE: XX/XX/XXXX
 PROJECT NO: SA-13119

BY: _____
 30% SUBMITTAL
 PLANS APPROVAL DATE _____



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 7300 FOLSOM BOULEVARD, SUITE 203
 SACRAMENTO, CALIFORNIA 95826
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CITY OF ROSEVILLE
 DEPARTMENT OF PUBLIC WORKS
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 ROSEVILLE, CA 95678
 (916) 746-1300

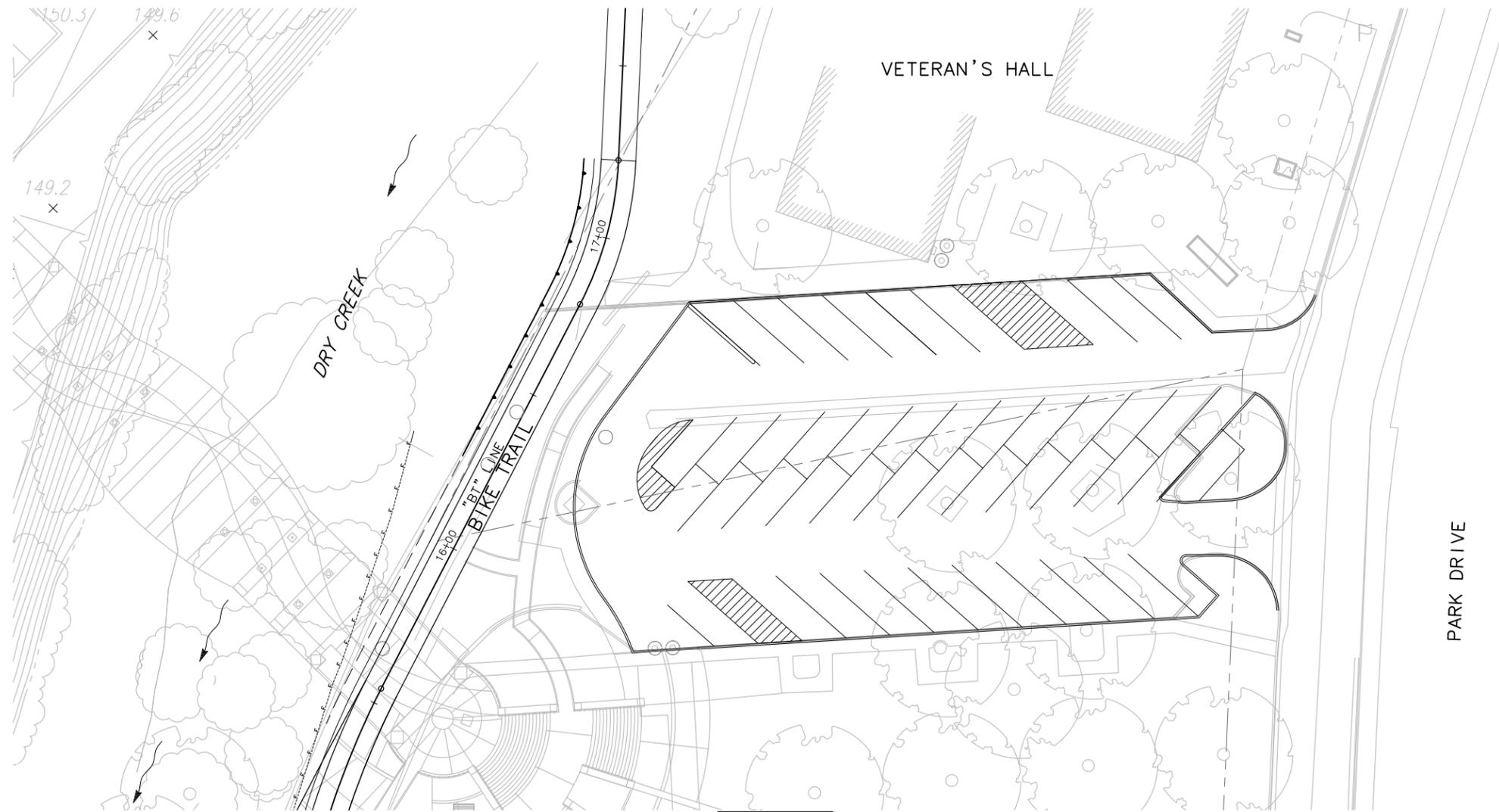


**DOWNTOWN BRIDGES AND
 TRAIL PROJECT
 LAYOUT**

8
 OF
 XX

NOTES:

1. ALL CALLOUTS ON THIS SHEET REFERENCE THE "BT" LINE UNLESS OTHERWISE NOTED.



PLAN
SCALE: 1"=20'

| NO. | REVISIONS | BY | DATE |
|-----|-----------|----|------|
| | | | |
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BENCH MARK
ELEVATION NGVD29 148.78' **DATUM** CITY
DESCRIPTION CITY OF ROSEVILLE BENCHMARK 68 -
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 311 VERNON STREET
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 (916) 746-1300



DOWNTOWN BRIDGES AND TRAIL PROJECT LAYOUT

| Item No. | Item Description | Unit | Estimated Quantity | Unit Price | Total Amount |
|--|--|------|--------------------|------------|---------------------|
| Bike Trail, Icehouse Bridge, Library Bridge | | | | | |
| <i>Civil Costs</i> | | | | | |
| General Costs | | | | | |
| 1 | Storm Water Pollution Prevention Plan | LS | 1 | \$ 50,000 | \$ 50,000 |
| 2 | Construction Area Signs | LS | 1 | \$ 2,000 | \$ 2,000 |
| 3 | Clearing & Grubbing | LS | 1 | \$ 6,000 | \$ 6,000 |
| 4 | Develop Water Supply | LS | 1 | \$ 5,000 | \$ 5,000 |
| General Costs Subtotal | | | | | \$ 63,000 |
| Bike Trail Costs | | | | | |
| 5 | Remove Concrete | CY | 13 | \$ 350 | \$ 4,550 |
| 6 | Aggregate Base (Class 2) | CY | 68 | \$ 75 | \$ 5,100 |
| 7 | Hot Mix Asphalt (1/2" Type A) | Tons | 72 | \$ 190 | \$ 13,680 |
| 8 | Decomposed Granite | SY | 32 | \$ 50 | \$ 1,600 |
| 9 | Striping | LF | 3500 | \$ 2 | \$ 7,000 |
| 10 | Roadside Sign | EA | 5 | \$ 300 | \$ 1,500 |
| 11 | Import Borrow | CY | 860 | \$ 30 | \$ 25,800 |
| 12 | Structural Concrete (Retaining Wall) | CY | 146 | \$ 700 | \$ 102,200 |
| 13 | Bar Reinforcing Steel (Retaining Wall) | LBS | 9075 | \$ 1 | \$ 9,075 |
| 14 | Structural Excavation | CY | 148 | \$ 80 | \$ 11,840 |
| 15 | Structural Backfill | CY | 425 | \$ 80 | \$ 34,000 |
| Bike Trail Subtotal | | | | | \$ 216,345 |
| Veterans Hall/Royer Park Parking Lot Costs | | | | | |
| 16 | Remove Concrete | CY | 10 | \$ 350 | \$ 3,500 |
| 17 | Aggregate Base (Class 2) | CY | 2 | \$ 75 | \$ 150 |
| 18 | Hot Mix Asphalt (1/2" Type A) | Tons | 236 | \$ 190 | \$ 44,840 |
| 19 | Remove Tree | EA | 3 | \$ 600 | \$ 1,800 |
| 20 | Striping | LF | 2028 | \$ 2 | \$ 4,056 |
| Veterans Hall/Royer Park Parking Lot Subtotal | | | | | \$ 54,346 |
| Library Bridge, Ramps and Parking Lot Costs | | | | | |
| 21 | Aggregate Base (Class 2) | CY | 38 | \$ 75 | \$ 2,850 |
| 22 | Hot Mix Asphalt (1/2" Type A) | Tons | 97 | \$ 190 | \$ 18,430 |
| 23 | Structural Concrete Retaining Wall | CY | 263 | \$ 700 | \$ 184,100 |
| 24 | Bar Reinforcing Steel (Retaining Wall) | LBS | 17355 | \$ 1 | \$ 17,355 |
| 25 | Structural Backfill | CY | 100 | \$ 80 | \$ 8,000 |
| 26 | Portland Concrete Cement | CY | 55 | \$ 700 | \$ 38,500 |
| Library Bridge, Ramps and Parking Lot Subtotal | | | | | \$ 269,235 |
| Civil Subtotal | | | | | \$ 602,926 |
| Bridge Items | | | | | |
| 27 | Relocation of Ice House Bridge (See Bridge Estimate) | LS | 1 | \$ 600,000 | \$ 600,000 |
| 28 | Library Bridge (See Bridge Estimate) | LS | 1 | \$ 500,000 | \$ 500,000 |
| | | | | | \$ 1,100,000 |
| Landscape Items | | | | | |
| 29 | Ice House Bridge Trailhead (See Site Improvement Estimate) | LS | 1 | \$ 127,684 | \$ 127,684 |
| *Note - Trailhead incidentals (site furnishings) total \$41,502, which is within the fundable 10% of project costs | | | | | \$ 127,684 |
| Mobilization | | | | | |
| 30 | Mobilization (10%) | LS | 1 | \$ 183,061 | \$ 183,061 |
| Subtotal | | | | | \$ 2,013,671 |
| Contingency (10%) | | | | | \$ 201,367 |
| Project Total | | | | | \$ 2,215,038 |
| All items are eligible for ATP funding | | | | | |
| Construction Management & Materials Testing (15%) | | | | | \$ 332,256 |
| Total (Construction + CM + Materials Testing) | | | | | \$ 2,547,294 |

Estimate of Probable Construction Costs

prepared for
City of Roseville

Downtown Bridge and Royer Park Site Improvements Concept Option 2-Grant Estimate

Callander Associates Landscape Architecture, Inc.
prepared on: 05/12/14
prepared by: MR
checked by: ES

| Item # | Description | Quantity | Unit | Cost | Item Total | Subtotal | ATP Participating | Incidentals Costs |
|------------------------------------|--|----------|------|-------------|-------------|---------------------|-----------------------|----------------------|
| | | | | | | | Y=Yes I=Incidental | |
| ICE HOUSE BRIDGE TRAIL HEAD | | | | | | | | |
| A. | Site Construction | | | | | | | |
| 1. | Specialty pavement | 5,020 | SF | \$10.00 | \$50,200.00 | | Y | |
| 2. | Bridge pilaster | 4 | EA | \$5,000.00 | \$20,000.00 | | Y | |
| 3. | Bench | 2 | EA | \$2,100.00 | \$4,200.00 | | I | \$4,200.00 |
| 4. | Trash receptacle | 1 | EA | \$2,400.00 | \$2,400.00 | | I | \$2,400.00 |
| 5. | Recycling receptacle | 1 | EA | \$2,400.00 | \$2,400.00 | | I | \$2,400.00 |
| 6. | Signage | 1 | EA | \$1,000.00 | \$1,000.00 | | Y | |
| 7. | Cast concrete plaque | 4 | EA | \$1,000.00 | \$4,000.00 | | I | \$4,000.00 |
| 8. | Bollards | 3 | EA | \$800.00 | \$2,400.00 | | Y | |
| 9. | Bike rack | 4 | EA | \$1,000.00 | \$4,000.00 | | I | \$4,000.00 |
| | | | | | | \$90,600.00 | | |
| B. | Irrigation | | | | | | | |
| 1. | Point of connection (including new water meter, backflow and controller shared with Oak Street Improvements) | ALLOW | LS | \$15,000.00 | \$15,000.00 | | I | \$15,000.00 |
| 2. | Spray irrigation | 2,250 | SF | \$1.15 | \$2,587.50 | | I | \$2,587.50 |
| 3. | Tree bubblers | 14 | EA | \$90.00 | \$1,260.00 | | I | \$1,260.00 |
| | | | | | | \$18,848.00 | | |
| C. | Planting | | | | | | | |
| 1. | Trees, 24" box | 7 | EA | \$220.00 | \$1,540.00 | | I | \$1,540.00 |
| 2. | Shrubs/groundcover | 2,325 | SF | \$1.25 | \$2,906.25 | | I | \$2,906.25 |
| 3. | Mulch | 2,325 | SF | \$0.52 | \$1,209.00 | | I | \$1,209.00 |
| | | | | | | \$5,655.00 | | |
| D. | Soil Prep and Fine Grading | 2,325 | SF | \$0.25 | \$581.25 | \$581.00 | Y | |
| E. | Park Lighting | ALLOW | LS | \$12,000.00 | | \$12,000.00 | Y | |
| F. | Subtotal Ice House Bridge Trail Head | | | | | \$127,684.00 | | |
| | Total Incidentals | | | | | | | \$41,502.75 |



BRIDGE GENERAL PLAN ESTIMATE

OR PLANNING ESTIMATE

| | | | | | |
|--|-------|------------------|----------------------|--------|------|
| STRUCTURE ICE HOUSE PEDESTRIAN BRIDGE | | BR. NO. | | | |
| TYPE Steel | | DIST. 3 | CO. | RTE. | P.M. |
| LENGTH | 180.0 | x WIDTH | 10.0 | = AREA | 1800 |
| QUANTITIES BY T. Pham | | DATE 4/8/2014 | QUANTITIES CHCKD. BY | | DATE |
| PRICED BY T. Pham | | DATE 4/8/2014 | | | |

| CONTRACT ITEMS | | UNIT | QUANTITY | PRICE | AMOUNT |
|-------------------------------|--|------|----------|----------|-------------------|
| 1 | BRIDGE REMOVAL (BRIDGE) | LS | 1 | \$10,000 | \$10,000 |
| 2 | STRUCTURE EXCAVATION (BRIDGE) | CY | 36 | \$100 | \$3,600 |
| 3 | STRUCTURE BACKFILL (BRIDGE) | CY | 7 | \$100 | \$700 |
| 4 | CLEAN AND PAINT STRUCTURAL STEEL (EXISTING BRIDGE) | SF | 12,500 | \$33 | \$412,500 |
| 5 | 24" CAST-IN-DRILLED-HOLE CONCRETE PILING | LF | 108 | \$400 | \$43,200 |
| 6 | STRUCTURAL CONCRETE, BRIDGE | CY | 29 | \$100 | \$2,900 |
| 7 | BAR REINFORCING STEEL (BRIDGE) | LB | 4,844 | \$1.25 | \$6,055 |
| 8 | TIMBER DECKING | SF | 2,700 | \$22 | \$59,400 |
| 9 | STEEL HAND RAILING | LF | 400 | \$100 | \$40,000 |
| 10 | BRIDGE RELOCATION | LS | 1 | \$30,000 | \$30,000 |
| 11 | | | | | |
| 12 | | | | | |
| 13 | | | | | |
| 14 | | | | | |
| 15 | | | | | |
| 16 | | | | | |
| 17 | | | | | |
| 18 | | | | | |
| 19 | | | | | |
| 20 | | | | | |
| SUBTOTAL | | | | | \$598,355 |
| MOBILIZATION (10 %) | | | | | 66,484 |
| SUBTOTAL STRUCTURE ITEMS | | | | | \$ 664,839 |
| CONTINGENCIES (20 %) | | | | | 132,968 |
| BRIDGE TOTAL (\$ 443 /SQ FT) | | | | | \$ 797,807 |
| GRAND TOTAL | | | | | \$ 797,807 |
| FOR BUDGET PURPOSES - USE | | | | | \$ 800,000 |

COMMENTS _____



BRIDGE GENERAL PLAN ESTIMATE

OR PLANNING ESTIMATE

| | | | | | |
|--|-------|-------------------|------------|----------------------|------------|
| STRUCTURE Roseville Library Bridge Over Dry Creek | | BR. NO. | | | |
| TYPE Steel Truss | | DIST. 3 | CO. PLA | RTE. | P.M. |
| LENGTH | 165.0 | x WIDTH | 10.0 | = AREA | 1650 SQ FT |
| QUANTITIES BY J. Hickey | | DATE 5/20/2014 | | QUANTITIES CHCKD. BY | |
| PRICED BY J. Hickey | | DATE 5/20/2014 | | | |

| CONTRACT ITEMS | | UNIT | QUANTITY | PRICE | AMOUNT |
|--------------------------------------|-------------------------------------|------|----------|-----------|----------------|
| 1 | FURNISH STEEL TRUSS BRIDGE | EA | 1 | \$300,000 | \$300,000 |
| 2 | INSTALL STEEL TRUSS BRIDGE | EA | 1 | \$40,000 | \$40,000 |
| 3 | STRUCTURE EXCAVATION (BRIDGE) | CY | 40 | \$90 | \$3,600 |
| 4 | STRUCTURE BACKFILL (BRIDGE) | CY | 24 | \$100 | \$2,400 |
| 5 | STRUCTURAL CONCRETE, BRIDGE FOOTING | CY | 11 | \$600 | \$6,600 |
| 6 | STRUCTURAL CONCRETE, BRIDGE | CY | 20 | \$650 | \$13,000 |
| 7 | BAR REINFORCING STEEL (BRIDGE) | LB | 2,500 | \$1 | \$2,500 |
| 9 | | | | | |
| 10 | | | | | |
| 11 | | | | | |
| 12 | | | | | |
| 13 | | | | | |
| 14 | | | | | |
| 15 | | | | | |
| 16 | | | | | |
| 17 | | | | | |
| 18 | | | | | |
| 19 | | | | | |
| 20 | | | | | |
| 21 | | | | | |
| 22 | | | | | |
| 23 | | | | | |
| 24 | | | | | |
| 25 | | | | | |
| SUBTOTAL | | | | | \$368,100 |
| MOBILIZATION (10 %) | | | | | 40,900 |
| SUBTOTAL STRUCTURE ITEMS | | | | \$ | 409,000 |
| CONTINGENCIES (20 %) | | | | | 81,800 |
| BRIDGE TOTAL (\$ 297 /SQ FT) | | | | \$ | 490,800 |
| GRAND TOTAL | | | | \$ | 490,800 |
| FOR BUDGET PURPOSES - USE | | | | \$ | 500,000 |

COMMENTS _____

City of Roseville

Downtown Roseville Class I Trails Project – Links to Approved Plans

The Downtown Roseville Class I Trails Project is included in and consistent with the following approved plans:

City of Roseville’s Downtown Specific Plan:

www.roseville.ca.us/planning/planning_document_library/specific_plans/downtown.asp

The realignment of Ice House Bridge and construction of the bike path was identified in Chapter 6, on page 6-48, and the construction of the Library Bridge is identified on page 6-45.

City of Roseville’s 2008 Bicycle Master Plan:

https://www.roseville.ca.us/transportation/bikeways/bicycle_master_plan/default.asp

Identified in the BMP as Segment ID “5c” on the “Proposed Bicycle Facilities” exhibit on Pages 47 and 68.

City of Roseville’s Pedestrian Master Plan:

https://www.roseville.ca.us/transportation/bikeways/ada_n_pedestrian_plans.asp

Placer County Transportation Planning Agency – 2035 Regional Transportation Plan Document:

<http://pctpa.net/library/placer-county-2035-regional-transportation-plan-document/>

<http://www.pctpa.net/library/rtp/2035/Chapter%206.6.pdf>

PLA 25469 – This project encompasses the extension of the Class I Trail from Lincoln Street to Royer Park, including the relocation and safety upgrades to the existing Ice House Bridge.

PLA 25465 – This project includes pedestrian and bicycle bridges in Downtown Roseville.

City of Roseville

Downtown Roseville Class I Trails Project – Public Participation

As described in the response to Question 3 of the Narrative, the City of Roseville conducted extensive public outreach in the development of the Downtown Specific Plan and the alternatives for the bike trail and pedestrian bridges. This included:

- A **“Peoples and Places”** forum, hosted by Fred Kent of the Project for Public Spaces collaborative
- A **Walking Audit**, led by Dan Burden, Executive Director of the Walkable and Livable Communities Institute.
- A **Community Immersion Process**, which included a series of workshops and downtown walks
- Twenty **one-on-one interviews** with individual stakeholders
- Development of a **Steering Committee** to refine key goals and improvements
- Twenty **public meetings**
- A **Design Contest** to allow public participation in the selection of the design consultants. Throughout the course of the design contest, further outreach included:
 - Two **Public Workshops**
 - Two **Focus Meetings** with the Veterans Hall members
 - A three week **Public Review**, in which the City received 100 comments

The City maintains a webpage dedicated to the outreach for the Downtown Roseville Class I Trails project and the Downtown Pedestrian Bridge project (which is included as a separate grant application). The website is at the following link:

https://roseville.ca.us/communityinvestment/revitalization/improvement_n_construction_projects/in_progress/downtown_bridges_project.asp

The following pages include the section from the Downtown Specific Plan that documents the public outreach process, as well as notifications, summaries and comments from several of the meetings.



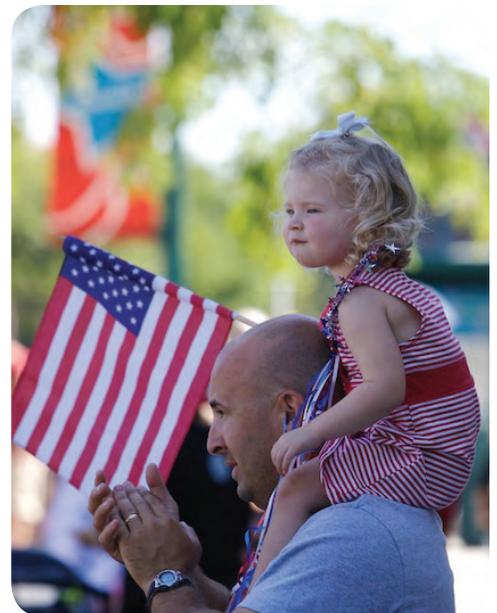
1.0 Introduction

1.1 Creating A Vision for Downtown

In 2005, the City of Roseville initiated a visioning process for the Downtown area. This was in response to a community desire to revitalize their existing downtown area into a vibrant and flourishing destination. The Vision for the Downtown has evolved through a series of community and public forums. The first of these efforts was a “People and Places” forum that was conducted in the spring of 2005.

The focus of this effort was on the Vernon Street core and uses adjacent to Dry Creek. A group of approximately 70 people were provided with a brief presentation of what makes a great park or square, and how Place-making works. They also were presented with examples from other communities, and how they addressed these through applying Place-making principles. The participants were then broken into groups assigned a specific geographic area in which they identified existing issues and opportunities to apply Place-making solutions. The groups then reported out and specific short and long term opportunities were consolidated into a conceptual plan.

The conceptual plan had a number of significant improvements that have been further developed as part of the Vision. These improvements included the creation of additional public plaza space. A key concept to the strategy of Place-making is to provide multiple public gathering areas and then provide 10 things to do within these spaces. The more variety and activity, the greater the benefit these areas will provide to the Downtown. The plan



1.0 Introduction



An unprecedented 900 total hours was invested by the public during a 3 day immersion process



Walking tours were a key component of the visioning process

also promoted a better interaction with Dry Creek. Instead of positioning the buildings and uses along Dry Creek so their backs are to the creek, future uses would integrate and focus on the creek. Land uses would also be modified to position more active commercial mixed-uses adjacent to the creek. The proximity to Royer Park and the natural amenities then become more accessible and integrated into the Downtown.

1.2 Visioning Project

With the support stemming from the Place-making exercise, the project then progressed to a more formal and intense Visioning project. In autumn of 2005 the community-based visioning effort focused all attention on downtown Roseville in a three-phase project effort. These phases consisted of:

- *Developing an understanding of existing conditions in the Downtown;*
- *Reviewing the existing plans and strategies; and*
- *Engaging the community to work together to create and refine a unified vision that they are committed to.*

Key to the success and realization of the Vision is the involvement of the community. The purpose of this process was to develop a downtown vision that was supported by the community and stakeholders. The Vision also needed to be consistent with recommendations of other committees, projects, and strategies that had previously been developed. In this form, it will serve as a catalyst for a reinvestment plan that identifies strategies to achieve the vision.

The effort to involve the community was completed through a community immersion process. During this process, the community was engaged through a series of workshops and downtown walks. The focus of these workshops included;

- *Providing an understanding of the existing conditions both physical and economically;*
- *Addressing architecture, building styles and the type and intensity of uses within the area;*
- *Design workshops that gave the public the opportunity to work in teams and draft concept plans for the Downtown; and*



1.0 Introduction

- *A series of Downtown walks to identify assets and opportunities in the area.*

From the unprecedented community participation and investment of over 900 citizen hours, there were several key findings and outcomes.

Key Findings: The community input received during the Visioning process was synthesized and summarized to create the Vision for Downtown Roseville. This included the draft vision statement, vision plan graphic, and accompanying implementation strategies, as shown on the Vision Poster (Exhibit 1.1).

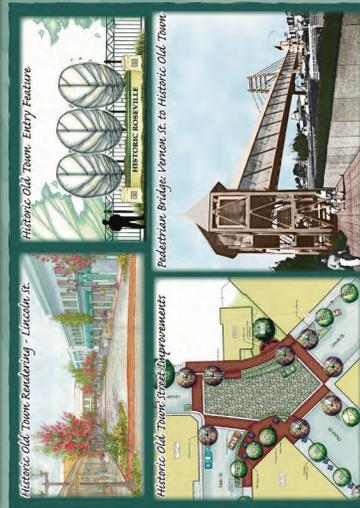
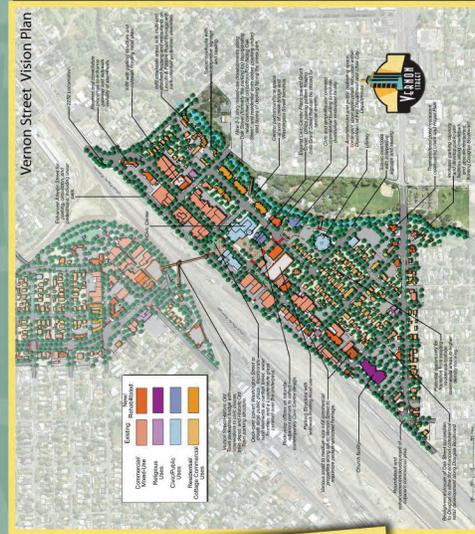
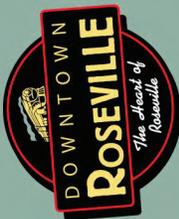
These findings provided a foundation for developing the Vision. These key findings are:

- *To increase connectivity between the Vernon Street District and Historic Old Town;*
- *Allow each district to maintain a unique identity;*
- *Incorporate more arts, culture, entertainment, and public spaces in the Downtown;*
- *Encourage a greater mix of uses and business types in the Downtown; and*
- *That housing and mixed-use development would be appropriate in the Downtown.*



Vision for Downtown Roseville...

Downtown Roseville is a prosperous and vibrant destination. Its pedestrian-friendly environment links a mix of retail and residential uses with parks, public plazas, the creekwalk, and cultural resources. Downtown is a place for the community and visitors to gather for festive events and celebrations. It is a place where people want to go, stay, and play – morning, noon, and night.



CITY OF
ROSEVILLE
PLANNING & REDEVELOPMENT

1.0 Introduction

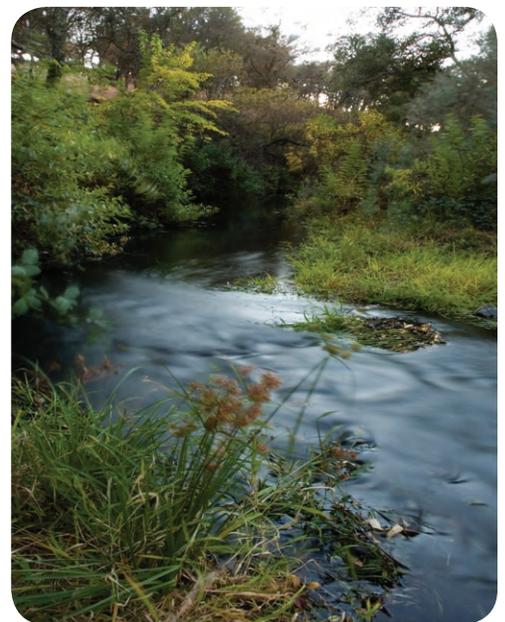
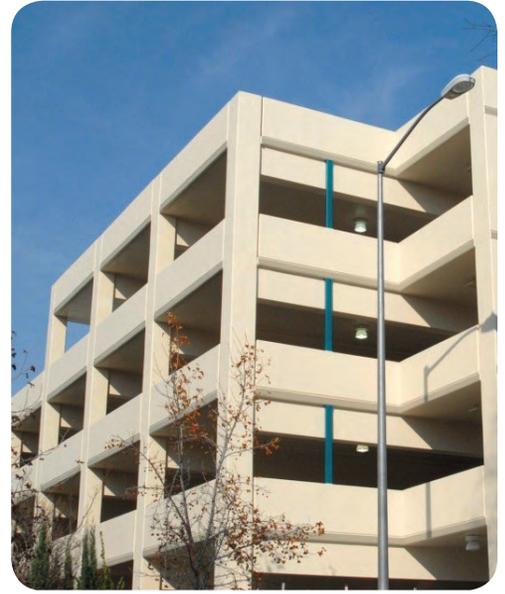
Based on the aforementioned findings, the community compiled and agreed upon the following Vision Statement;

“Downtown Roseville is a prosperous and vibrant destination. Its pedestrian-friendly environment links a mix of retail and residential uses with parks, public plazas, the creek walk, and cultural resources. Downtown is a unique place for the community and visitors to gather for festive events and celebrations and is considered the historic and civic center of Roseville. It is a place where people want to go, stay, and play – morning, noon, and night.”

At the completion of the Visioning project, the community also developed a set of “Next Steps”. The community recognized that in order to implement this Vision, a Specific Plan and accompanying Environmental Impact Report would be required. To address some of the specific goals associated with the future revitalization of the area, the Specific Plan would consider and address:

- *Promotional, Organizational, and Economic Development Strategies;*
- *A Parking Management System;*
- *A Creek Maintenance and Flood Management Strategy; and,*
- *Relocation Studies for the Fire Station #1 site.*

The investment of this community participation and vision development has established the overall principles and components of the Downtown Specific Plan. Recognizing the communities desire to move this revitalization effort forward, in January of 2006 the City Council gave the direction to move forward with the development of a Specific Plan and Environmental Impact Report.



1.0 Introduction

1.3 Specific Plan Building Upon the Vision

1.3.1 One-on-One Interviews

In a continued effort to involve the community in the development of a Specific Plan and Vision for Downtown, further community outreach was sought out. The purpose was to solidify what the community felt would work in the Downtown and then to build upon it. As part of this process, twenty (20) one-on-one interviews were conducted with individual stakeholders.

The participants were a diverse group of important stakeholders identified by the City of Roseville. The interviews consisted of a series of open-ended questions focused on further defining the goals of the community, as a whole. The outcome of these discussions provided valuable input.

The major concepts expressed during these interviews were that the plan should incorporate strategies that:

- *Encourage the right mix of diverse uses in Downtown, including residential, restaurant and retail uses;*
- *Residential should be structured to meet a wide array of potential consumers, providing affordable to luxury units;*
- *Create safe, attractive pedestrian, bicycle and vehicular connections between Royer Park, the creek and Vernon Street, and Vernon Street and Historic Old Town;*
- *Analyze parking and traffic flow; and*
- *Ensure implementation through funding projects and providing development incentives to private development.*



Members of the Steering Committee



1.3.2 Steering Committee Meetings

In addition to the one-on-one questions, and consistent with the Visioning project, the Specific Plan development process has encouraged the project area stakeholders and the public to participate in establishing the key goals, strategies and improvements within the plan. Public participation is integral to the Specific Plan process because broad community support will facilitate future implementation. In August of 2006, a diverse twenty six member Steering Committee was assembled. It was comprised of residents, property owners, business owners, appointed officials, and other dedicated individuals committed to seeing downtown Roseville become a vibrant, walkable city center that is both attractive and economically successful.

Through a series of nine public meetings, the Steering Committee finalized the Downtown Vision statement, established the desired level of intensity, worked to identify and prioritize needed improvements, and helped to produce a preferred development plan for the Downtown Specific Plan area. This Committee also worked in conjunction with the Roseville Revitalization Committee (RRC) to develop a conceptual phasing and implementation program for the key plan improvements.

1.3.3 Vision Statement

One of the first items for consideration by the Steering Committee was to revisit the Vision Statement that had originally been developed as part of the Vision project. The Vision Statement that resulted from the original visioning effort was built upon and changed slightly by the Downtown Specific Plan Steering Committee. It is as follows:

“Downtown Roseville is a prosperous and vibrant destination. Its pedestrian-friendly environment links a mix of retail and residential uses with parks, public plazas, the creek walk, and cultural resources. Downtown is a unique place for the community and visitors to gather for festive events and celebrations and is considered the historic and civic center of Roseville. It is a place where people want to go, stay, and play – morning, noon, and night.”



Streetscape provides improvements supporting and enhancing public spaces



The Blue Line Gallery is an example of the Cultural opportunities encouraged Downtown



Grand Opening of Dash of Panache on Vernon Street



1.0 Introduction

Core Principles:

Connectivity

Place-making

Land Use

Identity

Public Enhancement

Arts & Culture

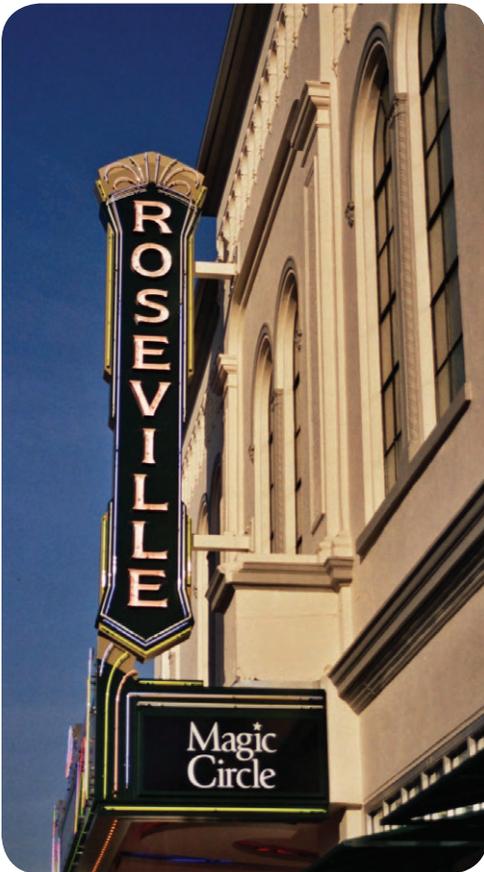
The Steering Committee felt that the statement developed as part of the Visioning process adequately described the key elements of what they envisioned for the future of the Downtown with a few minor changes. The minor edits, which are indicated by the underlining above, gave further clarity to the desire of the committee to create a unique place that also respected the historical nature of Downtown being the heart of the community.

1.3.4 Core Principles

In association with the Vision, a set of guiding principles was developed. These principles were established in the original Vision project discussion. As part of the Steering Committee tasks, these principles were reviewed and discussed at length.

The Committee's finalized version of these principles are reflected in the underlying components below.

- ***Connectivity:*** *Improve connectivity within Downtown as well as between Downtown and its surroundings;*
- ***Place-making:*** *Respect and honor the history and influence of the railroad;*
- ***Land uses:*** *Identify land uses and development standards that are responsive to market opportunities and that facilitate quality architecture and urban design;*
- ***Identity :*** *Reinforce identity through the establishment of character districts and gateway elements;*
- ***Public Enhancement:*** *Create and enhance public places that support community activity, spirit, and involvement; and*
- ***Arts and Culture:*** *Promote arts, culture, heritage, entertainment, and education.*



Existing theatres reinforce the opportunity to promote art and culture



Connectivity between the three core areas has been an on-going theme through the Vision process and the Specific Plan development. The Committee felt very strongly that this should be emphasized as a key principle.

Additionally, the railroad has been a key feature in the development of the City of Roseville. This asset and the overall heritage of the Downtown makes it unique to the community and the region. Combing a new land use pattern and an identity for the Downtown that enhances the public space is key to the redevelopment strategy envisioned by the Steering Committee.

1.4 Vision for the Civic Core –



Vernon Street District



Dry Creek Mixed-Use District



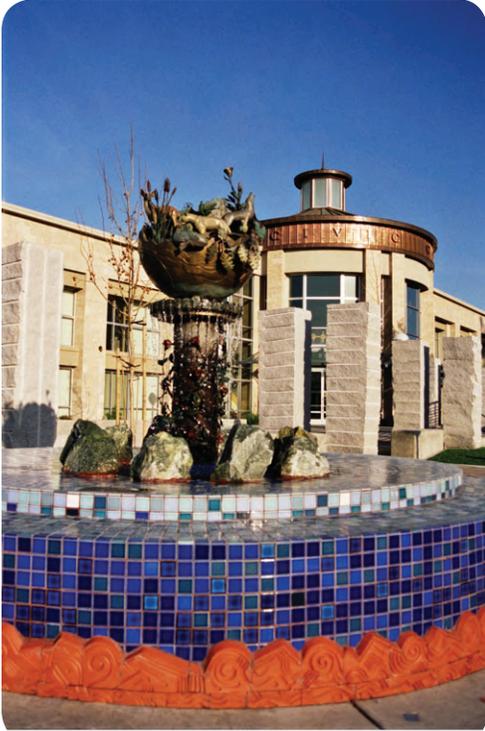
Douglas Corridor District

Creek View Residential District

Bungalow District



1.0 Introduction



Vernon Street

The Steering Committee identified the need to differentiate between the three unique areas that make up the Downtown Specific Plan area. These three core areas were identified as the Civic Core area (Vernon Street), Entertainment District (Historic Old Town) and Park Area (Royer/ Saugstad Park). It is important to keep them separate, yet complementary to one another.

Vernon Street is generally seen as “Roseville’s Civic Core” as it houses many of Roseville’s civic buildings (City Hall, the Fire Station, Post Office, and Library). As great downtowns should be seen as the cultural hub of the city, this Specific Plan envisions Vernon Street as the more urbane environment, the go to place for the arts, entertainment, dining, and family outings. Featured in this stylish hub will be the new Blue Line Gallery and two theaters, all within walking distance of each other and the revitalized public amenities (parks, creek, and amphitheater).

Downtown Vernon Street is comprised of the following five districts:

- *Vernon Street District;*
- *Creek View Residential District;*
- *Bungalow District;*
- *Douglas Corridor District; and*
- *Dry Creek Mixed-Use District.*

Connectivity is a crucial element in creating the fabric of the downtown, and Dry Creek will incorporate pedestrian and bicycle paths alongside the creek that will branch out throughout the downtown (see Chapter 5 for full details about mobility plans). Pedestrian trails will connect the two parks, Dry Creek, and the two project areas, Vernon Street and Historic Old Town. Downtown Gateway monuments on Douglas Boulevard adjacent to both parks will announce the arrival to Downtown Roseville and an open-air amphitheater and plant demonstration garden are planned southeast of the library. See Chapter 8 for further details about the parks, recreation, and open space.



1.0 Introduction

Roseville's new Town Square adjacent to City Hall combined with improvements to Grant Street will create a very special connection between the parks, Dry Creek, and the downtown urban environment. The Square will utilize a combination of hardscape and softscape elements, interactive water features, and gracious seating areas to create a special gathering space next to City Hall. Mixed-use development, that combines new residential, shopping, and dining opportunities focusing on the creek as a visual amenity will further generate the excitement of an urban space.



A combination of land use, design and facilities, both existing and planned, work to accentuate Vernon Street as the "Civic Core". These concepts are incorporated into the Specific Plan and reinforce the Community's desire to create an interesting and vibrant Core in the heart of Downtown.



Conceptuals of 316/320 Vernon Street



1.0 Introduction



The Bank of Italy building represents the historic architecture in the district



The HOT district already supports viable nighttime entertainment uses



1.5 Vision for the Entertainment District- Historic Old Town

Historic Old Town (HOT) is Roseville’s historic district as it was the original birthplace of the City of Roseville. Beginning in the 1860’s when Central Pacific Railroad intersected with California Central to create the Roseville junction and a railroad town was created. In 1874, it is here that the first buildings were built. Then in the 1920’s, Historic Old Town became the commercial and business activity center for Roseville.

Many of Roseville’s lifelong residents feel strongly that the railroad is an integral part of Roseville’s history and should remain as such and be honored today. As part of the Vision for this area, the Specific Plan looks to return the historic district back to its role as a vibrant area bustling with activity.

A mix of uses that will promote the area as “Roseville’s Entertainment District” are integrated into the plan. Historic Old Town is comprised of the following five districts:

- *Washington Corridor District*
- *Washington Corridor (Intensified)*
- *Historic Old Town Bungalow District*
- *Historic Old Town Commercial District*
- *Historic Old Town (Extension)*

This Specific Plan embraces a unique vision for how Historic Old Town develops in the future, focusing on how Historic Old Town is seen differently than Vernon Street. While Vernon Street is family-oriented, relaxed, upscale, romantic, and grand as a rose; Historic Old Town is trendier, hipper, funkier, more cutting edge with a vibrant nightlife. Historic Old Town is seen as a place of up and coming arts and entertainment, while Vernon Street is more “established.”



1.0 Introduction

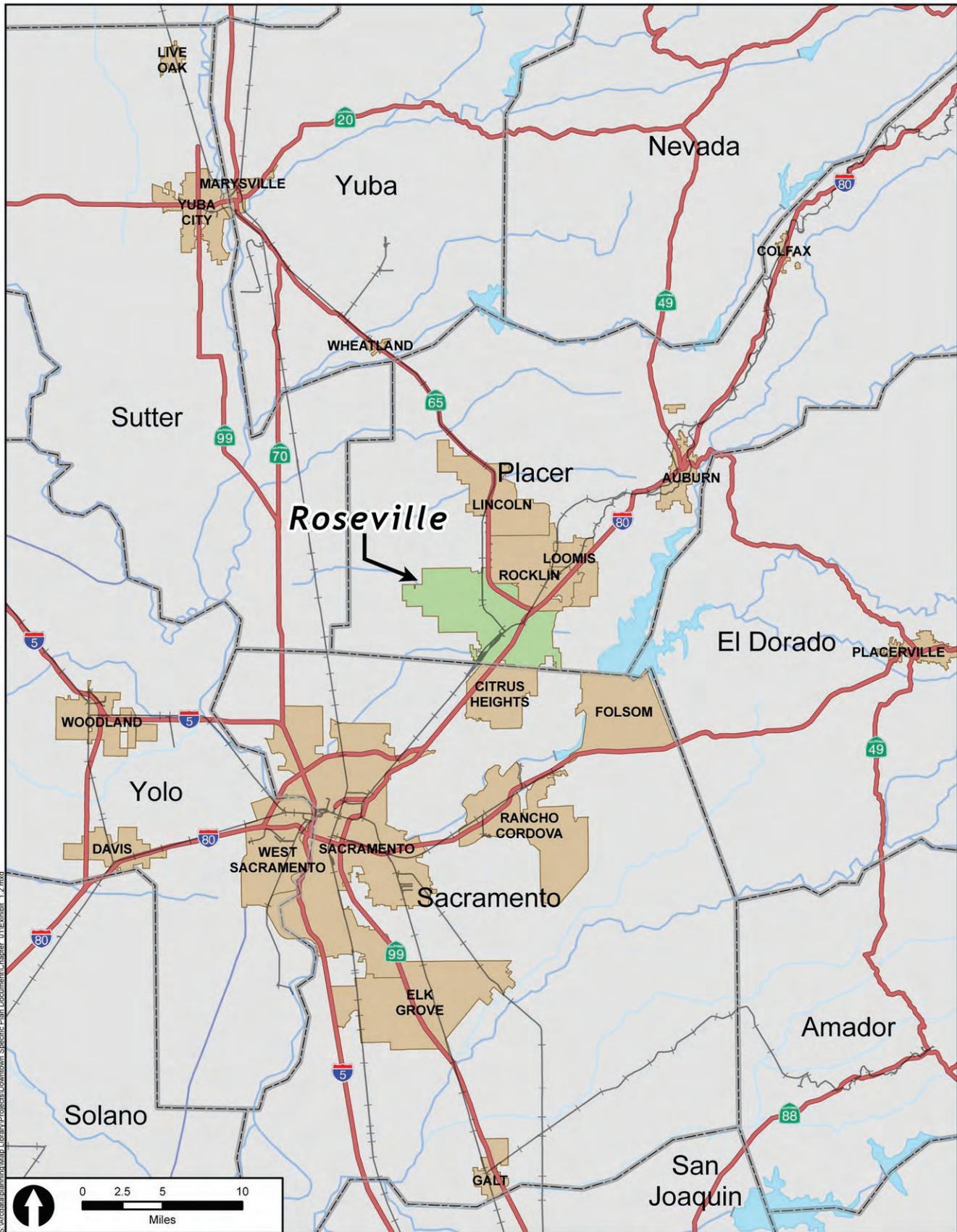
The focus in Historic Old Town is to promote a mix of entertainment/commercial/residential uses that will breathe life into Historic Old Town and create the desired aesthetic and intensity. Nearby residential units will complement Historic Old Town with a diverse blend of fully furnished efficiency units, single-family attached, multi-family (lofts, live-work units, etc.) featuring a full range of economic price-points for all income levels. Residential units downtown, and in close proximity to downtown, are an essential component of the downtown structure providing economic support for the downtown businesses and uses.

The goal is to have Historic Old Town become a vibrant, energetic, exciting urban area that draws young professionals, empty nesters, and families who enjoy energetic urban settings to it because of the lifestyle and convenience offered by shops, entertainment, restaurants and residences being all in one place. Historic Old Town will be highly walkable aided by this denser urban fabric.



Residential units at a high density will be an essential component of the Historic Old Town





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1.0 Introduction



Cook Shack and the Children's Art Center at Royer Park



Saugstad Park will be enhanced and re-energized



Salmon Habitat in Royer Park

1.6 Vision for the Park District – Royer/Saugstad Parks

The two parks (Royer Park and Saugstad Park), Dry Creek and the new Town Square will create gathering places that will be family-oriented and will allow Roseville's residents and visitors to enjoy the outdoors. Royer Park is envisioned to have large play areas, playing fields, group picnic areas, basketball courts, an art plaza, an interactive water plaza, and a natural history walk. Saugstad Park is envisioned to have a nine-hole pitch and putt golf course, tennis courts and an active dog park.

The key elements in each of the parks in the Park District are focused on achieving the guiding principles associated with the vision. Creating and revitalizing public spaces is accomplished with the creation of a town square with an active water feature. The proposed square would then be linked through pedestrian connections to Royer Park. This connectivity is strengthened through additional improvements within Royer Park including an interpretive walk along Dry Creek and a tie into the Citywide bike trail system. This bike trail system extends south of Douglas Boulevard into Saugstad Park. The Class 1 trails in Royer and Saugstad Parks are part of the Dry Creek Greenway Trail System, a planned bike trail system with important local and regional connections.

Place-making and public enhancements are also integrated into design principles for the Park District. These concepts are reinforced through such physical improvements as adding interactive water features to the parks, an amphitheater adjacent to Dry Creek and upgrades to the Children's Art Center and the cook shack at Royer Park. These elements all promote the guiding principles established by the Vision.



New bridges will promote connectivity



1.7 Location and Context

Roseville is located approximately 16 miles northeast of downtown Sacramento on the Interstate 80 corridor, in Placer County. The Downtown Roseville Specific Plan area is roughly 176 gross acres and is an infill planning area, completely surrounded by built out neighborhoods; Cherry Glen and Hillcrest to the south and southeast, Roseville Heights to the west, Los Cerritos to the northwest, Sierra Vista to the northeast, and Enwood, and Folsom to the east. It is comprised of two existing physical areas, Vernon Street and Historic Old Town, connected by the Washington Boulevard underpass. The major access to the Downtown Specific Plan area is Interstate 80.

Important regional elements within the Downtown Specific Plan area also include the two existing parks, Royer Park, and Saugstad Park. (see Exhibit 1.2, Regional map and Exhibit 1.3, Vicinity map).

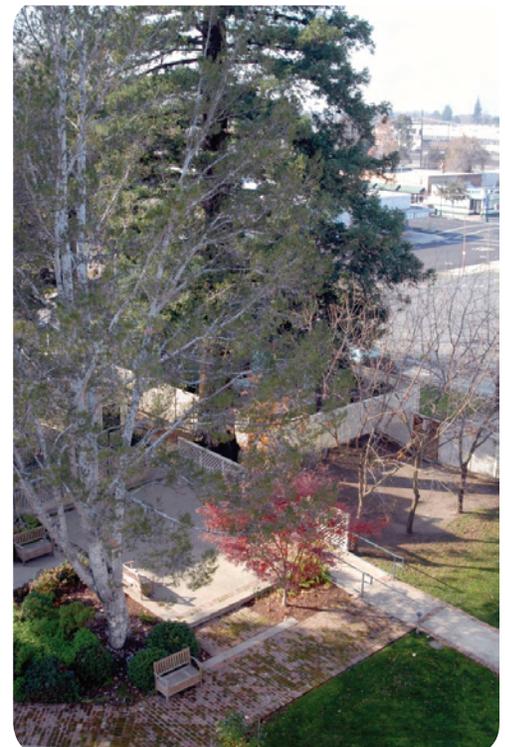
The Downtown Specific Plan area is bisected by a wide and vast railroad right-of-way, with both sides of the project area (Historic Old Town and Vernon Street) accessed by the Washington Boulevard underpass that runs under the railroad tracks. Both areas are seen as distinctive and have individual personalities and unique urban fabrics unto themselves.



Washington Blvd. overpass is the current link between HOT and Vernon Street



The Plan will promote a pedestrian environment



View from parking garage overlooking the Haman House



1.0 Introduction

1.8 Purpose and Intent of the Specific Plan

The Downtown Specific Plan will ultimately provide for the orderly and efficient implementation of the Downtown Roseville Visioning Project and uphold the core community values identified during this process. The Downtown Specific Plan is also based upon a Downtown Vision Feasibility Analysis of the study area. The Downtown Specific Plan will guide development of the downtown area for over the next twenty years.

The purpose of the Downtown Specific Plan is to:

- *Revitalize and enhance the economic, social, cultural, and recreational fabric of the city's downtown urban core;*
- *Clearly articulate land uses and development standards by district in the **Downtown Code**; and*
- *Create a structure and direction that will guide the economic revitalization of the Downtown with a mix of uses - retail, office, and residential – supported by enhanced mobility options.*

The Downtown Specific Plan provides detailed land use and form-based development regulations for the Downtown. This is further supported by focused design guidelines tailored to both private development and the public realm. This structure will ensure that the Downtown is a vital and vibrant destination both economically and physically. Historic Old Town and Downtown Vernon Street will be unified and holistic, responsive to the existing infrastructure, yet building upon the existing assets and uniqueness of each area. The Downtown Specific Plan is driven by the community wide visioning effort and the subsequent efforts of the Downtown Specific Plan Steering Committee.

The authority for specific plans was established under California law (Section 65451 et. Seq.) to provide a greater level of specificity in planning a site of special interest or value to a community. As required by law, a specific plan must contain a text and diagram(s) which specify all of the following in detail:

- *The distribution, location and extent of uses of the land including open space, within the area covered by the Plan;*
- *The proposed distribution, location and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy and other essential facilities to be located within the area covered by the Plan area and needed to support the land uses described in the Plan;*



- *Standards and criteria by which development will proceed and standards for the conservation, development and utilization of land resources, where applicable; and*
- *A program of implementation measures including regulations, programs and financing measures necessary to carry out the stated actions.*

The Plan is intended to guide the development and redevelopment in the area over a span of twenty years. The Downtown Specific Plan components are not intended to be static; rather they are meant to provide a framework for development that clearly expresses the community's vision.

It is the intent of this Specific Plan to support the Vision Statement and the directives and decisions made by the Steering Committee in response to the community's desires and expectations.

1.9 Relationship to the California Environmental Quality Act (CEQA)

An Environmental Impact Report (EIR), pursuant to the requirements of CEQA, has been prepared to provide an analysis of the potential impacts of this Specific Plan and to recommend appropriate mitigation measures as policies and features of the Plan. The Specific Plan and the EIR were prepared in close coordination with one another; mitigation measures have been incorporated into the Specific Plan, where possible.

The Downtown Specific Plan assesses the implications of an assumed program of residential, commercial, office and mixed-use development. When specific development proposals are submitted to the City for development in the Specific Plan area, the City will determine whether or not the environmental impacts were addressed in the Project EIR. If the City finds that the proposed project would not result in any additional environmental impacts that were not considered in the EIR, no new analysis would be required. (Appendix H)



Erosion along Dry Creek



Flood marker at Royer Park



Creek Restoration Improvements



1.0 Introduction



Existing Post Office site at 320 Vernon Street



Aerial view of the intersection at Washington Boulevard and Oak Street

1.10 Background Reports and Studies

This Specific Plan is based on data collection and a series of background reports and technical memorandum. The following provides a list of key reports and technical memoranda:

Traffic and Parking Assessment: Fehr and Peers compiled traffic and parking information that was presented to the Steering Committee in a series of technical memorandums. These memorandums were prepared and presented on October 30, 2006. In addition to this information, a Traffic and Parking Study was completed in February 6, 2007. The report provides the background data and summary information on the traffic and parking conditions both existing and within the project.

Royer/Saugstad Park Master Plan Update: The existing Master Plan was completed in 1995. As part of this project, Carducci and Associates has provided an updated Master Plan. This Master Plan provides for the framework for specific improvements within the overall context of the Specific Plan. The improvements contained within the Master Plan have also been analyzed as part of the Environmental Impact Report. (Appendix A).

Utility Assessment: Mark Thomas and Company performed a full utility analysis for the project area. This analysis included the review of existing and future capacity to service the Plan area. This analysis was prepared in a series of technical memorandums that were presented as part of the public outreach component. The compilation and presentation of this information occurred starting October 30, 2006 and ended on June 11, 2007. (Appendix E)

Roseville Economic Analysis Update: Keyser Marston Associates, Inc. prepared a review and update of the "Economic Analysis" associated with the Vision project. This study updated the market conditions for retail and residential uses within the Downtown. It also confirmed the challenges related to developing in the plan area and how the City could assist the private sector in order to encourage revitalization of the area. Keyser Marston's analysis has also provided key strategies on funding the improvements associated with the Specific Plan. (Appendix D)



Floodway Analysis: RBF Inc. was contracted by the City of Roseville to complete an assessment of the project improvements and the impacts to the designated floodway. This study provides updated topographic information for this section of Dry Creek and models the impacts to the floodway, per the City of Roseville's engineering standards. (Appendix F)

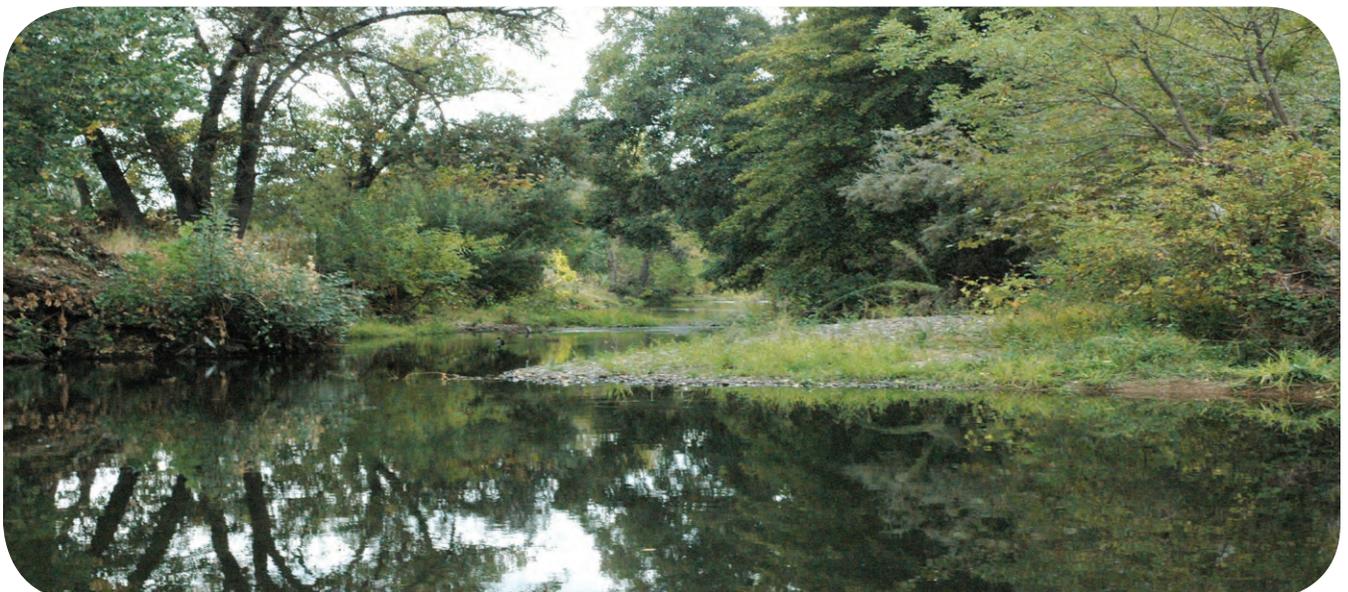
Dry Creek Restoration and Flood Management Plan: EDAW Inc. developed a restoration and flood management plan for Dry Creek, dated February 28, 2007. This plan provides a practical strategy for the restoration of Dry Creek. It includes recommendations that address restoration, public access and flood control in this reach of Dry Creek. (Appendix G)

In addition, prior reports that were relied upon in the completion of this document include:

Downtown Roseville Visioning Project Executive Summary and Economic Analysis: The City Council directed staff to develop a vision for Downtown Roseville – Vernon Street and Historic Old Town. This project included both the initial visioning and an economic feasibility analysis. The project was completed in December 2005 and accepted by the City Council on January 25, 2006.

Bicycle Master Plan (BMP): The BMP was adopted in 1994 pursuant to the Roseville General Plan and California Bicycle Transportation Act, and guides the development of off-street bike trails and on-street bike routes, and the operation of bike education programs, within the City. A comprehensive update of the BMP was completed in June 2008

Harding to Royer Bike Trail Project: The Harding to Royer Bike Trail is a planned Class I off-street trail that is identified in the Bikeway Master Plan as a priority project because it will provide an important link from outlying areas to downtown Roseville and because it will connect the existing Royer Park and Miners Ravine bikeways.



Dry Creek is a migratory route for Steelhead and Chinook salmon



1.0 Introduction

Short Range Transit Plan: The Short Range Transit Plan (S RTP) is a transit planning document for the City of Roseville's capital and operating expenditures for Roseville Transit over the next five (5) years. The document presently shows several local fixed routes, commuter routes and a connection to the Capital Corridor heavy rail commuter service in the Downtown Roseville area.

Roseville Creek and Riparian Management and Restoration Plan: This document is the city-wide guide for riparian management and creek restoration. A discussion on Downtown revitalization is on page 17 and restoration recommendations are on page 122.

Dry Creek Bank Erosion Management Plan: This study includes specific recommendations for bank stabilization in the Downtown and includes a detailed study of Dry Creek geomorphology.

Royer Park/Dry Creek Place-making Workshop: In February 2005, 70 stakeholders and city staff participated in a Place-making workshop for the Royer Park/Dry Creek project area. This half-day workshop proved to be very successful in defining the key interests and concepts for this area of the city's downtown. The recommendations from the report have been referenced and incorporated, as appropriate.

Roseville's Blueprint Implementation Strategy: This document outlines a menu of options to be considered by the City of Roseville to implement the Blueprint Growth Principles established by the Sacramento Council of Governments (SACOG) in December 2004. The options include programs and specific projects that are either being implemented today in Roseville or could be implemented in the future. The document provided options for the near-term up to 30 years out.

Comparative Site Analysis Report- Replacement Headquarters - Fire Station No. 1: This report looks at four options as identified by the City of Roseville and the Roseville Fire Department as potential locations for a new Headquarters Fire Station No. 1 in an effort to meet current and future community requirements. Authored by: RRM Design Group in September 1, 2007. (Appendix B)



Downtown Bridges Design Competition PUBLIC WORKSHOP

**Tuesday, October 2
5:30 – 7:30 p.m.**

A brief presentation will be held at 5:30 p.m. and the public workshop will be open until 7:30 p.m. Please join us at any time between 5:30 and 7:30 p.m.

**Royer Park
Children's Art Center**
190 Park Drive, Roseville



**Give us your
thoughts!**

Join us for an Interactive Workshop

The City is hosting a public workshop to kick off a competition to select the designer for the Downtown Bridge Project.

The workshop will feature brief presentations and opportunities to share your vision for the design of two new bridges and re-use of the existing Rube Nelson "Icehouse" Bridge across Dry Creek.

These bridges are integral to creating the pedestrian and bicycle friendly environment envisioned in the Downtown Specific Plan. Your input will be used by prospective design teams as they help bring the Downtown vision to life. Come learn about the design competition and how you can participate!

For more information

Please call
(916) 442-1168
or go to
[www.roseville.ca.us/
downtown](http://www.roseville.ca.us/downtown)

Learn more about the design competition at

[www.roseville.ca.us/
downtown](http://www.roseville.ca.us/downtown)



Building Connections to Downtown Roseville!

DOWNTOWN BRIDGES PROJECT

GOALS & OBJECTIVES

Overarching Goals & Objectives

- Consider and address citizen input received at the October 2, 2012 Downtown Bridges Public Workshop (see Public Input Exercises Summary).
- Preserve existing parking. If parking must be modified, complete a parking study that evaluates the effect upon parking and recommends appropriate mitigation.
- Enhance public places through high quality architecture and urban design.
- Respect and honor the history, tradition and influence of the railroad on Roseville. Evaluate the pros and cons of rehabilitation and re-use of the Rube Nelson “Icehouse” Bridge at the Replacement Bridge and Class I Trail/Bridge locations.
- Promote the WPA Style, a contemporary interpretation of Works Progress Administration (WPA) era projects typified by the economical use of natural materials such as concrete, stone, metal, stucco, wood and ornamentation and the use of craftsmanship and proportions to define the structure
- Consider opportunities to celebrate creek ecology, riparian habitat and fall-run salmon
- Provide safe, comfortable, interesting, convenient and connected paths and walkways, with consideration of anticipated movement patterns for pedestrians and bicyclists.
- Maximize user experience and sense of place through connections and views to Dry Creek.
- Bridge landings are well integrated into the project and existing and proposed land uses on each side of Dry Creek. If feasible avoid use of switchbacks at bridge landings.
- Lighting is integrated into the design, is vandal-proof and avoids excess glare.
- Meet Federal, State and Local design standards, including Americans with Disabilities Act.

Additional Goals and Objectives for Oak Street Class I Bridge/Trail Extension:

- Provide a seamless bikeway connection between the Miners Ravine Trail and the trail in Royer Park. This should be accomplished by providing: A Class I bikeway connection from the planned terminus of the Miners Ravine Trail at the Lincoln Street Parking lot to the Royer Park parking lot; and as feasible either a Class I connection or a Class III connection along or parallel to Dietrich Drive from the Royer Park parking lot to the existing Class I trail in Royer Park.
- The bikeway and bridge should be safe, comfortable and convenient for all users, and bicyclists should not be required to walk their bicycles across the bridge. A minimum width of 12' is recommended if a new bridge is used in this location.
- Maintain access to the Veterans Hall
- Avoid conflicts with the existing and planned sewer main/siphon.
- Accommodate the proposed Oak Street improvements.
- If feasible include a spur trail to link the fire station site to the Class 1 trail

Additional Goals and Objectives for Replacement Bridge:

- Provide a direct pedestrian connection between the library and the Class I trail in Royer Park, including a bridge and any necessary walkway extensions and curb ramps.
- The bridge should be safe and comfortable for pedestrian use and should anticipate occasional bicycle use.

- Accommodate future uses, including the planned amphitheater, creekwalk and plant demonstration garden.
- Avoid affects upon the floodwall and accommodate future efforts for bank stabilization and increased flood capacity.
- Maintain access to and accommodate future development of the courthouse site.
- Evaluate bridge loading in consideration of the bridge's proximity to the amphitheater and its potential use for viewing. service access for event set up and routine maintenance activities
- Minimize and, as necessary, mitigate impact upon existing gazebo, picnic area and Class I trail in Royer Park.
- Consider access for public safety personnel, including access from one side of the creek to the other through design of the bridge or design of a ramp from the existing trail to Douglas Boulevard

Additional Goals and Objectives for Fire Station No. 1 Site Improvements:

- Prepare plans and obtain necessary permits for pad grading to accommodate future re-location of Fire Station No.1.
- Design utility stubs to accommodate the future construction of re-located Fire Station No.1.

Additional Goals and Objectives for Downtown Bridge to Royer Park:

- The Downtown Bridge is envisioned as one of the landmark public improvements for Downtown Roseville.
- The Downtown Bridge is wide enough to include pedestrian overlook areas, benches and/or area for vendors.
- The bridge landing is incorporated into a plaza with an iconic or signature element within Royer Park. The current project will not be required to construct the plaza, but consideration should be given how the bridge landings relate to existing and planned improvements.
- The Downtown Bridge project will connect to the mid-block crossing at Oak Street that is planned for construction as part of a separate project in 2013.
- Design bridge and appurtenances in consideration of existing improvements and functions within Royer Park. Service access to the Royer Park Parks Maintenance buildings shall be maintained. This shall be accomplished by retaining Dietrich Drive, or providing a suitable alternative. The design shall also identify methods to maintain public access to rear of Children's Art Center and parking if feasible. Identify any necessary phasing of improvements to accommodate park improvements.
-
- The future Class I trail should be routed, if feasible, underneath the Downtown Bridge.

Downtown Bridges Public Workshop

Public Input Exercises Summary

October 2, 2012, 5:30 to 7:30 pm

Royer Park, Children's Art Center

INTRODUCTION

The City of Roseville hosted a public workshop to kick off a design competition to select the designer for the Downtown Bridge Project. The Downtown Bridge Project includes the design of two new bridges and re-use of the existing R.F. Rube Nelson "Icehouse" Bridge across Dry Creek. The following is a summary of the public input from each workshop exercise.



EXERCISE #1 – DESIGN INTENTION AND VISION

Objective: Provide Community Values input to the design teams for the Design Intention/Vision.

Meeting attendees were given an opportunity to provide input on bridge goals and objectives.

The Goals/Objectives included:

Oak Street Class I Bridge/Trail Extension

- Provide a Class I bikeway connection between the Royer Park parking lot and the planned terminus of the Miners Ravine Trail at the Lincoln Street Parking Lot.
- To the extent feasible, minimize the effect upon parking in Royer Park and the Oak Street parking lot.
- Plan for and accommodate the proposed roundabout at Washington and Oak Street.
- Avoid conflicts with the existing and planned sewer main/siphon.

Replacement Bridge near Main Library

- Provide a direct pedestrian connection between the library and the Class I trail in Royer Park, including a bridge and any necessary walkway extensions and curb ramps.
- Avoid effects upon the floodwall and accommodate future efforts for bank stabilization and increased flood capacity.
- Evaluate the potential impact resulting from the removal of parking adjacent to the Main Library and, to the extent feasible, minimize the loss of parking, which will be used until the amphitheater is constructed.
- Consider access for public safety personnel, including access from one side of the creek to the other through design of the bridge or design of a driveway from the existing trail to Douglas Boulevard.

Downtown Bridges Public Workshop
Public Workshop #1 Summary

Downtown Bridge to Royer Park

- The Downtown Bridge is wide enough to include pedestrian overlook areas, benches and/or area for vendors.
- Provide a direct pedestrian connection between the Town Square and the future creek-inspired plaza in Royer Park.
- Comply with floodplain ordinance to ensure no impacts related to flooding.

Attendees were given the option to email or fax responses to exercise #1; to date no responses have been received.

EXERCISE #2 – OPPORTUNITIES AND CONSTRAINTS

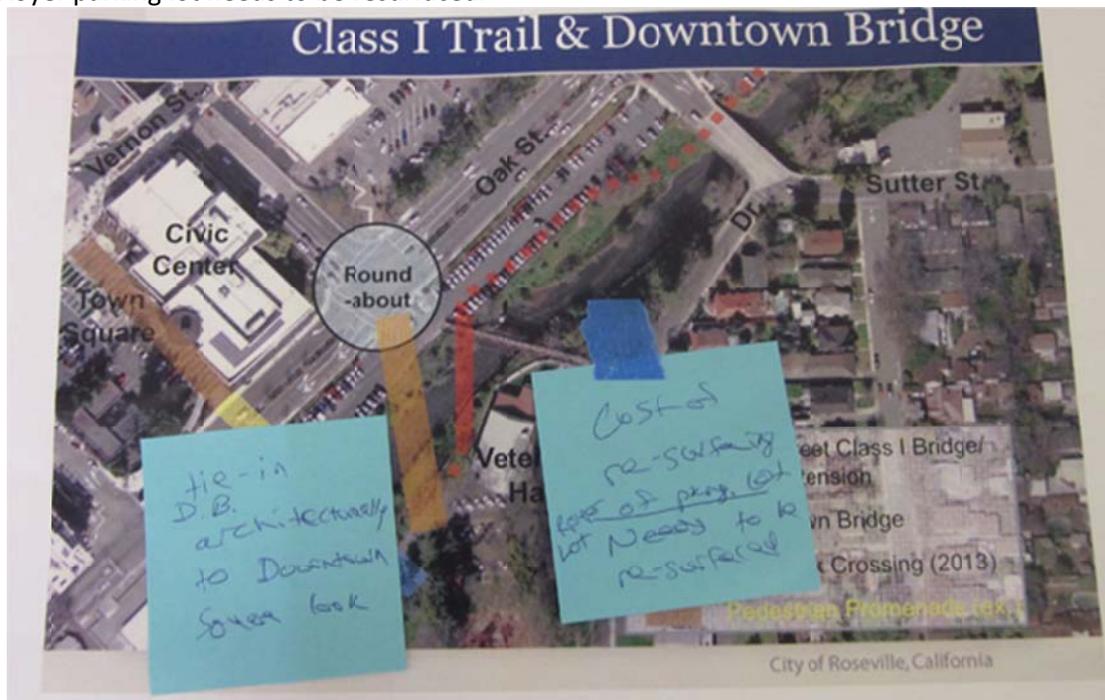


Objective: Obtain feedback on opportunities and constraints.

Meeting attendees were asked to identify additional considerations, write them on a sticky note, and place them on the enlarged site maps. Each map highlighted the City's identified constraints and opportunities. A summary of the public input is below (see photos of each map for placement of each specific comment):

Class I Trail and Downtown Bridge

- Tie-in downtown bridge architecturally to downtown over-look.
- Royer parking lot needs to be resurfaced.



Downtown Bridges Public Workshop
Public Workshop #1 Summary

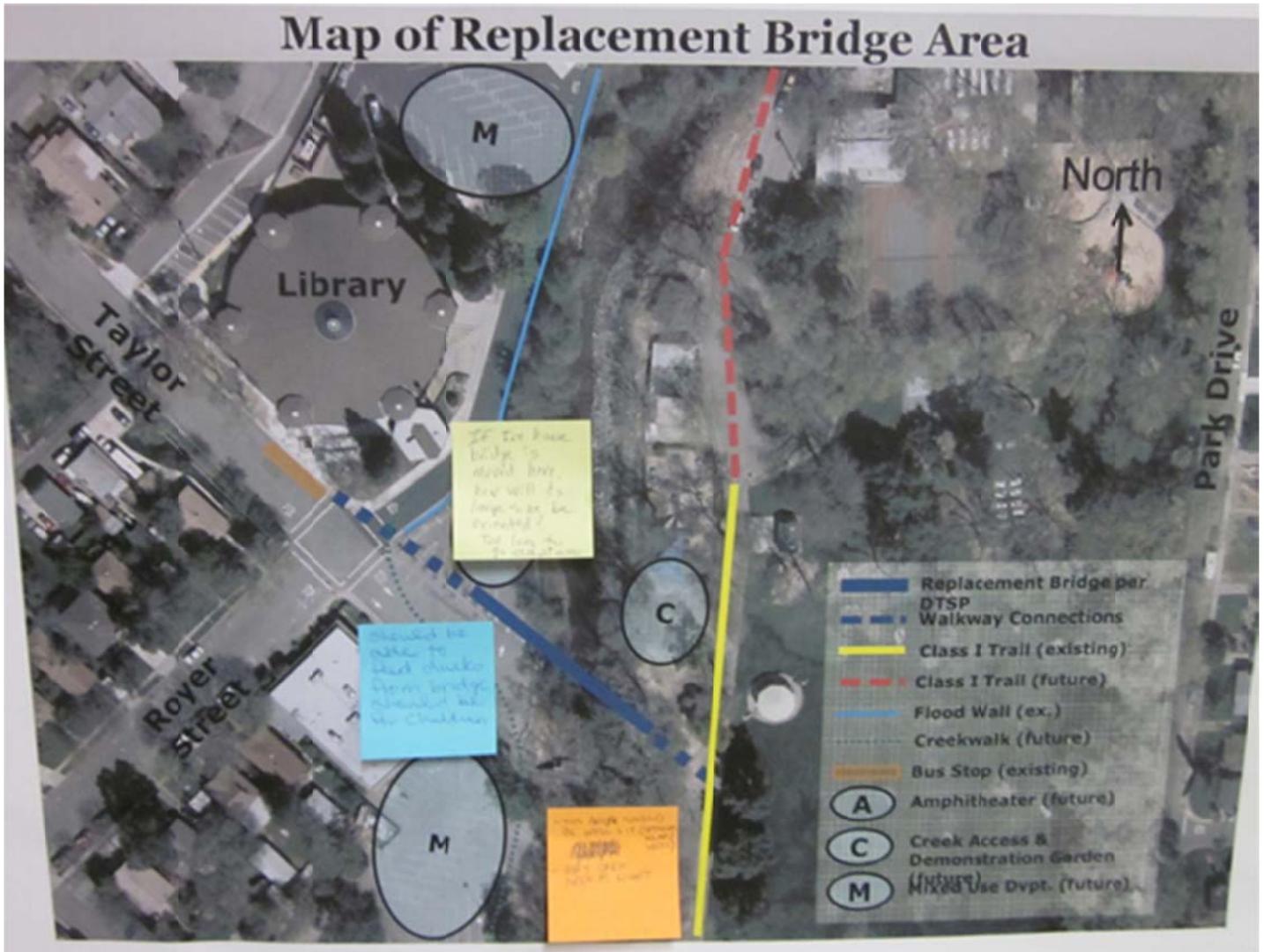
Map of Class I / Downtown Bridges

- The bridge has strong ties to the veteran groups. The American Legion was one of the ones responsible for getting the bridge where it is.
- We need parking close for disabled veterans on both sides of Memorial Hall.
- The Roseville veteran groups would like to have the Rube Nelson Icehouse Bridge left where it is – or swing around to the other side of Veterans Hall (American Legion #169, Veterans of Foreign Wars #1489, Fleet Reserve #230, Vietnam Veterans #500).
- Use natural materials (e.g. permeable pavers, stones, etc.)
- Do we need three bridges? Remove main middle bridge.
- Don't want all concrete.
- Don't clear creekway – Don't create "fountains", man-made environment, like nature.
- Keep all existing trees – Don't push city into peaceful park.
- Put in diagonal parking on Park Drive and/or parking on the other side of the Veteran's Hall.
- Parking? (see map photo for placement of note, near Veteran's Hall)
- There seems to be a difference in height of creek banks here. How will that be handled?
- This area should be well-lit (optional solar/LED's).
- Round-a-bout could have a train theme (similar to round-house/train turning underground, machine "look")
- Good lighting behind.
- The Icehouse Bridge is inappropriate as part of the bikeway.
- Keep in mind that large expanse of concrete facing makes graffiti inevitable.
- Any bridge design must be salmon and riparian life friendly – Do No Harm!



Map of Replacement Bridge Area

- If Icehouse Bridge is moved here how will its large size be oriented? Too long to go straight across.
- Should be able to see ducks from bridge. Should be for children.
- This area should be well-lit (optional solar/LED's). This is a very dark area at night.



Downtown Bridges Public Workshop
Public Workshop #1 Summary

Specific Plan Map

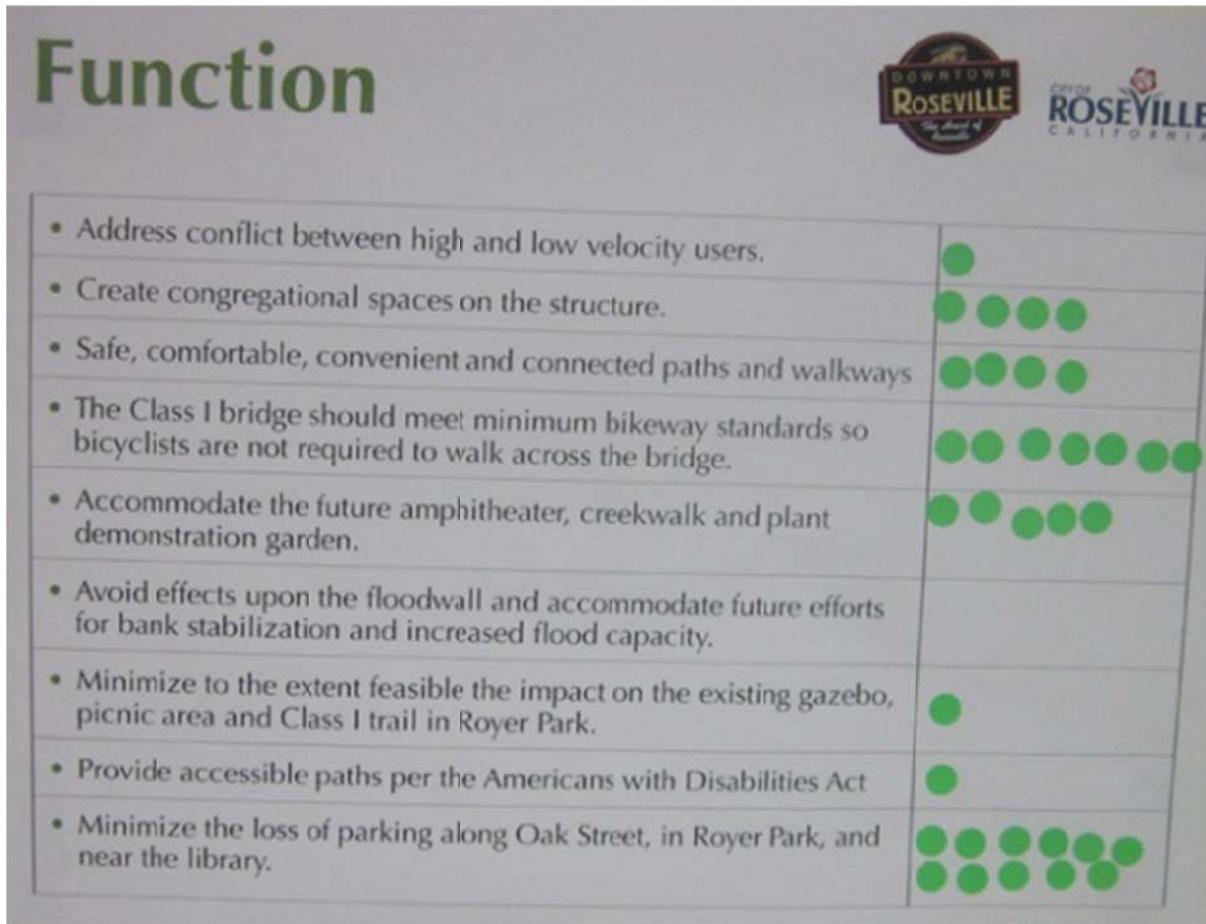
- Consider different lengths.
- Like view / Sitting on downtown bridge.
- Maximize visibility here where bikes and peds will cross.
- Consider shaded structure bridge at downtown bridge.
- Easy walking path from bridge to Park Drive needs to be free of rivets and slippery debris.
- Maximize visibility here too.



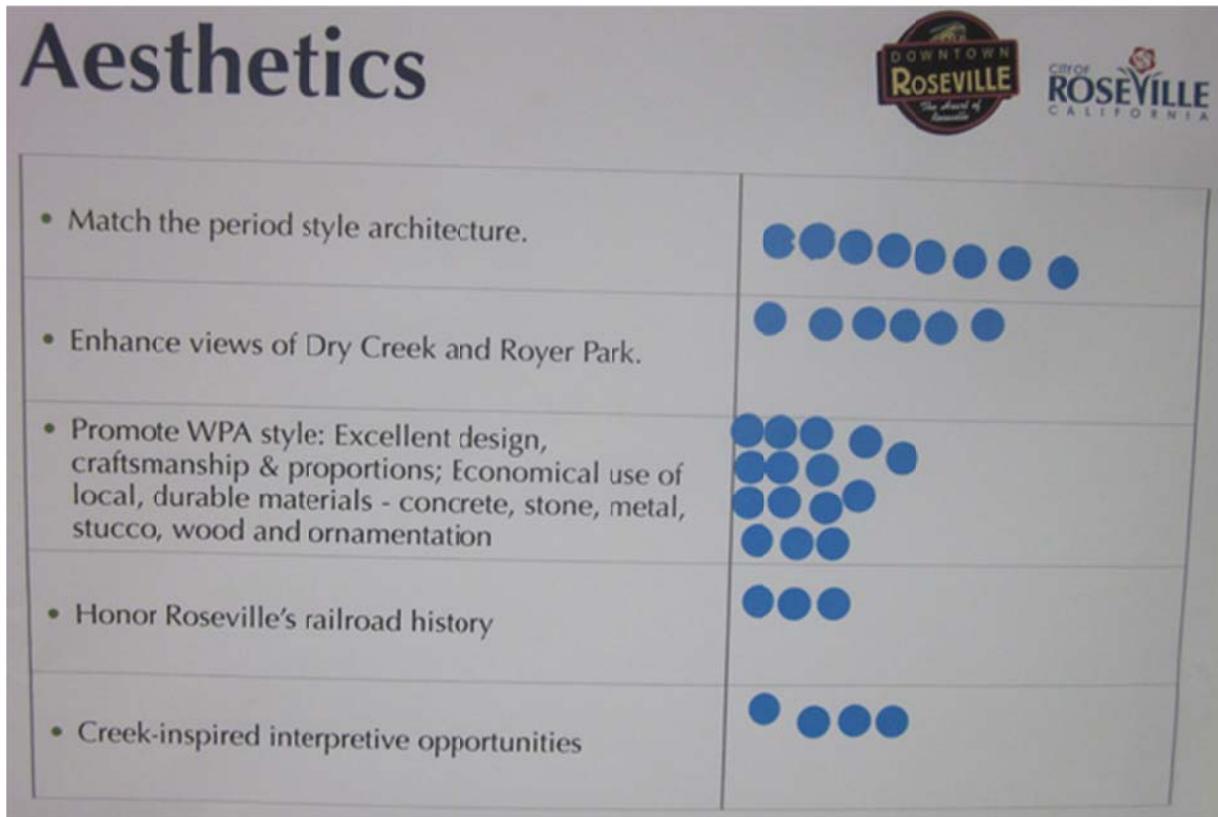
EXERCISE #3 – FUNCTION AND AESTHETIC CONSIDERATIONS DOT EXERCISE

Objective: Obtain feedback on function and aesthetics.

Meeting attendees were provided enlarged posters with lists of function and aesthetic considerations, and asked to place two dots to represent the considerations they felt were most important for the bridge design. A summary of the considerations and quantity of dots placed on each is as follows:



| Function Considerations | Dots |
|--|------|
| Address conflict between high and low velocity users. | 1 |
| Create congregational spaces on the structure. | 4 |
| Safe, comfortable, convenient and connected paths and walkways. | 4 |
| The Class I bridge should meet minimum bikeway standards so bicyclists are not required to walk across the bridge. | 7 |
| Accommodate the future amphitheater, creek walk and plant demonstration garden. | 5 |
| Avoid effects upon the floodwall and accommodate future efforts for bank stabilization and increased flood capacity. | 0 |
| Minimize to the extent feasible the impact on the existing gazebo, picnic area and Class I trail in Royer Park. | 1 |
| Provide accessible paths per the Americans with Disabilities Act. | 1 |
| Minimize the loss of parking along Oak Street, in Royer Park, and near the library. | 11 |



| Aesthetic Considerations | |
|--|----|
| Match the period style architecture. | 8 |
| Enhance views of Dry Creek and Royer Park. | 6 |
| Promote WPA style: Excellent design, craftsmanship and proportions; Economical use of local, durable materials – concrete, stone, metal, stucco, wood and ornamentation. | 15 |
| Honor Roseville's railroad history. | 3 |
| Creek-inspired interpretive opportunities. | 4 |

EXERCISE #4 – BRIDGE EXAMPLES FEEDBACK EXERCISE

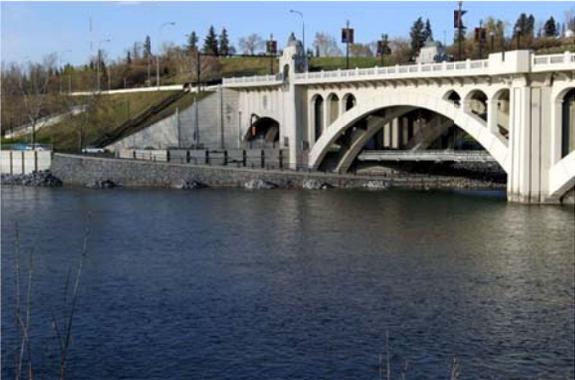


Objective: Obtain feedback on bridge architecture.

Meeting attendees were provided enlarged posters of photographs of a variety of existing bridges, and asked to place a sticky note at photos with a comment of what appeals or does not appeal to them about the specific photo. A summary of the public input related to each bridge photo is below.

| Photo | Comments |
|--|--|
| <p>Photo 1: Bike Trail Bridge, Austin</p>  | <ul style="list-style-type: none"> • Not very attractive. • Harder on bikes and strollers. |
| <p>Photo 2: Ped Bridge, Seattle</p>  | <ul style="list-style-type: none"> • Too modern for area. • Like the idea of a shaded bridge, but not in the way shown in this images. |

Downtown Bridges Public Workshop
Public Workshop #1 Summary

| Photo | Comments |
|---|--|
| <p>Photo 3: Ped Bridge, Seattle</p>  | <ul style="list-style-type: none"> • Like the progressive modern design. • No cover on bridge. • Employing free-form/expressive language into the bridge. • Like the idea of a shaded bridge, but not in the way shown in this images. |
| <p>Photo 4: Calgary Bridge</p>  | <ul style="list-style-type: none"> • I like the traditional design of this one. • Seems over-scaled for the site. |
| <p>Photo 5: Calgary Bridge</p>  | <ul style="list-style-type: none"> • Too cold looking. Design needs warmth. • Interesting design (in a good way), except bridge railing should be more delicate in proportion. • Definitely not. |

Downtown Bridges Public Workshop
Public Workshop #1 Summary

| Photo | Comments |
|--|---|
| <p data-bbox="151 260 448 289">Photo 6: Olympia Bridge</p>  | <ul style="list-style-type: none"> • Too cold looking. Design needs warmth. • Downtown bridge: 1) Concrete; 2) Match walk surface style with Civic Center mid-block crossing; 3) Have creek view point pop-outs. • Like places to pause and survey from the bridge. • This is obviously larger than we need (wider), but aesthetically it's really pleasing. • Like the brick work tiles, slate, art – no concrete. • Would like to see the ability for vendors to sell/set-up booths with adequate room for bicycles/wheelchairs to go by. |
| <p data-bbox="151 840 586 869">Photo 7: Park Trail Tunnels, Boulder</p>  | <ul style="list-style-type: none"> • Too cold looking. Design needs warmth. • Not very attractive. • Posts from Photo 11 design and railing from Photo 3 on this bridge – practical and appealing. |
| <p data-bbox="151 1291 521 1320">Photo 8: Trails/Tunnels, Aspen</p>  | <ul style="list-style-type: none"> • Too cold looking. Design needs warmth. • Stone anchors seem out of scale (too large). • Consider adding a “portal” at the entrance and exit to bridge. |

Downtown Bridges Public Workshop
 Public Workshop #1 Summary

| Photo | Comments |
|--|---|
| <p data-bbox="152 254 698 289">Photo 9: Winnipeg Bridge Decorative Feature</p>  | <ul data-bbox="834 254 1386 541" style="list-style-type: none"> • Consider decorative lighting as an additive alternate given the limited available budget. • Match period lighting (low profile) to minimize light pollution, but maintain safety. • Interesting lighting styles would really improve the look of bridge. |
| <p data-bbox="152 751 470 787">Photo 10: Winnipeg Bridge</p>  | <ul data-bbox="834 751 1052 787" style="list-style-type: none"> • No comments |
| <p data-bbox="152 1255 479 1291">Photo 11: Winnipeg Bridge</p>  | <ul data-bbox="834 1255 1052 1291" style="list-style-type: none"> • No comments |

| Photo | Comments |
|--|---|
| <p>Photo 12: Bicycle/Ped Bridge, Winters</p>  | <ul style="list-style-type: none"> • Like the warmth of the wood and natural steel. • Will the railing height here meet the bicycle standards? Is this high enough? • Rusty look – old looking – dates place. • Bridge is good but needs a “portal” entrance. Not just a railing to start the bridge. |
| <p>Photo 13: Bicycle/Ped Bridge</p>  | <ul style="list-style-type: none"> • Wood surfaces can be slippery when wet and are uncomfortable to ride across (bikes and wheel chairs). • Wood surface may help slow cyclists in this potentially congested area (good). • Bridge – no, walkway and benches – yes. • Like the raised sidewalks (good)! |
| <p>Photo 14: Bicycle/Ped Bridge</p>  | <ul style="list-style-type: none"> • Love the wood and natural steel. • No. • Wood floor hard to bike on and for strollers. • Definitely not. • Too utilitarian. Looks pre-fab – no craftsman qualities. |

Downtown Bridges Public Workshop
 Public Workshop #1 Summary

| Photo | Comments |
|---|--|
| <p data-bbox="152 254 503 289">Photo 15: Bicycle/Ped Bridge</p>  | <ul data-bbox="834 254 1382 464" style="list-style-type: none"> • Looks pre-fab – Not enough craftsman character. • This lacks character, but could be improved possibly with additional lighting treatment. • Yuk. |
| <p data-bbox="152 705 521 741">Photo 16: Ornamental Feature</p>  | <ul data-bbox="834 705 1357 852" style="list-style-type: none"> • Prefer aesthetic to be formed by bridge structure rather than arbitrary add-ons. • Looks like a prison. • Sac State bridge with fishes. |
| <p data-bbox="152 1157 521 1192">Photo 17: Ornamental Feature</p>  | <ul data-bbox="834 1157 976 1192" style="list-style-type: none"> • Like art. |

Downtown Bridges Public Workshop
Public Workshop #1 Summary

| Photo | Comments |
|--|--|
| <p>Photo 18: Steel Bridge</p>  | <ul style="list-style-type: none"> • Timeless bridge • Too rusty looking. Dates the place. • Yuk. • Like this bridge. • Keep this bridge. • Love this old bridge – very simple. |
| <p>Photo 19: Stone Bridge</p>  | <ul style="list-style-type: none"> • Very classic – beautiful! • I like the look of the stone bricks. Looks solid and old fashioned. • Take a look at bridge on Parkside Drive, near La Provence – Nice looking bridge. I like this one as well! • Could this style be added on to the side of a prefabricated steel bridge? • Timeless design. • Love this – Let’s keep it simple and classy. • This would be good for the smaller bridge. • This one, but with plenty of lighting for safety. • Has a lot of character. |
| <p>Photo 20: Wood Bridge/Walkway</p>  | <ul style="list-style-type: none"> • Love trees and natural landscaping. • Do not disturb wildlife. • Great use of landscaping. This should be strongly considered in all bridge designs. • Cement floor with wood railings is nice as well. • Curves are much more aesthetic than straight – Love walkway – Seating also needed for us seniors. • Looks like Roseville. • Nice for library bridge. |



Meeting Notes - Downtown Specific Plan Project Update to Veterans

October 30, 2012, 9:00 to 11:00 am

Veterans Memorial Hall

ATTENDEES

See attached attendance list.

INTRODUCTION

American Legion Commander Mike Scott introduced the attending City and County staff. Mr. Scott mentioned that four Veterans groups use the Veterans Hall and each group has a large membership (over 300 members each for Veterans of Foreign Wars, Fleet Reserve Association and Vietnam Veterans Association, and over 500 for American Legion.) Other groups that use the Veterans Hall include numerous dance groups and the Boy Scouts.

PRESENTATION

Mike Dour, Kevin Payne and Tara Gee provided a presentation describing the projects that are underway in Downtown Roseville. The projects and their anticipated year for implementation are listed below:

- Roadway Improvements along Oak Street (2013)
- Fire Station Re-location (2015)
- Downtown Bridges, Trail & Fire Station Grading (2014/15)
- Royer Park Improvements (future)
- Oak Street Mixed Use Development (future)

See attached slide show for additional information about the presentation. City staff agreed to host an additional meeting at the Veterans Hall in November or December to obtain feedback from the Veterans groups regarding the conceptual design drawings submitted for the design contest. Staff will work with Mary Dietrich (Placer County) to schedule the meeting.

COMMENTS AND QUESTIONS

Comments and questions posed by the attending Veterans group members are provided below. Information provided by City staff is shown in *italics*.

Parking Concerns

- When Royer Park was first built, there were only 8,000 people living in Roseville. Population growth has increased park use and the demand for parking.
- The military is currently down-sizing and troops are being brought back from overseas. This will increase the membership of the Veterans organizations and increase use at the Veterans Hall.
- Veterans groups include seniors as well as veterans who may have suffered injuries during service. When planning for projects near the Veterans Hall, consider the needs of seniors and persons with disabilities. They've helped keep our country free and have earned the freedom to move about.
- Lighting in the existing parking lot is not bright enough. Also, the pavement is in need of repair.
- Activities at Royer Park and in the Veterans Hall often occur simultaneously. The demand for parking at Royer Park outstrips the current supply and sometimes a parking spot can't be found in the parking lot.

October 30, 2012

- Consider moving the tot lot playground or other uses from Royer Park to Saugstad Park to free up room for parking. *City staff explained that Saugstad Park is a former landfill whose boundaries are not defined. The City may continue to use existing Saugstad Park facilities. However, the City would need to conduct a landfill study before conducting further improvement to Saugstad Park. The study would be very expensive, and we don't have funds to conduct a study at this time. City staff agreed to find any existing master plans approved for Saugstad Park.*
- Suggest moving attractions in Royer Park such as Music in the Park to other park sites to reduce the demand for parking at Royer Park.
- Increased connectivity between Royer Park and the Town Square will increase parking demand at Royer Park, since now people could park at Royer Park and walk directly to the Town Square.
- The bridges and trail project should not remove any parking at Royer Park. Don't ask Veterans to bear the burden of this project.
- The Veterans have been asking for more parking at Royer Park for 9 years. Royer Park's existing parking lot should be doubled in size. The City should find money to do this.
- One attendee asked whether or not the triangle-shaped property north of the Hall could be used for parking. Several Veterans noted the presence of the memorial plaque in that area, and that the Veterans occasionally host picnics there. *City staff noted that use of this area is possible if funding is available, but that other options could also be available.*
- If the City establishes parking meters or paid parking along Vernon Street or in the parking garages, that will push demand to the Royer Park parking lot, especially if the new Downtown Bridge is constructed.
- Parking lots on the other side of Dry Creek near the library don't always help alleviate parking demand at Royer Park because park users often bring chairs and coolers to the park and, when they do, they use Dietrich Drive and the Royer Park parking lot to drop off their belongings.

Re-use of R.F. Rube Nelson Bridge

- The Rube Nelson Bridge is in dire need of maintenance.
- The existing wood deck for the Rube Nelson Bridge is sufficient for bicyclists. Bicyclists do not need a new bridge with a new deck material. Don't move the Rube Nelson Bridge down to the Library.
- During construction, the City should minimize the amount of time that the bridge crossing between the Oak Street parking lot and the Veterans Hall is out of service. Also, consider temporary loosening of parking restrictions around the Veterans Hall.
- The existing abutment may be part of the floodwall around the Hall. Maintain the integrity of the floodwall.
- Since the Rube Nelson Bridge is Historical Marker No. 2, will this restrict the City's ability to move the bridge? *City staff agreed to follow up with an answer to this question.*

Proposed Downtown Pedestrian Bridge

- The Downtown Pedestrian Bridge provides little or no increased utility that isn't already provided by the other existing and planned bridges (Library Replacement Bridge and Rube Nelson Bridge).

Roundabout

- Roundabouts increase speed. This will make it difficult for pedestrians to cross Oak Street.
- If traffic signals are needed at the nearby intersections, then what is the value of having the roundabout?

Fire Station Re-location

- One attendee asked what will replace the existing fire station? *City staff explained that it will be redeveloped with commercial and residential uses as identified in the Downtown Specific Plan.*



Meeting Notes

Open House at Veterans Hall for Downtown Bridges Trail and Fire Station Site Grading Project

December 3, 2012, 3:00 to 5:00 pm

Veterans Memorial Hall

ATTENDEES

See attached attendance list.

INTRODUCTION

Mr. Dour introduced the open house meeting format.

COMMENTS AND QUESTIONS

Comments and questions posed by the attending Veterans group members are provided below. Information provided by City staff is shown in *italics*.

Parking

- Although the Riverfront Development on Oak Street is not moving forward at this time, decisions on the Downtown Bridges & Trail project, especially as they relate to parking, need to be made in context with this future project.
- As it currently stands, there is not enough parking at Royer Park to handle existing uses at the park and sometimes the City “overuses” Royer Park. The Cinco de Mayo Festival was noted as a special event that created parking problems. Additional parking needs to be added, and some Veterans suggest doubling the amount of parking. *The selected consultant will be asked to study existing and future parking demand associated with Royer Park as part of their parking study for the project. City staff noted that the Cinco de Mayo Festival did result in parking issues, but that event is no longer held at Royer Park since it outgrew the space. City staff continuously monitors how special events function at Royer Park and other parks.*
- One attendee suggested removing the tennis courts in favor of parking. Another attendee suggested shifting some/all of the tot playground towards the grade school playground to create more area for parking. Another attendee mentioned the idea that removing trees in the parking lot could add several spaces. Another attendee noted that the parking lot shown on the Mark Thomas plan is in an area that the Veterans use for an annual picnic. That area is used for the picnic because the existing Memorial is in that area, and because it is in close proximity to the Veterans Hall kitchen area. *Some attendees, City staff and consultant representatives went outside and looked at parking options. These could also include re-striping the existing lot to gain more parking space. We also discussed options to minimize effect upon the picnic area as it relates to the proposed parking shown on the Mark Thomas Plan north of the Veterans Hall. Funding availability will play a part in any parking decisions, as new parking is not an eligible use for the project’s grant funding.*
- There is a concern that the Downtown Bridges and Trail project may increase parking demand at Royer Park, since it would facilitate pedestrian activity across Dry Creek. An example is that during Downtown Tuesday Nights, attendees might choose to park at Royer Park and then cross the creek to Vernon Street. *The selected consultant will be asked to study this as part of their parking study for the project.*
- Consider restricted parking such as “Veterans Only” parking signs. *It was noted that signs already restrict one bank of parking in the parking lot for Veterans Hall use, but the signs have had limited results.*

Meeting Notes – Open House for Downtown Bridges Trail & Fire Station Site Grading Project at Veterans Hall
December 3, 2012

- Parking nearby the Veteran’s Building was especially important to the group because of the disproportionate number of Vets with disabilities as compared with the general population. It was even mentioned that more than a minimum number of ADA accessible parking spaces might be provided adjacent to the Veteran’s Hall.

Other Miscellaneous Comments

- Potential conflicts between pedestrians and bicyclists needs to be addressed when placing the Class I trail.
- The bridge and trail design must consider homeless issues. In particular, design the trail undercrossings to discourage loitering.
- The Veterans expressed appreciation to both consultants for their efforts to listen to the community and submit designs that attempt to address community concerns related to parking.
- The Veterans expressed appreciation that both consultants propose to keep the Rube Nelson Bridge near the Veterans Memorial Hall.
- The Veterans Group overall expressed that both proposals met the design requirements and had capable firms



Downtown Bridges Public Workshop #2

November 29, 2012, 5:30 to 7:30 pm

Tower Theatre

417 Vernon Street, Roseville

INTRODUCTION

The City of Roseville hosted a second public workshop for the Downtown Bridges, Trail, and Site Grading project. The project includes the design of two new bridges and re-use of the existing R.F. Rube Nelson "Icehouse" Bridge across Dry Creek. The workshop included presentation from two prospective design teams, KPFF Consulting Engineers and Mark Thomas & Company, Inc. Over 55 members of the public attended the workshop; in addition to members of the project Selection Committee which is made up of representatives from: the Transportation Commission, Revitalization Committee, Folsom Road Neighborhood Association, and the Parks and Rec Commission.



KPFF CONSULTING ENGINEERS TEAM PRESENTATION

Following the presentation by the KPFF Consulting Engineers team, the Selection Committee asked the following questions:

- Are the main Downtown Bridge abutments open underneath?
 - Yes, this is to keep them out of the flood plain.
- Is there an ADA accessible ramp at the south end of the Downtown Bridge steps?
 - Yes, all aspects of the design are ADA compliant.
- What is the slope of the Downtown Bridge?
 - 1-2% grade.
- Really like this design, the team thought out of the box.
- Currently, the bike trail ends abruptly at Linda drive; will it be continuous with this design?
 - Yes, the trail will connect to Lincoln Street.
- Does the design incorporate erosion control and creek bank support?
 - Yes, trail designs will incorporate scour analysis and erosion control as needed.
- Most important aspects are connectivity of trails and erosion control for the creek.
- Handrail looks very urban, gets away from the WPA style.
- Would like to see more connections to the historical aspects of Downtown, like the railroad.
- There appears to be a conflict where the bike trail crosses the plaza on the north side of the Downtown Pedestrian Bridge. Also, there may be a conflict with the Icehouse Bridge and the proposed roundabout.
- The creek walk may present a conflict between cyclists and pedestrians.
- Appreciate keeping the Ice House Bridge pedestrian only.
- Concerned about grade change at Ice House, people will want to use/walk on bike trail as well
- Ice House Bridge may have conflicts with the roundabout.
- Is the landing on the Veteran's Hall side ADA accessible?

Downtown Bridges

Public Workshop #2 Summary

- Yes, the ramp will be left as is.
- Have you looked at enhancements to the Ice House Bridge?
 - Yes, there will be a lot of historical preservation work on this bridge.
- Disappointed with the prefab style of the Library Bridge, would like something with an arch that would match the Ice House Bridge style.
 - The design team is open to suggestions.



Questions and comments from the public included:

- The flooring on the Ice House Bridge is difficult for people to use, will this be replaced?
 - Yes, cosmetic improvements are planned for the Ice House Bridge.
- Flood protection is very important.
- Will the trail location take into account future proposed Riverfront development?
 - Yes, the trail is the first step in developing the entire creek walk.
- How will pedestrians cross Oak Street to access the Downtown Bridge?
 - There will be a mid-block signalized crossing.
- Concerned for access to the bridge at Oak Street.
- How will the bike trail get across the floodwall that is currently in place behind the Fire Station?
 - Possibly by reducing the height of the floodwall.
- What is the expected cost of construction?
 - Cannot quote a price at this point, but will have a cost consultant on the team.
- What is the reason for putting the bike trail on the north side of the creek? Will there be conflicts between cyclists and pedestrians?
 - Cyclists will have to slow down in this area.
- Want something that is functional and beautiful, but what about parking at the Veteran's Hall?
 - There will be no impact to existing parking.
- With the amount of use at the Veteran's Hall, parking needs to be expanded not just preserved.
 - The team may look at bringing in a parking consultant.
- Is it possible to run the trail under the bridge as well as on the Downtown Bridge?
 - This is possible but the floodwall will raise some complications.
- Will the bike trail be on the creek side or the Fire Station side?
 - It will be on the Fire Station side.
- Do you have a cost consultant for this project or will it be done in house?
 - Currently we do not have a cost consultant on the team, but one can be added.
- What is the plan for lighting the bridges/trails at night?
 - High Efficiency lights that have the least amount of spillover onto the creek will be used.
- Will there be restrictions on bike usage on the Downtown Bridge?
 - Cyclists will be allowed to use the Downtown Bridge.

Questions and comments related to other Downtown projects included:

- Will there be a barricade along the creek near the Ice House Bridge and the roundabout? Vehicles have driven into the creek in the past.
 - The City will evaluate the need for a barricade as the roundabout and the bridge rotation are designed.

Downtown Bridges

Public Workshop #2 Summary

- Concerned for congestion at the park with the proposed amphitheater, congestion is bad already and will only get worse.
- How will the parking at the Library, near the amphitheater, impact the already limited parking?
 - The lot will be restriped to retain as much parking as possible.

**MARK THOMAS & COMPANY, INC. TEAM
PRESENTATION**

Following the presentation by the Mark Thomas & Company, Inc. team, the Selection Committee asked the following questions:

- Like the incorporation of the WPA style, the Railroad elements, and the details on the bridge. Like the bike trail separate from the creek walk and the rotation of the Ice House Bridge.
- Like the separation of the creek walk with the option for cyclists to continue or cross
- Currently the bike trail ends at Folsom Road, is the idea to connect behind the existing Fire Station?
 - The bike trail would connect under Lincoln Avenue.
- Will there be any cosmetic upgrades to the Ice House Bridge?
 - Yes there will be some preservation and restoration such as sandblasting the steel structure and replacing the wooden decking with a concrete deck.
- Would like to hear more information about the shade structure on the Downtown Bridge.
 - The purpose is to have a seasonal shade structure that will be easy to install, maintain, and take down; something that is cost effective for the City.
- Will there be lighting treatments on the Library Bridge?
 - Yes, there will be lighting.
- What is the width of the Library Bridge?
 - About 10-12' wide.
- Question the feasibility of rotating the Ice House Bridge.
 - The team has done some preliminary engineering and believes it will be feasible to pick up the bridge and set it down in its new alignment.
- It looks like the proposed plaza area extends into Royer Park, concerned that it will take space from the play area.
- The bulb outs on the bridge and the plaza area; it is a lot going on in one place.
- Can you describe the underpass at Lincoln Avenue in more detail?
 - The underpass will be a full Class I bike trail, it will be well lit and very tall and wide to receive as much natural light as possible.
- Will the small parking lot near the existing Fire Station go away?
 - The lot will be reconfigured.
- Will the bike trail along the creek go up Folsom and connect into Miner's Ravine?
 - Yes.

Questions and comments from the public included:

- The Ice House Bridge seems too narrow for a Class I bike trail, can it be widened?
 - The bridge will not be widened but will meet the minimum 8' requirement for a Class I trail.
- Does the Downtown Bridge landing into Royer Park cut off access to Dietrich Drive and remove some parking spaces? Parking is already constrained in this location.
 - It will remove some spaces but the lot can be restriped to accommodate as many cars as possible.



Downtown Bridges

Public Workshop #2 Summary

- There will be a lot of traffic into Royer Park coming from the Civic Center, but there is also a lot of traffic going the other way.
 - The plaza area at the landing on the park side will accommodate the movement of a lot of people.
- Would like to have the additional parking lot near the Veteran's Hall included with this phase of the project.
 - The design team can work with the City on the budget for the project to see if this is feasible.
- Currently the Ice House Bridge structure keeps water out of the Veteran's Hall, there is also a chain link fence near the Hall, 2' flood wall would be better to protect the Hall.
 - The design allows for flood protection.
- There seems to be a lot of accessibility and parking issues, there is also a monument at the Veteran's Hall near the proposed parking lot.
- How will pedestrians access Oak Street?
 - Walkway connections are proposed from each bridge leading to Oak Street.
- If the Ice House Bridge is rotated, how close will it be to the Veteran's Hall?
 - The exact measurements are not known at this stage in the design process; however there will be enough room for maintenance to occur and the bridge will not but up against the building.
- How tall will the tunnel on the north side of the Downtown Bridge be?
 - It will be tall and wide to let in as much natural light as possible.
- Like that the bike trail is being routed to the south side of the creek, however is it possible to leave the Ice House Bridge where it is and tie in a Class I bike trail on the north side of the Downtown Bridge?
 - This is possible, but was not included as a part of the design due to the conflict with pedestrians. This suggestion can be looked at by the design team.
- Don't recommend the proposed parking on the east side of the Veteran's Hall, currently there is an ADA accessible ramp at that location that is used when the Hall puts on outside activities. Would suggest moving the proposed parking to the left of the existing parking, where the old playground is. It appears the old playground is being replaced.
 - The old playground is being refurbished right now, but is not being replaced and will remain in its current location.
- Currently there are not enough accessible spaces at the Veteran's Hall for the amount of users it accommodates.

Questions and comments related to other Downtown projects included:

- Would extending the Downtown Bridge across Oak Street present a problem with the design?
 - This would need to be looked at by the design team.
- Will there be a cross walk or a signal at Oak Street?
 - Yes, the Oak Street Improvements Project, which is currently under design, will include an enhanced pedestrian crossing to slow vehicles and increase pedestrian visibility.

Next Steps

- Next Selection Committee meeting December 17th



DOWNTOWN BRIDGES PROJECT

Building Connections to Downtown Roseville!

The summary of public comments below reflects a compilation of input received between November 14th and December 6th. Public comments were submitted through the Downtown Bridges website, display stations at the Roseville Civic Center and Downtown Public Library, the Downtown Bridges Public Workshop on November 29th, or via email and fax.

Public Comments received for the KPFF Consulting Engineers Design Proposal

| |
|--|
| <p>I like that you guys keep the icehouse bridge in place. I also like the idea of having art pieces on the bridge.</p> |
| <p>I really love your design it looks very nice.</p> |
| <p>Personally I like the plans of the KPFF Design Team much better. Good design, with good ideas helping the City move forward! Plus looks more appealing to all cultures & opens up the landscape for much better use.</p> |
| <p>This is Best. I like that this option has neat sculptures at the bridge along with seating options that are like sculptures. It's artistic! Like that it has rails that you can see over and through like the ramp for downtown bridge and bike access to both bridges.</p> |
| <p>I like it because it's very organized. The boys like the sculpture skateboard ramp bench. Mom likes the sculpture garden & neat benches. Teen boy likes the bike trail going by the library. Girls like leaving the Icehouse bridge in its spot. Mom likes the continuity of design with existing buildings and bridge design (flat bottom) w/Icehouse bridge. Concerns: Library parking, Bike path crossing the downtown bridge.</p> |
| <p>I like the elements of each proposal. I like the decking material of KPFF that make a visual link to the Civic Center Plaza but, I don't like the modernistic look for a traditional downtown. I also think the Ice House bridge needs to be pointed as shown in the specific plan.</p> |
| <p>I apologize, but I incorrectly labeled my previous post. This is the correct posting. I feel this design is a starting point, but I am much more impressed with the MTDT design and vision for our City (SORRY). Our city needs to be aligned with a vision that fits its creative and progressive direction; one which our City's leaders have already exhibited within in their planning. The MTDT design adds an additional element of charm, which is missing in this design.</p> |
| <p>I like how KPFF designed the bike trail to not take out additional parking as well as putting the ice bridge on the other side of the Veteran's Hall. This design seems to not take up a larger foot print of Royer Park where the bridge enters into the park compared to the other designer. I still would like to see a classier look for the bridge with more hometown accents.</p> |
| <p>The downtown bridge design and surrounding area looks like it will be very appealing to walkers, runners, cyclists, and lots of families looking for a friendly atmosphere for a financially friendly afternoon outing/picnic. The artwork will work as great conversation starters as well. I bet the city leaders can envision a quaint ice cream vendor alongside the walkway to entice those Residents with a sweet tooth!</p> |
| <p>Great design. I think it will be great for the people of Roseville.</p> |

Public Comments KPFF Consulting Engineers

The general concept of the KPFF proposal, with the bike trail moved to the downtown side of the creek, is a good idea. That would keep "through" bike traffic on the trail separate from pedestrian activity in the park, preventing conflicts. The main pedestrian bridges ("downtown" and "icehouse") would be free of bike traffic, so pedestrians would have a safer and more enjoyable experience. Bicyclists would also benefit because the new "library" bridge can have a deck much more suitable for bike traffic than the plank deck on the icehouse bridge. The icehouse bridge would remain suitable for pedestrian use, and its current location would be fine, as shown.

Love the bridge idea and openness. Seems inviting to all

I like the organization of this plan and the Washington round-about has my vote, but it feels too modern. I would like to see some of the classic design from the other project incorporated into this one. You worked so hard to emphasize the old railroad feel of our town and this seems to go too far in the opposite direction. As a Royer Street resident, I would like to continue to feel "at home," even after this project is done.

I am intrigued by the concept of locating the bike trail on the north side of Dry Creek and using the library bridge to connect it back to the south side. Realize that this concept in effect renders the Icehouse Bridge unnecessary (its location and orientation is not conducive to where people will primarily be moving to and from.) I would hate to see the Ice House bridge not being used frequently and sitting there as a relic. It would be nice to see an exploration as to whether it could be retrofitted as the Downtown Bridge or the Library Bridge in this design concept? One other comment...the architectural character of the Downtown Bridge in this proposal seems to lack special quality. I would like to see the design of that bridge revised if this proposal is selected.

I'm concerned with the bike path's impacts on wildlife. I'm also concerned that the trail will make regular pedestrians less comfortable since they would have to share the trail with fast runners.

Where are we going to Park?

How does either of the proposals deal with the parking required for the Library? The lot in back is not enough to accommodate the amount of cars that use the Library. I hope the Planners Do Not Expect the patrons of the Library to use the parking garage, which is roughly 2 blocks away. If so, the Planners are not taking into account the elderly who use the Library extensively and the times that it is very cold and rainy. I do hope that the local Dry Creek lot is expanded and left close to the Library!

I think KPFF has done a better job with the lay out of the bridges and bike path. Everything will center around bringing people to downtown. Because they aren't moving the historical bridge you get rid of the historical headaches due to moving it, and you probably reduce the total project cost since there are only 2 bridges. For those that like the Mark Thomas architectural features (the aesthetics like stonework, flooring), that can be changed I'm sure. Based on layout, practicality, and cost, I'd give it to KPFF. Mark Thomas may have the better looking bridges, but that can be changed to how the public wants it.

Much better idea and design concept to keep the Ice House Bridge and only build two bridges. This provides less impact to the creek and wildlife along with being more cost effective.

This bridge is straight and open to allow efficient traffic flow and feels like a fit where volume is a concern (which is not important for this downtown park location). It is too modern and doesn't communicate warmth or community. The straight lines aren't inviting for lingering but instead feel too industrial, commercial.

I like the attention to lighting which does not increase light pollution, and leaving the ice house bridge in place, without additional parking in the small grassy triangle area at the base of the bridge. I am concerned about traffic congestion with additional parking, entrance to resident driveways, and the dangerous cross street at Sutter St. /Park Drive. I find the other proposal more aesthetically pleasing, and would prefer a combination of designs.

I like the modern look of KPFF's design. Looks clean, inviting, and up-to-date. The handrail design allows people to enjoy looking at the creek. Keeping the icehouse bridge is a good use of existing resources. The layout looks like it will function better than the other proposal.

I love this design but think the bridge designs do not show much, at least on this visual computer plan I see. I am thinking a mix of the two designs in regard to the bridges design would be a perfect solution. The flow of the movement looks to be very in keeping with what I have heard most want from meetings. My concern is that parking garage has been nearly full during the day when I visit the area since the city removed the parking at the civic center. With the additional development it would seem that parking has been overlooked.

Public Comments
KPF Consulting Engineers

This design is updated, light, and inviting. It covers every aspect well, inviting young and old alike! Great lighting, seating, an area that invites you downtown!

Likes: - Icehouse bridge is preserved - bridge designs in terms of style Dislikes: - I'd prefer the bike trail south of the creek - Don't like the use of Dietrich. Would rather all vehicular traffic be kept off that road. Summary: - Why so many bridges, and why such a grandiose City Center bridge? I'd prefer keeping the area more quaint. A single well-designed bridge can accommodate traffic to and from the park for everyone, including library patrons. And please keep the Icehouse bridge exactly as it is.

I've walked along Dry Creek for years and have been awed by its simple beauty and myriad wildlife in all seasons. I understand the need for change, but would hope for the least possible impact on the area. I'm leaning towards the KPF concept because it leaves the Icehouse bridge intact and plans for the replacement of the popular crossing to the library. I would, however, prefer to see a less imposing City Center bridge plan.

After reading both proposals, including the current comments and concerns that are written, my choice would be for the KPF design. I like that the current "Ice Bridge" remains in place, both for cost and historic value. I like that the bike trail is suited for a pleasing ride, yet out of the way of foot traffic. The attention to the lighting as to the environment is important. The downtown bridge has a grand staircase area that is beautiful, the style of handrails are perfect for viewing and at the same time, aesthetically pleasing. The artistic thoughts for the seating area and having a "sculpture garden" is an artistic, and also "fresh" addition to the site, along with using the established pattern of paving for continuity of a matching fit to what is already in place. This would be a beautiful area with the historic feel that represents both the past and the future for the city of Roseville perfectly. I believe that KPF is by far, the best proposal for the Downtown Bridges Project.

By keeping the Nelson/Ice House bridge in place, I fear the encroachment of the Oak St. /Washington Blvd. roundabout will endanger the pedestrians using this bridge. I like the Royer Park landing of the 'Downtown' bridge is better incorporated into the existing park. Both of the Downtown bridge designs I believe would benefit from a different start point, that being across Oak St. and eliminate the at-grade crosswalk. Better pedestrian safety, more display opportunity if that is one of the goals of this project. Both designs need to address the parking needs of the area in some way, it seems to me there is a disconnect in either the direction, or presentations by and to the city. There is no doubt that more than one bridge is needed for adequate access between downtown and Royer Park. In short, the resulting project needs to be a combination of the two plans presented. Both have merits.

Whatever the plan...please make sure to keep enough parking to enable practical use of the surrounding areas including the park and the Veteran's Hall - otherwise it becomes inaccessible for the disabled, for families, etc. Please make sure to keep and/or build in enough parking.

No, the whole project is too upscale for Roseville.

I'd like to have seen more attention paid to aesthetics and how the design of the bridges link to one another. Intersecting the trail and the bridge at grade increases the risk of collision and conflict.

Leave Ice House Bridge in place. Parking at Vet's Hall needs to be enlarged to at least double in size from 42 to 84 parking spaces.

I think more discussions should be given to the pedestrian crossing on Oak Street. It should be included in the project. It was good to see that the Ice House Bridge would stay as is. Cost wasn't discussed; I think the cost should have been discussed more. There was a lot of discussion about maintenance. Don't you have to know how much money is available before a design can be selected? There can be a drastic difference between cost and design.

As with any construction project, the potential for crime increases. Consultation and input from the Roseville Police Department is vital to mitigate this potential increase. I would suggest involving the Police Department early so that issues of crime and traffic can be addressed. Crime prevention and crime prevention through environmental design (CP & ED) concepts are very important for the safety of those who use the facilities.

I like this concept better because it makes the bike trail flow better by going under cross streets and off the main road, it also ties in the park to the downtown area.

Doesn't seem as flexible as the Mark Thomas proposal.

Public Comments KPFF Consulting Engineers

This proposal looks to be more cost-conscious. It also seems flawed because the bike trail and river-walk on the same side of the creek is problematic because of unavoidable congestion. The bike trail winding around the amphitheater seems to be a poor design. Better to have the bike trail away from the library/amphitheater/river walk/roundabout/multi use facility/automobile traffic.

Let's start with the FINE Print; ---- KPFF did! They went into detail as to what & why they designed it the way they did. I think they looked more ahead to the future of how things will be utilized, by you, your son & daughter, your grandparents, & your future grandchildren. Where's our future going? A healthier generation. More & more people are bicycling, walking, running, to keep in shape. Their design gives it a flow for all to enjoy, & adds the wide open space, the views of the park, creek, and downtown for all to enjoy. Roseville is built on "Tradition, Pride, & Progress"; I believe KPFF has touched bases on all. Let's move ahead & show how Roseville can be "Always Amazing"!

KPFF has my vote! Their design is by far the best for a thriving Roseville!

Stark. I don't think the modern lights add character or distinction. This is part of Old Downtown.

I want to agree with the comment here about the crossing on Oak Street. I didn't see that in either design. This one pictures "future developments" that are not in the other design. --Have we considered what "future development" we want next to the park? I would like to see more open space around the creek rather than less. Royer is popular and a bit cramped. I imagined a contiguous space from the plaza across into Royer.

I like the overall plan. I like how the Ice House bridge is preserved. I particularly like how the Civic Center is tied in with the main bridge/walkway. This design seems to be the most pleasing and I especially like the placement of the bike trail.

I like this design team's out of the box thinking! They also appear to be good stewards of the city's money by suggesting many cost saving elements to their design. Leaving the Ice House bridge in place and not trying to retrofit it for cyclists seems like it may be a wise move. The pre-fab Library Bridge also seems very practical as a cost saving measure. I do like that they planned for the future amphitheater in the path of the bike trail. Their design seems to have more accessibility options to cyclists who want to frequent the businesses downtown, although the appropriate turn off points are not indicated explicitly. I like the at-grade bridge that will appear to be an extension of the street. Even though it may not be in the WPA style, I like the minimalist design of the bridge, especially the railing that will allow full views of the creek and not dominate the landscape. I also appreciate the consideration of the deck lights to minimize light spilling into the creek and "polluting" the night sky. I do not think it is a good idea to plan to eventually have the nature trail use the bike trail. However, I think the nature trail would be better located south of the creek in the future to be close to the park, farther from the downtown infrastructure, and built with a natural surface that would have a smaller footprint. I also think it may cause too many problems for cyclists passing through downtown to have to intersect the pedestrians crossing onto the downtown bridge. A below grade route with the option to pull off into downtown, like they propose at the Ice House Bridge, would be much more preferable. I have the same comment about the other design, but I want to emphasize that connectivity for cyclists to Vernon Street and north of the railroad tracks needs to be planned for. This is as simple as providing protected splits that would lead to bike lanes into downtown.

I prefer the proposed plan from KPFF Consulting Engineers: 1.) From a historical perspective retaining the placement of the R.F. Rube Nelson "Ice House" Bridge is preferred. It will also be more cost effective by just updating it and not moving it. 2.) I am very happy with the replacement on the popular crossing to the library. The new library bridge is very attractive even though they said it would be a pre-fab bridge. I'm sure that will be more cost effective of the two proposed plans. This deck will also be more suitable for bike traffic. 3.) KPFF's plans for the downtown bridge are the most attractive architectural design. The grand staircase area is very inviting, along with the adjoining sculpture garden and seating area. The decking material of KPFF attains the continuity to the Civic Center Plaza. The attractive yet functional design of the handrails allows for excellent viewing of the creek and surrounding landscape. The energy efficient LED lighting provides lighting only on the bridge, thus avoiding excessive light pollution.

Leave the Ice House Bridge alone. Make other creek crossings tie to this bridge - classic. Too much "pavement." Can't see any LID elements.

Public Comments KPF Consulting Engineers

The MT Co design was better and more complete than the KPF design. Since no costs were included it could not be determined if either one was cost effective. I hope that information is available to the Selection Committee when they choose which design team is awarded the project. Locating the bike trail in the park on the south side of the creek makes much more sense. Separating pedestrian traffic from cyclists is safer and more reasonable. The cyclists can access the downtown area by walking across the bridges or using the bike trail access points on either end of the park if they chose to do that. Reorienting the Ice House Bridge may offer a better path into the park but it is more expensive and may create problems with the structure itself while moving it. New bridge footings and abutments must fit into the existing grade and how the bridge interfaces with the Veterans Hall facilities appeared to be a potential problem. Nothing appeared to have been done to resolve the parking concerns of the Veterans. Comments from meeting attendees indicated a conflict between residents who use the park and playground and the Veterans with their parking concerns. The Parks Department should be asked to find a solution to those opposing interests. The existing library parking is marginal at best and the designs did not offer any improvement. If anything, the additional vehicles who may park in the library lot to access the park will exacerbate the problem. Some additional library parking should be included in the design. The Downtown Bridge to Royer Park was the main focus of the design presentation and will be the main access for pedestrians between the park and the downtown area. How that is compatible with the vehicle traffic in Oak Street is unclear, regardless of whether a roundabout is used at Washington and Oak Street or not. It seems dangerous and ill advised to design a surface grade pedestrian crossing at the location shown in the design. It should be moved to the nearest traffic signal or changed to an elevated grade crossing. Regarding the proposed roundabout for the intersection of Washington and Oak Street, I cannot visualize how that will improve traffic flow, particularly with signals added at Oak and Taylor and Oak and Lincoln as proposed. I assume the traffic simulation models indicated it is workable but I can only see it as being worse than the current traffic flow. Lighting was mentioned as important to security. Because there are homeless people who use the park and that is not likely to change in the future, a police substation located in the park would enhance security of the area and make people feel safer in the park and more likely to use it. The riverfront development is the key to drawing people to downtown Roseville. Until that project is completed, the number of people visiting downtown is not likely to increase, regardless of the other downtown projects including the Bridges and Trail Project. Thank you for the opportunity to provide input on this project.

I prefer KPF's plan: I walked out of your meeting just before the end. It is obvious the selection committee was showing bias and already had their minds made up to go with Mark Thomas and Co. Does the City want to move forward? Or stay in the WPA era? We already have old Roseville, Cherry Glen, the Sierra District, and Vernon Street, which has implemented WPA. A team you've already worked with, well yeah we have to go with them, What about the future? What about going forward, let's have new Roseville. Go above and beyond. Open up the dreams and aspects of a thinking, reinvigorated Roseville. Are we moving forward or staying stagnant! The same thing that got Roseville in trouble before. I agree with the Veteran's I believe their day will come when a new hall will be built for VFW also. This plan is about the bridges and Royer Park and connecting with Civic Center and Downtown. Move forward with new ideas, new design, new blood, new vision, not backwards with WPA era, WPA bridges, WPA walkways, WPA design. I love them and their history and work ethic but let's move the City forward.

Love, love the art slide bench etc. Long time by Royer Park, grand parents also. It is bike and pedestrian friendly. May be inflation by the time project is finished.



DOWNTOWN BRIDGES PROJECT

Building Connections to Downtown Roseville!

Public Comments received for the Mark Thomas & Company, Inc. Design Proposal

I like most of this design. I like the light posts, the building materials, and the overall look...it seems to fit in with the current decor of downtown Roseville, which is traditional and classic. The bridge has a railroad inspired look, which fits perfectly with Roseville. I am not happy about the ice bridge cover being removed. I really like the look, but hate the wooden flooring, (I ride/walk it once a week). Isn't there some way to save the upper part of the bridge and make it look cohesive too? Good luck!

I was reading the notes, and saw that "shade structures will be incorporated for use during all seasons." In the summer time that will be very important, however I do not see any imagery that tells me anything about how shade will be implemented. We all know trees take time to grow, so if trees are to be incorporated, what kind, how long will they take to mature, etc.? And if not trees, what?

This design feels very natural and compliments surrounding Roseville history and architecture. I like that this design strays away from a more modern concept and connects with Roseville's historic roots. The Mark Thomas company's overall project design is also much more comprehensive and appears to be a lot more flexible in terms of cost and integration with future Roseville design projects. Great work! I very much appreciate the transparency and voice afforded to Roseville citizens by their local government. I'm excited about the future of this community.

I really like how you guys keep the bike trail on the Royer Park side and have it go under the downtown bridge. I like the simplicity in the style and design of these bridges.

I enjoy the sense of character that this bridge evokes. The design's functional ability is obvious, but the aesthetic design aims to blend it with pre-existing elements of downtown, which I feel is necessary. It seems unique and inviting to use.

I really like the Mark Thomas' design. Very classy!!

My choice. Great enhancement to the area.

Very attractive bridge, exactly what Roseville needs?

It is my belief that The Mark Thomas Design Team has a brilliant vision for a progressive City. It is my contention that the Mark Thomas Design greatly enhances the Cities charm, and brings to life an area worthy of a world-class design for a future world class City. Charm equals charm, and that is what I see in this design. I expect this to be a rewarding and model project (based on their vision) deserving the cities full and complete attention.

Like the wideness and seating of downtown bridge. Connects directly to plaza and make ease of parking for park functions and access to both Park, Library and Downtown. Refurbishments and general tying in of the stones.

I like it because it is organized. Mom likes the bridge lighting. Teen girl likes the literary quotes in decking (Mom too). Concern: arches on new bridges not consistent with Ice House Bridge, Library parking.

I like the more traditional elements except for the gun turrets.

I like the other design better. This one seems a bit stark. I also like the ice bridge on the other side of the Veteran's Hall.

Public Comments
Mark Thomas & Company, Inc.

The MT Co proposal closely follows the current Downtown Specific Plan, but does not show how some critical issues would be handled. It shows using the icehouse bridge for the main bike trail, but the existing deck on that bridge is unsuitable for bikes. The proposal notes "deck replacement," but without a hint what it would be replaced with. It is also unclear how the "new handrail treatment," showing a half-through truss bridge, would be implemented on the existing through truss span. It seems the reuse and refurbishment of the icehouse bridge has not been thought through.

I really like the look and functionality of the library and downtown bridges. Very thoughtful and good looking design. I'm not so sure about the ice bridge though simply because I'm having trouble understanding what's going to be done with it. I am rather fond of the covering on the ice bridge if you're getting rid of it for aesthetic reasons. My only concern would be the potential light pollution from those lights but that's fairly minor. I definitely prefer this one.

The architectural character of the bridge designs in this proposal is a good fit for Royer Park / Downtown. They have a timeless quality. There are some nice details too. I particularly like the bowed overlooks on the Downtown Bridge as it passes over the creek. I do think that both proposals could improve how their Downtown Bridges land on the Royer Park side, but hopefully that will be developed as the design process progresses.

This is a great plan it thinks of the many people that uses the veterans hall. This is the only plan that Parking is thought thru

I'm concerned with the bike path's impact on wildlife. Why put up statuary of wildlife, when the project will at the same time be hurting wildlife?

We need up close parking Please!

I prefer this bridge design. It has character, shapes that are interesting and organic lending to the location. I will walk with my children to this bridge to play, look over the edge and picnic.

I really like your downtown bridge concept. I'm mildly concerned with the width of the icehouse bridge for a class 1 bike trail. I do however prefer the alignment of the KPFF library bridge. I like the way the walkway wraps around the planned outdoor amphitheater. I like the alignment of the class 1 bike trail on the south side of the creek but I think the planned pedestrian walkway on the north side should be bike friendly also. Slow bike friendly. I think both design teams did a good job with their presentations and I think the City of Roseville is on the right path.

this is the design I prefer for available parking and access to Royer Park and the Veterans Hall

I like this bridge best.

I really like this design. I vote for this one.

Winner. The round lines are much more relaxed and invite a restful feeling along with the traditional lighting. This design is more appropriate for this park/downtown link. This looks like a park bridge which invites you to relax where the other design looks like a people mover encouraging you to quickly get across. Reconsider the square shape of the supports under the light posts, maybe a softer look to blend with the round edges instead of such a contrast.

I prefer these overall design elements, however have 2 concerns. 1. Lack of attention to light pollution (which the other plan addresses) 2. Additional parking to the right of the Veterans Hall, which may cause dangerous congestion for resident driveways near the entrance and for traffic turning onto Park Drive.

This is too big and cumbersome. It is heavy and not very inviting.

I like this design better, primarily because it keeps the bike trail south of the creek and provides plenty of space on the north side for foot traffic. Keeping the two separate is very important, as experienced by numerous towns and cities across the country. This way we get the best of both worlds. My one issue is the movement of the Icehouse bridge, which I think is completely unnecessary. If I had a choice, I'd reject both designs, but this one is the better of the two. Below is the same comment I posted on the other design. Summary: - Why so many bridges, and why such a grandiose City Center bridge? I'd prefer keeping the area more quaint. A single well-designed bridge can accommodate traffic to and from the park for everyone, including library patrons. And please keep the Icehouse bridge exactly as it is.

I really like the Mark Thomas design. It looks very elegant, but simplistic at the same time. Really like the idea it is more than a bridge but also an area for people to relax around and around the bridge. This really seems to blend in with the area, like it has always been there.

Public Comments
Mark Thomas & Company, Inc.

I prefer the Mark Thomas design, because it keeps the bike trail in the park, It provides access to the front of the Veterans hall from the Oak street parking area, The rear parking area would provide handicap parking for veterans using the other entrance to the hall, both upper and lower floors. The front parking area could be upgraded and improved by removal of the two sycamore trees repaving and stripping. The ice house bridge would be refurbished.

Additional comment, on ice house bridge, with the new concrete stamps, the covering could be made to look like railroad ties.

I like this bridge design much better.

I like this project, but do have concerns. One of the items I like is the rotation of the Ruben Nelson/Ice House bridge, but do have concerns about the landing position in Royer Park and the proximity to the front of the building. I like the continuity of the primary/new bridge over the creek being in-line with the plaza in front of the Civic Center. I would encourage a look at the use of a bridge over Oak Street in place of the at grade crosswalk currently planned. This would provide more pedestrian safety, as well as opportunity for display space. A greater opportunity to incorporate historical themes into this bridge as well. I think the 'landing plaza' in Royer park is overdone, and will intrude into currently used spaces. Additional parking is needed for the park, and Veteran's building, but I understand that is not a part of these plans. I must find the appropriate outlet for those concerns. The concept of using some of the park on the 'back side' or east side of the building is not suitable in my opinion.

Whatever the plan...please make sure to keep enough parking to enable practical use of the surrounding areas including the park and the Veteran's Hall - otherwise it becomes inaccessible for the disabled, for families, etc. Please make sure to keep and/or build in enough parking.

I am just going to say I would go with this design and of course I do have some misgivings. I would like the playground moved for more parking for the Vet Hall.

Innovative bulb outs on main bridge make it more of a destination. Interesting concept of having a seasonal cover. Good tie in of design elements across bridges. Considers the addition of a river walk. Great focus on all the expected flow into Royer park. Appears to be more costly, I hope we can fund it.

Over designed and overpriced to area. No to Vet's Hall parking on east side. This area is used by the Vet's picnics and outings.

Bike trail should be on the other side of creek and Ice House Bridge should stay as is. Money was not discussed. Why a roundabout? I think it makes more traffic congestion than the current light.

As with any construction project, the potential for crime increases. Consultation and input from the Roseville Police Department is vital to mitigate this potential increase. I would suggest involving the Police Department early so that issues of crime and traffic can be addressed. Crime prevention and crime prevention through environmental design (CP & ED) concepts are very important for the safety of those who use the facilities.

I believe the Mark Thomas proposal is the best. The only thing I wonder about is when the parking spaces are added behind the lodge can they figure a way to keep the bar-b-q area. Maybe move it a little. We definitely need to preserve parking for the hall in light of disabled veterans.

This design is more expansive and feature-rich. It allows for more varied use of the Civic Center Bridge. This design is preferable.

Not much to like here. A lot of old ways, old designs, & old ideas! Lines are too harsh, seems too costly, why move the Ice House Bridge. What is the cost alone to move the bridge?? Let's look to the future, more open ideas, & ease for bicycles & pedestrians! And who is paying for these canopies on the main bridge to go up, and then taken back down? All the time more costly to implement this & pay for park personnel to maintain. Remember the plan is for bridges & bike trails, Are we being fooled with parking that is not in the plan.

Public Comments
Mark Thomas & Company, Inc.

I hope that there is a process for making changes in the design. -I definitely vote for the old style of the lighting on the Downtown Bridge. What is the lighting for the Library and Icehouse bridges? It should be in the same style, and not glaring and minimizing pollution. -There seems to be a lot of clutter in the design of the Downtown Bridge, boxy elements with no apparent function? -I don't think that "vendor activities" should cause the bridge to be bigger than needed. -I'd rather see a smaller, simpler design that fits into the scenery, rather than a showpiece bridge. I think the icehouse bridge should set the tone for the other 2. I like the simplicity of the Library bridge here. I wish that the bridge could connect from Civic Plaza across Oak Street--was there attention to that street crossing? As a segue way somehow, rather than an interruption of the public space?

I like the design of the bridge. Its modern and open design will bring a fresh look to Downtown Roseville.

The Mark Thomas Design Team has captured the criteria set by the City. The bridge will be good for walking and biking. Overall, it is a nice design.

I think both designs are big steps forward for Roseville and I would accept either. I like that the MTC o design allows cyclists using the bike trail to travel south of downtown unencumbered by separating the trail from the pedestrian additions being added. However, I did not see a clear explanation of how cyclists who were travelling to downtown as a destination would have easier access to the businesses on Vernon St. It appears they would have to walk their bike up the steps and over the new Downtown bridge, then across the crosswalk and up another set of steps. Or possibly use the reconfigured Ice House bridge or Library Bridge to get across the creek and then ride in traffic to the main strip. Likewise in the reverse direction, I am not sure how the completed bike path will be accessible for cyclists connecting from north of the tracks, for instance from the bike path being extended along Washington Blvd. I would like to know that this system wide integration is being planned for. While I prefer modern architecture, I believe the design team has met the criteria of a WPA style, and has the advantage of designing the architecture of the surrounding area so that they will integrate seamlessly. I like the modular tent structure design that can be changed with the seasons. Lastly, even though I appreciate that the team has separated the cyclists just passing through from the majority of foot traffic by locating the trail on the south side of the creek, it seems to make more sense given the location of the more natural park setting and children's facilities on the south side of the creek to plan for the interpretive nature trail to be located eventually on the south side of the creek. Perhaps a hybrid of both team's designs would allow for a separate nature trail in the future, would limit cyclist and pedestrian conflicts, and provide a more inviting route into downtown for cyclists with a well thought out connection to the north of the tracks.

Neither of these bridge designs feels very organic but the Mark Thomas one is better. However, all the cement definitely subtracts from the nature of the creek and is a shame to the beautiful naturescapes they are trying to connect. I dearly hope that the Icehouse bridge can remain intact as that is a beautiful organic piece of construction that I feel exemplifies nature and the classic feel of Roseville. It is a shame for me to see these designs both creating a concrete jungle feel instead of a natural park like feel.

I like the design proposed by Mark Thomas Design Team best. The bridge is very modern and intriguing. It's hard to see, but I think benches/concrete blocks or somewhere to sit on the bridge would be a nice addition. As someone else had mentioned, it would be great to have an extension to the bridge from the Civic Center. Anytime to walk from downtown across Oak Street is crazy with a lot of traffic. During such events as 4th of July this would be great access from the parking structure and plaza to the park with little impact to traffic. The bike trail seems to flow very nicely through the park. I like the additional parking on the east side of the Veteran's Hall as well.

Unsure if LID elements included. City of Roseville should be in the forefront of supporting more LID design/projects. How does this design pull together the downtown area, such as Riverside/Vernon streets - no common elements?

Like the lighting, framing of creek. Love literary quotes - teacher.

Need auto and foot access to veteran's hall that is safe!

Public Comments
Mark Thomas & Company, Inc.

The MT Co design was better and more complete than the KPFF design. Since no costs were included it could not be determined if either one was cost effective. I hope that information is available to the Selection Committee when they choose which design team is awarded the project. Locating the bike trail in the park on the south side of the creek makes much more sense. Separating pedestrian traffic from cyclists is safer and more reasonable. The cyclists can access the downtown area by walking across the bridges or using the bike trail access points on either end of the park if they chose to do that. Reorienting the Ice House Bridge may offer a better path into the park but it is more expensive and may create problems with the structure itself while moving it. New bridge footings and abutments must fit into the existing grade and how the bridge interfaces with the Veterans Hall facilities appeared to be a potential problem. Nothing appeared to have been done to resolve the parking concerns of the Veterans. Comments from meeting attendees indicated a conflict between residents who use the park and playground and the Veterans with their parking concerns. The Parks Department should be asked to find a solution to those opposing interests. The existing library parking is marginal at best and the designs did not offer any improvement. If anything, the additional vehicles who may park in the library lot to access the park will exacerbate the problem. Some additional library parking should be included in the design. The Downtown Bridge to Royer Park was the main focus of the design presentation and will be the main access for pedestrians between the park and the downtown area. How that is compatible with the vehicle traffic in Oak Street is unclear, regardless of whether a roundabout is used at Washington and Oak Street or not. It seems dangerous and ill advised to design a surface grade pedestrian crossing at the location shown in the design. It should be moved to the nearest traffic signal or changed to an elevated grade crossing. Regarding the proposed roundabout for the intersection of Washington and Oak Street, I cannot visualize how that will improve traffic flow, particularly with signals added at Oak and Taylor and Oak and Lincoln as proposed. I assume the traffic simulation models indicated it is workable but I can only see it as being worse than the current traffic flow. Lighting was mentioned as important to security. Because there are homeless people who use the park and that is not likely to change in the future, a police substation located in the park would enhance security of the area and make people feel safer in the park and more likely to use it. The riverfront development is the key to drawing people to downtown Roseville. Until that project is completed, the number of people visiting downtown is not likely to increase, regardless of the other downtown projects including the Bridges and Trail Project. Thank you for the opportunity to provide input on this project.

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**ASSEMBLY
CALIFORNIA LEGISLATURE**



BETH GAINES
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SIXTH DISTRICT

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May 19, 2014

Malcolm Dougherty, Director
State of California Department of Transportation
1120 N Street
Sacramento, CA 94273-0001

Subject: Downtown Roseville Class I Trail Project & Downtown Pedestrian Bridge Project

Dear Director Dougherty,

Thank you for the opportunity to express my support for the City of Roseville's Downtown Roseville Class I Trail & Pedestrian Improvements Projects.

I am a strong supporter of bicycling and walking, both in Placer County and throughout the State of California. The Downtown Roseville Class I Trail Project and the Downtown Pedestrian Bridge Project will both facilitate bicycle and pedestrian activity to and through Downtown Roseville. Roseville's Downtown Specific Plan identifies making Downtown Roseville pedestrian and bicycle friendly as one of the key components to attract new residents and businesses to Downtown Roseville.

The projects will result in fewer automobile trips and reduced air pollutant emissions. In addition, the bikeway component ties into a regional bikeway system that includes connections into neighboring communities. These projects promise to be a highlight of the City of Roseville's many improvements planned for Downtown Roseville.

Please join me in my support for both the Downtown Roseville Class I Trail Project and the Downtown Pedestrian Bridge Project and the corresponding ATP grant applications.

Sincerely,

A handwritten signature in blue ink, appearing to read "Beth Gaines", with a stylized flourish at the end.

BETH GAINES
Assemblywoman, Sixth District



ROSEVILLE

CHAMBER of
COMMERCE

SERVING ROSEVILLE
AND GRANITE BAY

650 DOUGLAS BLVD.

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May 13, 2014

Malcolm Dougherty, Director
State of California Department of Transportation
1120 N Street
Sacramento, CA 94273-0001

Subject: Downtown Roseville Class I Trail Project & Downtown Pedestrian Bridge Project

Dear Director Dougherty,

Thank you for the opportunity to express the Roseville Chamber of Commerce's support for the City of Roseville's Active Transportation Program grant applications for the Downtown Roseville Class I Trail & Pedestrian Improvements Projects.

The Chamber of Commerce is a strong supporter of the City of Roseville's efforts to revitalize Downtown Roseville. We spent many hours helping the City develop the Downtown Specific Plan, which includes the proposed projects. Also, we have backed up our support for Downtown Roseville by locating our offices Downtown near the proposed improvements.

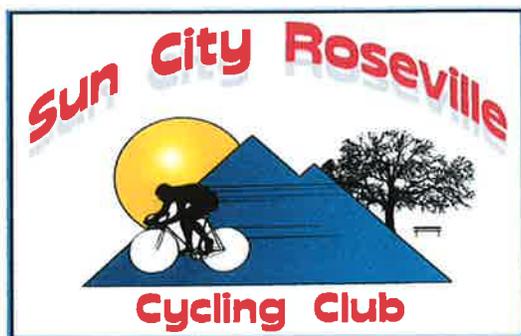
We are excited about the Active Transportation opportunities that these projects would provide for our employees and for the community as a whole. The proposed projects will add to the number of residents walking and biking in Downtown Roseville, and this means more business for Downtown merchants and more economic vitality for the City overall.

Once again, thank you for the opportunity to submit this letter in support of the grant applications for the Downtown Roseville Class I Trail Project and the Downtown Pedestrian Bridge Project.

Sincerely,

Wendy A. Gerig
CEO





May 14, 2014

Malcolm Dougherty, Director
State of California, Department of Transportation
1120 North Street
Sacramento, CA 94273-0001

Subject: Downtown Roseville Class I Trail Project

Dear Director Dougherty,

The Sun City Roseville Cycling Club is sending this letter to extend its support of the Downtown Roseville Class I Trail Project Active Transportation Project grant application. Sun City Cyclists often use the existing Miners Ravine Trail on our rides, to which this project will connect. The proposed project will give our club another opportunity for rides in the local area. Our club holds daily bike rides for our members. We look forward to your support of the Downtown Roseville Class I Trail Project grant application.

Sincerely,

**President
Sun City Cycling Club**

May 15, 2014

Malcolm Dougherty, Director
State of California Department of Transportation
1120 N Street
Sacramento, CA 94273-0001



Subject: Downtown Roseville Class I Trail Project & Downtown Pedestrian Bridge Project

Dear Director Dougherty,

Thank you for the opportunity to express our support for the City of Roseville's Downtown Roseville Class I Trail & Pedestrian Improvements Projects.

The Downtown Roseville Merchants, Inc. are key stakeholders in the City of Roseville's efforts to revitalize Downtown Roseville. We spent countless hours helping the City develop the Downtown Specific Plan, and we are excited to see the City moving forward on the projects.

Many residents already walk and bike to Downtown Roseville. The City has made great efforts to support this by improving the pedestrian friendliness of the street frontage and adding bike racks. We believe that the proposed projects will only add to the number of residents walking and biking in Downtown Roseville. This means more business for our merchants and more economic vitality for the City overall.

I also serve as a Parks & Recreations Commissioner for the City of Roseville, and I know this project will make a significant impact on accessibility between our downtown and our beautiful Royer Park, a key property in our downtown area. As a commissioner, I'd love to see this bridge become that "bridge" between our Vernon Street Town Square (park) and Royer Park.

Please join the merchants and myself in support for both the Downtown Roseville Class I Trail Project and the Downtown Pedestrian Bridge Project and the corresponding ATP grant applications.

Sincerely,

A handwritten signature in black ink that reads "Scott Alvord". The signature is written in a cursive, flowing style.

Scott Alvord
President, Downtown Roseville Merchants, Inc.
President@DowntownRoseville.com

Megan Johnson

From: Wallace, Melanie@CCC <Melanie.Wallace@ccc.ca.gov> on behalf of Clark, Virginia@CCC <Virginia.Clark@CCC.CA.GOV>
Sent: Friday, May 16, 2014 4:42 PM
To: Megan Johnson
Cc: Thornhill, Rod@CCC; 'calocalcorps@gmail.com'
Subject: FW: ATP Grant Application - CCC Coordination - City of Roseville projects

Megan,

Thee CCC may be able to participate in this ATP project. Please include this email in your application.

Thank you,

Virginia Clark
Region Deputy, Region 1



California Conservation Corps
(916) 341-3147
fx(877) 834-4177
virginia.clark@ccc.ca.gov



PLEASE CONSIDER THE ENVIRONMENT BEFORE PRINTING THIS EMAIL

Visit our web site at www.ccc.ca.gov for more information about the California Conservation Corps
Visit our web site at www.WatershedStewards.com for more information about the Watershed Stewards Program

From: Thornhill, Rod@CCC
Sent: Friday, May 16, 2014 3:44 PM
To: Clark, Virginia@CCC
Subject: RE: ATP Grant Application - CCC Coordination - City of Roseville projects

There are components of this application that fits with CCC capabilities.

From: Clark, Virginia@CCC
Sent: Friday, May 16, 2014 12:25 PM
To: Thornhill, Rod@CCC
Subject: FW: ATP Grant Application - CCC Coordination - City of Roseville projects

Please review ATP project

Virginia Clark
Region Deputy, Region 1



California Conservation Corps

(916) 341-3147

fx(877) 834-4177

virginia.clark@ccc.ca.gov



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Visit our web site at www.ccc.ca.gov for more information about the California Conservation Corps

Visit our web site at www.WatershedStewards.com for more information about the Watershed Stewards Program

From: Megan Johnson [<mailto:mjohnson@markthomas.com>]

Sent: Friday, May 16, 2014 10:19 AM

To: Clark, Virginia@CCC

Subject: ATP Grant Application - CCC Coordination - City of Roseville projects

Hi Virginia,

Melanie asked me to send you PDF copies of the grant application information for our projects. These are for two separate projects for the City of Roseville – the first is a bike trail and bridge project, and the second is a pedestrian bridge project. Please let me know if you need any other information.

Thanks,

Megan Johnson

Project Engineer

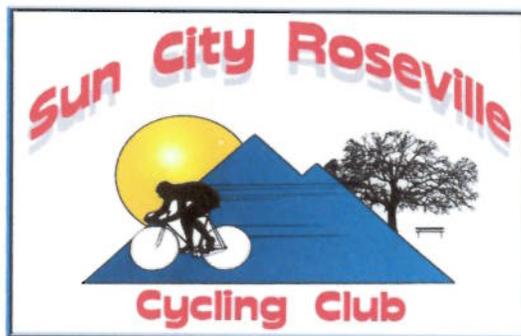
(916) 381-9100

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DLA



May 14, 2014

Malcolm Dougherty, Director
State of California, Department of Transportation
1120 North Street
Sacramento, CA 94273-0001

Subject: Downtown Roseville Class I Trail Project

Dear Director Dougherty,

The Sun City Roseville Cycling Club is sending this letter to extend its support of the Downtown Roseville Class I Trail Project Active Transportation Project grant application. Sun City Cyclists often use the existing Miners Ravine Trail on our rides, to which this project will connect. The proposed project will give our club another opportunity for rides in the local area. Our club holds daily bike rides for our members. We look forward to your support of the Downtown Roseville Class I Trail Project grant application.

Sincerely,


President
Sun City Cycling Club

RECEIVED

MAY 19 2014

DIRECTOR'S OFFICE

DEPARTMENT OF TRANSPORTATION

OFFICE OF THE DIRECTOR
P.O. BOX 942873, MS-49
SACRAMENTO, CA 94273-0001
PHONE (916) 654-5266
FAX (916) 654-6608
TTY 711
www.dot.ca.gov



*Serious drought.
Help save water!*

June 3, 2014

The Honorable Jim Nielsen
California State Senate
State Capitol, Room 4062
Sacramento, CA 95814

Dear Senator Nielsen:

Thank you for your letter supporting the request for Active Transportation Program funding for the Downtown Roseville class I Trail Project and Downtown Pedestrian Bridge Project. Your support of this project has been submitted to the Program's staff.

The Active Transportation Program's core purpose is to encourage increased use and safety of active modes of transportation. By augmenting regional agencies efforts with this funding, Caltrans hopes to see improvements not only in greenhouse gas reductions throughout the State but also enhance public health outcomes.

The first call for projects is scheduled to end on May 21, 2014. All applications will be scored based on previously established criteria and then evaluated by the California Transportation Commission's Project Evaluation Committee. We anticipate the final selection of projects in late August.

Thank you again for your interest in the Active Transportation Program.

Sincerely,

A handwritten signature in black ink, appearing to read "Malcolm Dougherty".

MALCOLM DOUGHERTY
Director