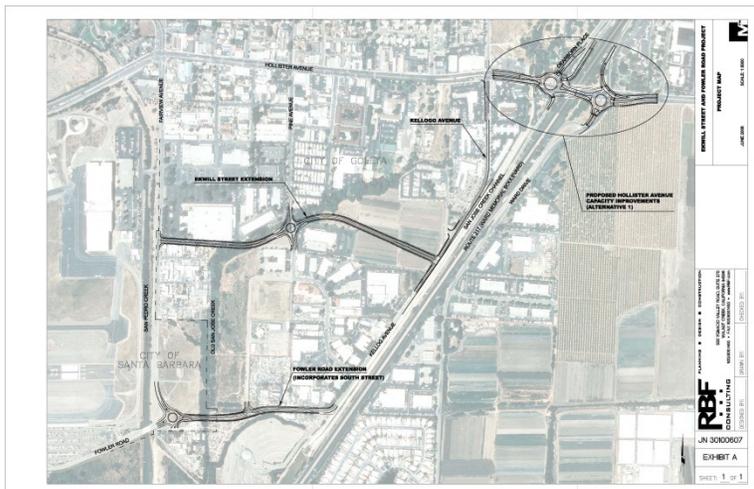


CITY OF GOLETA

EKWILL STREET & FOWLER ROAD EXTENSIONS PROJECT – BICYCLE/PEDESTRIAN IMPROVEMENTS



ACTIVE TRANSPORTATION PROGRAM CYCLE 1 PROJECT PRIORITY: 2 OF 2

Ekwill-Fowler Bicycle and Pedestrian Improvement Project

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Figure 2 –Location Map

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ATTACHMENT D – Project Detailed Cost Estimates

AATTACHMENT E – Ekwil-Fowler Project Schedule

ATTACHMENT F – SBCAG 2040 RTP/SCS – Abbreviated

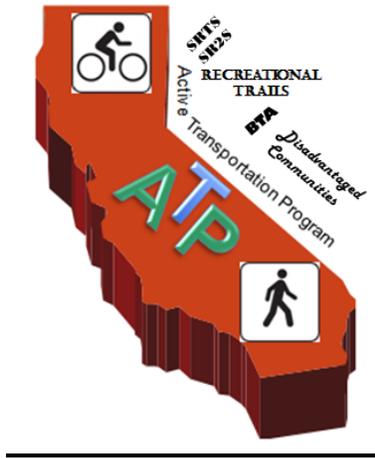
ATTACHMENT G – Kittleson Technical Memo

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ACTIVE TRANSPORTATION PROGRAM CYCLE 1

APPLICATION Part 1 (Includes Sections I, V, VI, VII, VIII & XI)

Please read the Application Instructions at
<http://www.dot.ca.gov/hq/LocalPrograms/atp/index.html>
prior to filling out this application

Project name:

For Caltrans use only: ___TAP ___STP ___RTP ___SRTS ___SRTS-NI ___SHA
___DAC ___Non-DAC ___Plan

I. GENERAL INFORMATION

Project name:

(fill out all of the fields below)

1. APPLICANT (Agency name, address and zip code)	2. PROJECT FUNDING ATP funds Requested \$ _____ Matching Funds \$ _____ (If Applicable) Other Project funds \$ _____ TOTAL PROJECT COST \$ _____
3. APPLICANT CONTACT (Name, title, e-mail, phone #)	5. PROJECT COUNTY(IES):
4. APPLICANT CONTACT (Address & zip code)	7. Application # _____ of _____ (in order of agency priority)
6. CALTRANS DISTRICT #- Click Drop down menu below	

Area Description:

8. Large Metropolitan Planning Organization (MPO)- Select your "MPO" or "Other" from the drop down menu>	
9. If "Other" was selected for #8- select your MPO or RTPA from the drop down menu>	
10. Urbanized Area (UZA) population (pop.)- Select your UZA pop. from drop down menu>	

Master Agreements (MAs):

11. Yes, the applicant has a FEDERAL MA with Caltrans.
12. Yes, the applicant has a STATE MA with Caltrans.
13. If the applicant does not have an MA. Do you meet the Master Agreement requirements? Yes No
The Applicant MUST be able to enter into MAs with Caltrans

Partner Information:

14. Partner Name*:	15. Partner Type
16. Contact Information (Name, phone # & e-mail)	17. Contact Address & zip code

Click here if the project has more than one partner; attach the remaining partner information on a separate page

*If another entity agrees to assume responsibility for the ongoing operations and maintenance of the facility, documentation of the agreement must be submitted with the application, and a copy of the Memorandum of Understanding or Interagency Agreement between the parties must be submitted with the request for allocation.

Project Type: (Select only one)

18. Infrastructure (IF) 19. Non-Infrastructure (NI) 20. Combined (IF & NI)

Project name:

I. GENERAL INFORMATION-continued

Sub-Project Type (Select all that apply)

21. Develop a Plan in a Disadvantaged Community (select the type(s) of plan(s) to be developed)
 Bicycle Plan Safe Routes to School Plan Pedestrian Plan
 Active Transportation Plan

(If applying for an Active Transportation Plan- check any of the following plans that your agency already has):

- Bike plan Pedestrian plan Safe Routes to School plan ATP plan

22. Bicycle and/or Pedestrian infrastructure
Bicycle only: Class I Class II Class III
Ped/Other: Sidewalk Crossing Improvement Multi-use facility

Other:

23. Non-Infrastructure (Non SRTS)
 24. Recreational Trails*- Trail Acquisition

***Please see additional Recreational Trails instructions before proceeding**

25. Safe routes to school- Infrastructure Non-Infrastructure

If SRTS is selected, provide the following information

26. SCHOOL NAME & ADDRESS:
27. SCHOOL DISTRICT NAME & ADDRESS:

28. County-District-School Code (CDS)	29. Total Student Enrollment	30. Percentage of students eligible for free or reduced meal programs **
31. Percentage of students that currently walk or bike to school	32. Approximate # of students living along school route proposed for improvement	33. Project distance from primary or middle school

**Refer to the California Department of Education website: <http://www.cde.ca.gov/ds/sh/cw/filesafdc.asp>

- Click here if the project involves more than one school; attach the remaining school information including school official signature and person to contact, if different, on a separate page

II. PROJECT INFORMATION

1. Project Location

The City of Goleta (City) in the County of Santa Barbara, California (See Figures 1 and 2, Attachment A) is one of the northern most communities in Southern California. The City is situated in the broader Goleta Valley region which is bordered to the north by the Santa Ynez Mountains, to the south by the Pacific Ocean, to the east by the City of Santa Barbara and to the west by the Gaviota coastline. Immediately adjacent to the University of California at Santa Barbara (UCSB), the City has significant student, faculty and staff populations in addition to being home to many working class families.

The City of Goleta is bisected by United States Highway 101 (US 101) and the Union Pacific Railroad (UPRR). Other major east–west roadways are Cathedral Oaks Road to the north of US 101 and Hollister Avenue to the south of US 101. Hollister Avenue is the main east-west corridor through Goleta’s commercial core and Old Town Goleta. Major north-south roadways include Fairview and Patterson Avenues and Storke, Los Carneros, and Cathedral Oaks Roads which provide access to the US 101. State Route 217 (SR 217), which runs northeast-southeast between the US 101 and UCSB, provides another connection to the City (See Figures 2 and 3, Attachment A).

The Ekwill-Fowler Bicycle and Pedestrian Improvement Project (Project) is in close proximity to several prominent natural and community features, including the Pacific Ocean, Goleta Beach Park, the Goleta Slough, the Ellwood-Devereux Open Space, Ellwood Main Monarch Butterfly Grove, UCSB, the Santa Barbara Municipal Airport, and Goleta’s Old Town historic district.

The Project is bounded by Hollister Avenue to the north, SR 217 to the east, Fairview Avenue to the west, and the Goleta Slough to the south and southwest (See Figure 4, Attachment A). This area can be described generally as mixed-use as it includes commercial, industrial, residential, and agricultural uses. Much of the project area is within the coastal zone.

2. Project Coordinates

Latitude
(Decimal degrees)

Longitude
(Decimal degrees)

3. Project Description

The ATP grant Project is a part of the larger Ekwil Street and Fowler Road Extension Project that includes public infrastructure improvements that are identified in the Goleta Old Town Revitalization Plan (Revitalization Plan) and incorporated into the Goleta General Plan/Coastal Land Use Plan (General Plan).

The larger Ekwil Street and Fowler Road Extension Project (Ekwil-Fowler Project) components include improvements to the existing interchange at Hollister Avenue and State Route 217 (between PM 2.1 and 2.4), the extension of Ekwil Street from Fairview Road to Kellogg Avenue, and the extension of Fowler Road between Fairview Road and School Bus Lane.

The Project includes constructing Class II bike lanes and sidewalks along the new roadway extensions, new and improved intersection crossings at existing intersections, roundabouts that include improved bicycle and pedestrian configurations, and a multi-use facility adjacent to the Old San Jose Creek. The roundabouts will slow traffic moving through the intersections, reducing the frequency and severity of accidents between vehicles and bicyclists and pedestrians. These improvements will also help traffic flow in the area and will provide some relief to congestion on Hollister Avenue. It will provide new access to the Santa Barbara Airport and will provide new sidewalks, bikeways and improved transit access between Goleta Old Town, the Santa Barbara Airport, Goleta Beach and the UCSB campus. The new roadways will also include modern infrastructure upgrades such as drainage systems that will improve water quality that is currently impacted by surface runoff from industrial land uses close to the coastline.

1. **Ekwil Street (Photos 1 through 5, Attachment B)**

The Ekwil Street extension will extend approximately 1,350 feet from Pine Avenue to Fairview Avenue with a right-of-way from 60 to 72 feet in width, with no parking on either side of the new road.

Specific features of the Ekwill Street extension (including ATP Project) include the following:

- A four-leg, single lane roundabout and appropriate traffic signage at the intersection of Ekwill Street and Pine Avenue. The roundabout will accommodate pedestrian crossings on each leg of the roundabout and a raised, center median with landscaping.
- Two 12-foot-wide lanes, one eastbound and one westbound, with standard traffic signs and a right turn lane from westbound Ekwill Street to northbound Fairview Avenue.
- Eight foot wide Class II bicycle lanes on both sides of the road between Fairview and Pine Avenues.
- A continuous five to six foot wide sidewalk along the north side of Ekwill Street between Fairview Avenue and Pine Avenue.
- A five foot wide sidewalk along the south side of Ekwill Street beginning at Pine Avenue extending approximately 450 feet west providing a connection to a future multi-use path along Old San Jose Creek.
- A multi-use path along the north side of Ekwill Street beginning just east of Pine Avenue extending approximately 1,000 feet east to complete a portion of the Old San Jose Creek Trail.
- Where sidewalks are built, five foot wide planted parkways will be located between the curb and the sidewalks. To minimize the impact to coastal riparian resources, the planted parkway has been eliminated on the north side of Ekwill Street between Old San Jose Creek and Pine Avenue and a retaining wall up to six feet high is proposed along the back of sidewalk to contain the fill slopes and minimize the disturbance.

2. Fowler Road Extension (Photos 6 through 14, Attachment B)

This portion of the Project will rebuild the existing South Street and add an extension of the road from its current terminus to the west and into a roundabout intersection with Fairview Avenue, for a distance of approximately 1,250 feet.

Specific features of the Fowler Road extension (including ATP Project) include the following:

- A four-leg single lane roundabout and appropriate traffic signage at the intersection of Fowler Road, Fairview Avenue and, to the south, Placencia Street. The roundabout will accommodate pedestrian crossings on each leg of the roundabout and a raised, center median with landscaping.

- Two 12-foot-wide travel lanes with standard traffic signs.
- Eight foot wide Class II bicycle lanes on both sides of the new road and a four to five foot bike lane along both sides of the existing South Street.
- Five to six foot wide sidewalks on both sides of the new road and along the existing South Street.
- A planted, five foot wide parkway on both sides of the new road located between the curb and sidewalk between Fairview Avenue and Old San Jose Creek. The planted parkway will not be installed over Old San Jose Creek or along the reconstructed section of South Street.

3. Hollister Avenue/SR 217 Interchange Improvements (Photos 15 through 21, Attachment B)

This portion of the Project will modify the ramp intersections at the Hollister Avenue at SR 217 interchange and reconfigure the existing signalized intersections of the westbound off-ramp/on-ramp and eastbound on-ramp/Ward Drive at Hollister Avenue as roundabouts. Hollister Avenue will be realigned slightly between Kellogg Avenue to just west of Ward Drive as part of the construction of the roundabouts, over a distance of approximately 1,400 feet. If roundabouts were not being installed at these locations, Hollister Avenue would need to be widened to six lanes, thereby making it even more difficult for bicyclists and pedestrians to navigate.

Specific features of the Hollister Avenue/SR 217 interchange modifications (larger Ekwill-Fowler Project roadway project and ATP grant Project) include the following:

- A five-leg, two lane roundabout and appropriate traffic signage at the intersection of Hollister Avenue, the westbound off-ramp from SR 217, the westbound on-ramp to SR 217, and Dearborn Place. The roundabout will accommodate bicyclists and pedestrian crossings on each leg of the roundabout and a raised, center median with landscaping.
- A four-leg, two lane roundabout and appropriate traffic signage at the intersection of Hollister Avenue, the eastbound on-ramp to SR 217, and Ward Drive. The roundabout will accommodate bicyclists and pedestrian crossings on each leg of the roundabout and a raised, center median with landscaping.
- Four 12-foot-wide travel lanes with standard traffic signs.

- Eight foot wide Class II bicycle lanes on both sides of Hollister Avenue. Bicyclists will be taken off of the on-street bike lanes onto a ten foot wide multi-use path at the roundabouts. Stronger cyclists will enter the roundabouts as vehicles.
- Five to six foot wide sidewalks on both sides of Hollister Avenue.
- A planted, variable width parkway between the curb and multi-use path around the perimeter of the roundabouts

A copy of the preliminary plans included in Attachment C. A detailed Project and Ekwill-Fowler Project cost estimate is included in Attachment D. A Project schedule is included in Attachment E.

4. Project Status

The City, as Lead Agency under California Environmental Quality Act (CEQA) approved the Environmental Impact Report (EIR) on November 28, 2011. Caltrans, as the lead agency under the National Environmental Policy Act (NEPA), signed off on Categorical Exemption on December 15, 2011. The design of the Hollister Avenue/SR 217, the Ekwill Street extension and the Fowler Road extension is approximately 90% complete. The California Coastal Commission is reviewing the Coastal Development Permit application. The City is in the process of acquiring the necessary right of way and securing the final right-of-way clearance.

Construction is anticipated to begin in late summer of 2015 and be completed by fall of 2017.

III. SCREENING CRITERIA

1. Demonstrated Needs of the Applicant

The City in the area of Old Town does not have east west linkage roads between Kellogg Avenue and Fairview Avenue. As a result, access to and within Old Town as well as to the Airport, UCSB and the beaches is very constrained and cumbersome for vehicle drivers, pedestrians and bicyclists alike. Currently, Old Town Goleta south of Hollister Avenue is not pedestrian- or bicyclist- friendly. There are no Class II bike facilities within the project vicinity, and many existing streets have no sidewalks. To address these deficiencies, the City's General Plan Transportation Element includes policies to increase

pedestrian and bicyclist access. These policies include planned sidewalks and Class II bikeways along extensions of Ekwil Street and Fowler Road.

One of the major public infrastructure improvements identified in the City's General Plan is the construction of two new roads, Ekwil Street and Fowler Road, to provide new east-west routes linking Fairview Avenue to Kellogg Avenue. These new roadways, sidewalks and bike lanes would improve traffic flow in the currently deficient local grid roadway system, and provide alternate internal circulation routes for vehicles, pedestrians and bicyclists alike as well as provide better public transit routes linking Goleta Old Town to other areas.

Purpose and Objectives:

The purpose of the Ekwil-Fowler Project (including the ATP grant Project) is to improve connectivity and access to, from, and within southern Old Town as well as to the Santa Barbara Airport, UCSB and coastal recreational destinations and to reduce congestion along Hollister Avenue. Specific objectives of the Project include:

- To improve vehicular, bicycle and pedestrian access to Old Town
- To improve connectivity within southern Old Town
- To reduce congestion on Hollister Avenue thereby improving bicyclist safety
- To improve access to Santa Barbara Airport
- To improve pedestrian and bicycle circulation and safety
- Expanded public transit routes within the southern section of Old Town, to UCSB, to Goleta Beach County Park and to the Airport

Figure 5, Attachment A shows schematically the new public bike lanes, sidewalks, and multi-use access paths that will be built as part of the Ekwil-Fowler Project along with the regional bicycle and pedestrian facilities existing near the Project (Figures 6 and 7) and how the Ekwil-Fowler Project and inclusive ATP grant Project improvements will enhance regional connectivity for these alternative transportation methods.

In 1993, the Goleta Community Plan (Community Plan) was adopted by the Santa Barbara County Board of Supervisors. The City adopted the Community Plan upon incorporation in 2002. Subsequent to the County's adoption of that program, a Goleta Old Town Revitalization Plan was adopted by Santa Barbara County in 1998 which was inherited by the City of Goleta in 2002 upon the City's incorporation. The Revitalization Plan identified a set of public infrastructure improvements that have been incorporated into the City's General Plan to help revitalize the Goleta Old Town Old Town area. Implementation procedures for various bicycle and vehicular circulation improvements are specified in the Goleta Transportation Improvement Program (GTIP).

2. Consistency with Regional Transportation Plan

The Ekwill-Flower Project is included in the 2040 Regional Transportation Plan/Sustainable Communities Strategy (2040 RTP/SCS), adopted by the Santa Barbara County Association of Governments (SBCAG), the Metropolitan Planning Organization (MPO), on August 15, 2013. The Ekwill Street and Fowler Road Extension Project is listed as a Programmed Project in the City of Goleta (ID# Go-103) (Attachment F – SBCAG 2040 RTP/SCS – Abbreviated Appendix E for Goleta Projects). The project is also programmed in the 2014 State Transportation Improvement Program (STIP) and the 2013 Federal Transportation Improvement Program.

IV. NARRATIVE QUESTIONS

1. POTENTIAL FOR INCREASED WALKING AND BICYCLING, ESPECIALLY AMONG STUDENTS, INCLUDING THE IDENTIFICATION OF WALKING AND BICYCLING ROUTES TO AND FROM SCHOOLS, TRANSIT FACILITIES, COMMUNITY CENTERS, EMPLOYMENT CENTERS, AND OTHER DESTINATIONS; AND INCLUDING INCREASING AND IMPROVING CONNECTIVITY AND MOBILITY OF NON-MOTORIZED USERS. (0-30 POINTS)

A. Describe how your project encourages increased walking and bicycling, especially among students.

The Ekwil-Fowler Project is located in arguably the most active part of the City of Goleta. The project is only a short distance to UCSB, less than a mile from the Pacific Ocean, and Old Town Goleta (located on Hollister Avenue between SR 217 and Fairview) (Figures 3 and 4). These existing amenities in addition to the plethora of planned and existing commercial businesses all grouped into a relatively small area between SR 217, Hollister, and Fairview, create an ideal setting for walking and biking atmosphere, especially with the mild climate in Santa Barbara County. The project encourages walking by providing new Class II bicycle facilities and sidewalks that connect to the City's alternatives modes network.

It is anticipated that in addition to recreational users accessing the Goleta State beach just south of the project, the new facilities will also be used by staff and students from UCSB as well as commuters who work in some of the existing and planned businesses in the area. The planned Fairview Corporation Center, Gilkeson Industrial project, Thornwood Industrial project, Page Live/Work project and the Fairview Mixed Use project are just a few of the exciting new development activities that are happening within a half mile of the project which are expected to generate foot and bicycle traffic in the area.

B. Describe the number and type of possible users and their destinations, and the anticipated percentage increase in users upon completion of your project. Data collection methods should be described. \

To estimate the increase in bicycle and pedestrian volumes associated with the Project the City recruited Kittelson and Associates, Inc. to look at the increased demand which would result from the new facility (See Attachment G).

There are many light industrial, office buildings, and heavy commercial facilities located in the area. Additionally, restaurants, offices and business are located in the Old Town Goleta historic corridor of Hollister Avenue.

Bicycle Demand

After construction of the Class I bicycle path, expected bicycle demand was calculated by adapting the Benefit-Cost Analysis Tool (developed as part of *NCHRP Report 522: Guidelines for Analysis of Investments in Bicycle Facilities*) to the local conditions of the City of Goleta. This adapted methodology for estimating the use of a new facility is based on two assumptions:

All existing commuter bicyclists, defined as the bicyclist estimates available from intersection counts along the corridor (see Bicycle and Pedestrian Volume above), will use the new facility; and,

The new facility will induce new bicyclists based on the local bicycling commute rate (based on the 2012 ACS 5-year estimates), children’s bicycling rate (5%, based on the 2011 NHTS), and nearby residential population. The method uses Census commute shares to extrapolate high, moderate, and low estimates of new bicyclists, the results of this analysis is shown in the following table:

Ekwill-Fowler Project Bicycle Estimates

	Adult Bicycling Rate	Existing Ekwill Bicyclists	Existing Fowler Bicyclists	New Ekwill Bicyclists	New Fowler Bicyclists
High	16.91%	188	171	71	60
Low	5.44%	60	55	38	32

For the complete explanation of the methodology used to calculate the estimated new bicyclists see the full traffic memorandum in Attachment G.

- C. Describe how this project improves walking and bicycling routes to and from, connects to, or is part of a school or school facility, transit facility, community center, employment center, state or national trail system, points of interest, and/or park.

The City has existing and planning Class I and Class II bicycle facilities which link up every corner of the City, unincorporated County, UCSB, and City of Santa Barbara Municipal Airport for

residents or visitors who wish to explore the City on foot or on bike. The bike and pedestrian facilities will eventually connect to the County's Goleta Beach Park and the County/UCSB Class I bike path between UCSB and the City of Santa Barbara 12 miles east. The facilities will also provide increased access to the businesses, schools, recreation and employment opportunities in the historic Old Town Goleta corridor. Residents north of Hollister Avenue will have improved access to the local Sunday Santa Barbara Swap Meet located at 907 S Kellogg Avenue (Figure 7). Residents are frequently seen walking and biking on the local roads because there are no sidewalks or bike lanes. The Project, in conjunction with the larger Ekwil-Fowler Project, will provide improved means to access the swap meet location via bike lanes and sidewalks. Parents and students commuting to the local Rainbow School located at the Goleta Valley Community Center will benefit from the Project.

Additionally, the new facilities will connect, via other Class I and II bikeways in the City, to the existing California Coastal Trail and the planned future Juan Bautista de Anza national trail system, which would ultimately provide contiguous bicycle access from the U.S. border with Canada to the U.S. border with Mexico.

- D. Describe how this project increases and/or improves connectivity, removes a barrier to mobility and/or closes a gap in a non-motorized facility.

The Project will increase and improve connectivity and remove barriers because two new road extensions will be constructed thereby connecting two north-south routes in an area of the City that has limited connectivity. The portion of the Ekwil-Fowler Project that is bicycle and pedestrian, the Project, will greatly increase the connectivity and improve safety for pedestrian and bicyclists. Gaps in the roadway grid will be removed. Additionally, the Santa Barbara Airbus transit terminal is located in middle of the two road extension projects. Local MTD bus connections to the Santa Barbara Airbus, which provides services to the Los Angeles Airport, will be improved. Access for residents and visitors from the entire South Coast to the Airbus terminal will be improved as a result of the roadway extensions.

2. POTENTIAL FOR REDUCING THE NUMBER AND/OR RATE OF PEDESTRIAN AND BICYCLIST FATALITIES AND INJURIES, INCLUDING THE IDENTIFICATION OF SAFETY HAZARDS FOR PEDESTRIANS AND BICYCLISTS. (0-25 POINTS)

A. Describe the potential of the project to reduce pedestrian and/or bicycle injuries or fatalities.

This section of Hollister Avenue is the main east-west arterial in the vicinity for both alternatives modes users as well as automotive users. Old Town, south of Hollister Avenue, does not have east west linkage roads between Kellogg Avenue and Fairview Avenue. North of Hollister Avenue, the local grids is discontinuous as well and bordered by the UPRR and US 101. The addition of two new east-west roadways between SR 217 and Fairview will provide parallel capacity for alternatives modes traffic on sidewalks and Class II bike facilities. This new capacity combined with the planned Old San Jose Creek trail project will enable alternative modes users to more easily travel between destinations west of Fairview Avenue and east of SR 217.

B. Describe if/how your project will achieve any or all of the following:

One section that is expected to achieve the most significant enhancement for bicycle and pedestrian safety is at the interchange of SR 17 with Hollister Avenue. Mainline interchanges with urban arterials are common pinch-points in the local bicycle and pedestrian grid that often pose safety risks for alternatives modes traffic. At this location the mainline on and off ramp intersections with Hollister Avenue will be reconfigured as roundabouts on both the east and west sides of the freeway. Roundabouts have been used effectively throughout California in this configuration near interchanges where they have provided an array of benefits to alternatives modes users. Among the advantageous elements of roundabouts are the following features:

- ✓ Promote safe/efficient traffic flow (Reduces speed or volume of motor vehicles)
- ✓ Traffic moves in only one direction (Improves sight distance and visibility)
- ✓ Channelized approaches limit erratic/unexpected movements by motorists (Improves sight distance and visibility and addresses inadequate traffic control devices)
- ✓ Slower intersection entry speed (Reduces speed or volume of motor vehicles)
- ✓ Lower number of conflict points (Improves compliance with local traffic laws)

- ✓ Bifurcated sidewalks keep pedestrians well away from automobiles (Eliminates behaviors that lead to collisions and addresses inadequate bicycle facilities, crosswalks or sidewalks)
- C. Describe the location's history of events and the source(s) of data used (e.g. collision reports, community observation, surveys, audits) if data is not available include a description of safety hazard(s) and photos.

Within the last 10 years the stretch of Hollister Avenue between SR 217 and Fairview has had over two dozen traffic collisions involving an automobile and either a pedestrian or a bicycle (See Figure 8, Attachment A and Accident Data, Attachment H). The majority of the vehicle verses pedestrian accidents in this area are related to pedestrian right-of-way violations. This can mean improper intersection crossings or walking in the roadway because of lack of sidewalks. Many of the vehicle verses bicycle accidents are due to turning conflicts by either the vehicle or the bicycle. The Project will create new roadway connections that include Class II bike lanes, sidewalks, and improved crosswalks (especially at the Hollister Avenue and SR 217 ramp intersections) that will improve access, safety, and provide alternate routes for bicyclists and pedestrians.

3. PUBLIC PARTICIPATION and PLANNING (0-15 POINTS)

- A. Describe the community based public participation process that culminated in the project proposal or plan, such as noticed meetings/public hearings, consultation with stakeholders, etc.

The Ekwill Street and Fowler Road Project have had enormous public and agency participation dating back to the late 1980's. The proposed project(s) is referenced in numerous planning documents dating back to 1993. Relevant planning documents include (Attachment I):

1. July 1993 - Goleta Community Plan (GCP). Adopted by the County of Santa Barbara.
2. June 1998 - Goleta Old Town Revitalization Plan and EIR. Adopted by the County of Santa Barbara.
3. September 1997 - Project Study Reports (PSR) for the Ekwill Street Extension and Fowler Road Extension Projects. Approved by the County of Santa Barbara and Caltrans.
4. State Transportation Improvement Program (STIP). Approved by the California Transportation Commission (CTC).
5. Federal Transportation Improvement Program (FTIP). Approved by the Santa Barbara County Association of Governments and the Federal Highway Administration (FHWA).
6. Goleta General Plan/Coastal Land Use Plan. Adopted by the City of Goleta.

7. Santa Barbara Airport Layout Plan.
8. November 2011 – Final Environmental Impact Report. Approved by City of Goleta.

B. Describe the local participation process that resulted in the identification and prioritization of the project:

The following is a chronological list of agency approvals, agency meetings/hearing and community meetings that occurred for the Ekwill Street and Fowler Road Extensions Project:

1. Goleta Community Plan (GCP). Adopted by the County of Santa Barbara.
It is estimated that preparation of the Goleta Community Plan in July 1993 involved over twenty community meetings.
2. Goleta Old Town Revitalization Plan and EIR. Adopted by the County of Santa Barbara.
It is estimated that the five year effort leading up to the approval involved over twenty community meetings.
3. August 2001 - Value Analysis Study (VAS) of the Ekwill Street and Fowler Road Extension Alternatives with participation from Caltrans, the County of Santa Barbara, The University of California at Santa Barbara, The Santa Barbara Airport, The Santa Barbara Association of Governments and the Goleta Old Town Planning Area Committee.
4. February 2002 - Incorporation of the City of Goleta - Public Workshops.
During the months of July, August and September 2002, the Project Team held three workshops meeting with Goleta Old Town community.
5. December 2002 – Initial Notice of Preparation Hearing under CEQA.
6. April 2004 - Santa Barbara County Association of Governments (SBCAG) approves the revised project scope.
7. June 2004 – Revised Notice of Preparation Hearing under CEQA.
8. October 2004 – The California Transportation Commission (CTC) approved a STIP Amendment defining the revised project scope, funding and the combining of Ekwill Street and Fowler Road into a single project.
9. September 2008 – Updated Notice of Preparation under CEQA.
10. June 2010 – Conceptual Approval of Project by Design Review Board (DRB).
11. September 2011 – Public Hearing for the Draft EIR for Project.

The following is a list of all public agencies involved in with the project and a description of their involvement:

- The City of Goleta – project sponsor, funding agency, Lead Agency under CEQA.
- Caltrans – lead Agency under NEPA, proving technical oversight for Project and supports Project improvements to the Hollister Avenue/State Route 217 Interchange.
- The County of Santa Barbara – original project sponsor (prior to formation of City of Goleta), author of Goleta Community Plan, and author of Goleta Old Town Revitalization Plan
- City of Santa Barbara Airport – responsible Agency under CEQA (a portion of Project lies with Airport jurisdiction), Coastal Development Permit, and supports Projects because of resulting improved access to Airport.
- SBCAG (MPO) – approved inclusion of Project in 1998 Regional Transportation Improvement Plan for funding through STIP.
- UCSB - involved in identification of Project Alternatives acceptable to the University, supporter of revised project scope that was approved in the Final EIR, and supporter of Project because of resulting enhanced access to UCSB campus.
- California Transportation Commission (CTC) – the CTC approved funding for the Project through the State Transportation Improvement Plan (STIP).

C. Is the project cost over \$1 Million? Y/N Y

If Yes- is the project Prioritized in an adopted city or county bicycle transportation plan, pedestrian plan, safe routes to school plan, active transportation plan, trail plan, circulation element of a general plan, or other publicly approved plan that incorporated elements of an active transportation plan? Y/N Y

Yes, the Ekwil-Fowler Project is listed and prioritized in the City's General Plan, Transportation Element. The Ekwil-Fowler Project was also presented to City Council in the City's Capital Improvement Program (CIP) in 2002 and has been incorporated into the CIP annual presentations.

IV. NARRATIVE QUESTIONS- continued

4. COST EFFECTIVENESS (0-10 POINTS)

- A. Describe the alternatives that were considered. Discuss the relative costs and benefits of all the alternatives and explain why the nominated one was chosen.

The purpose and need for the Ekwil-Fowler Project was to provide better access to Old Town Goleta and the Municipal Airport, to increase safety, and to improve circulation in the vicinity of the project. The first alternative that was considered and which went into the project development process was a relinquishment of SR 217. The concept was that by converting SR 217 from a state highway to a local road, additional east-west connections could be created which would lead to more access points to Old Town Goleta and the many developable parcels to the west of SR 217. The concept would have required the City to incur the maintenance responsibility for the roadway and bridge structures, and construct access improvement improvements including new bridge structures over the San Jose Creek channel. Ultimately the alternative was eliminated due to the access problems that were posed for the UCSB, which is located at the southern terminus of SR 217, and which uses SR 217 as its primary ingress/egress.

After the relinquishment was determined to be infeasible, the current alternative was developed, which accomplishes many of the same goals as the original concept, but at a lower cost. The extension of Ekwil Street and Fowler Road were always a part of the relinquishment project. The cost of the relinquishment alternative was estimated at around \$60 million. The current alternative is considerably less expensive because it has eliminated the bridge structures over San Jose Creek channel, the connections to SR 217, as well as the improvement costs associated with converting SR 217 from an access controlled state highway into a local road.

- B. Calculate the ratio of the benefits of the project relative to both the total project cost and funds requested (i.e., $\frac{\textit{Benefit*}}{\textit{Total Project Cost}}$ and $\frac{\textit{Benefit*}}{\textit{Program Funds Requested}}$).

For the Ekwill-Fowler Project, the benefit associated with bikeway and pedestrian improvements has been calculated at \$5,709,106. This benefit assumes that the project goes into service in 2016. Annual benefits were calculated out to the year 2030, and the benefit numbers were discounted to 2015 values at a 3% rate.

Based on the calculated project benefit and the costs associated with only the alternatives modes improvements that the project will construct (\$3,986,078), the B/C ratio is 1.33 (Attachment D).

Factors that were readily quantifiable and lent themselves to monetization through some standardized and/or reproducible method were used for the calculation of benefit.

Health Benefits:

The Caltrans ATP application instructions contain a link to a benefit analysis reference, "Evaluating Active Transportation Benefits and Costs" (Litman, 2014). This reference lists benefits associated with cycling and walking in terms of reduced occurrence of disease, health care costs, and overall improvements in wellness. In order to apply these benefits it is necessary to determine how much walking and bicycling will take place on the new facility. A traffic analysis performed during the project report provided projected automotive traffic volumes on the new roadways. The American Community Survey, published by the U.S. Census Bureau, provided the percentage of roadway users who commute by bicycle and by walking for Santa Barbara County (7.2% and 3.6%, respectively). By combining these datasets with monetization values located in the publication referenced in the ATP program instructions, it is possible to calculate the health benefit of the project associated with walking and biking, using the following equation:

$$(\text{VMT on new facility}) \times (\% \text{ of trips on bike/foot}) \times (\$/\text{mile health benefit}) = \$ \text{ value of benefit}$$

Vehicle Operation Benefits:

Again, the reference material from the application instructions (Litman, 2014) specifies the benefit associated with a reduction in automotive use due to reductions in vehicle operating costs and vehicle depreciation. The reference material states that the 2008 operating costs are between \$0.10 and

\$0.15 per mile, and for depreciation they are between \$0.05 and \$0.15 per mile. For this analysis, average values of \$0.13 and \$0.10 were used for operating and depreciation costs, respectively. The assumption was made that the 7.2% of bicycle trips and 3.6% of pedestrian trips are a 1:1 substitution for what would have otherwise been automotive trips. The benefit is only assumed on the new facilities, not at any origin or designation points beyond the project. This is because no data exists to quantify benefits back to trip origins or destinations.

Emissions Benefits:

Since a mode switch has been calculated for the Health Benefits and Vehicle Operation Benefits, it is possible to use the reduction in VMT to calculate a reduction in greenhouse gas emissions. The California Air Resource Board website has information on the amount of Volatile Organic Compounds (VOC), Carbon Dioxide (CO₂), and Nitrous Oxide (NO_x) as a function of VMT (<http://www.arb.ca.gov/msei/modeling.htm>). Information needed to monetize the output of these materials is available through the U.S. DOT TIGER grant program, BCA Resource Guide (<http://www.dot.gov/sites/dot.gov/files/docs/TIGER%20BCA%20Resource%20Guide%202014.pdf>).

Using the following formula, the costs associated with emission reductions can be calculated:

(Emission generated/vehicle-mile) x (\$ Cost/ton of emission) = Benefit associated with emission reduction.

Other Factors:

Funding was not available during the preparation of the ATP grant application to collect the data and perform sufficient analysis to do a truly comprehensive Benefit Cost Analysis, so many common BCA benchmark subject areas were not evaluated. Economic development potential, real property value, accident costs (property damage, injury, and fatality), and delay productivity costs associated with delay are just a few areas where the entire roadway improvement project is expected to create significant benefits, not just the bicycle and pedestrian portion.

5. IMPROVED PUBLIC HEALTH (0-10 points)

- A. Describe how the project will improve public health, i.e. through the targeting of populations who have a high risk factor for obesity, physical inactivity, asthma, or other health issues.

The Ekwil-Fowler Project provides ample opportunity for the improvement of public health through the creation of public infrastructure that will encourage bicycling and walking in the City of Goleta. In Santa Barbara County (the county in which Goleta is located), approximately 49,000 children and adults have been diagnosed with asthma. The highest risk factor for asthma in Santa Barbara County, more than twice that of any other risk factor is obesity (www.californiabreathing.com). Physical activity is among the most effective ways to combat obesity. The US Department of Health and Human Services 2008 Physical Activity Guidelines for Americans recommends at least 2.5 hours of moderate activity (such as brisk walking or brisk bicycling) a week (<http://www.webmd.com/fitness-exercise/walking-for-wellness>). The proposed project provides significant opportunities to increase physical activity through increased walking and bicycling.

The proposed project will provide new connections which effectively create a grid structure in the vicinity of Old Town Goleta. A grid structure is the most efficient configuration for a street network because it allows maximum flexibility for circulation and trip distance optimization. The area to the south of Old Town Goleta is a historically industrial area where a variety of new commercial development is planned. Projects such as the proposed Gilkeson Industrial Project, the proposed Thornwood Industrial Project, and the proposed Page property live/work residential and commercial units will offer new employment opportunities that will be accessible by bicycle once the new roadway extensions on Ekwil and Fowler are in place with their alternative modes facilities. The new job centers proposed for the area will be easily reachable from UCSB, as well as many of the adjacent residential areas in Goleta, by bicycle and to some extent, on foot. The project is also located between the Goleta State beach, UCSB, and Old Town Goleta (on Hollister Avenue) which are already significant generators of alternatives modes traffic. The proposed Ekwil Fowler project will provide more opportunities or alternatives modes connectivity in the area. In addition to the health benefits created by

increased walking and bicycling associated with the new infrastructure, the project will also result in a reduction in greenhouse gas emissions as residents elect to travel by bike or foot instead of in automobiles.

6. BENEFIT TO DISADVANTAGED COMMUNITIES (0-10 points)

A. I. Is the project located in a disadvantaged community? Y/N

7. USE OF CALIFORNIA CONSERVATION CORPS (CCC) OR A CERTIFIED COMMUNITY CONSERVATION CORPS (0 to -5 points)

- A. The applicant has coordinated with the CCC to identify how a state conservation corps can be a partner of the project. Y/N
 - a. Name, e-mail, phone # of the person contacted and the date the information was submitted
Ms. Virginia Clark, virginia.clark@ccc.ca.gov, 916. 341.3147
Phone conversation on Thursday, May 15 at 5:01 pm.
E-mailed information on Friday, May 16 at 2:27 pm.
- B. The applicant has coordinated with a representative from the California Association of Local Conservation Corps (CALCC) to identify how a certified community conservation corps can be a partner of the project. Y/N
 - a. Name, e-mail, phone # of the person contacted and the date the information was submitted
Ms. Cynthia Vitale, calocalcorps@gmail.com, 916. 558.1516
E-mailed information on Friday, May 16 at 2:26 pm.
- C. The applicant intends to utilize the CCC or a certified community conservation corps on all items where participation is indicated? Y/N

I have coordinated with a representative of the CCC; and the following are project items that they are qualified to partner on:

The CCC is interested in partnering on this ATP project. The following are items the CCC is capable and qualified to work on:

- a. Installing stormwater protection controls: Fiber rolls, silt fence,
- b. Removing chain link fence,
- c. Removing Trees,
- d. Landscaping,
- e. Posts and sign installation, and
- f. Installation of chain link fencing.

I have coordinated with a representative of the CALCC; and the following are project items that they are qualified to partner on:

Thank you for contacting CALCC. Unfortunately, no local conservation corps will be able to partner on this project due to geographic constraints

8. APPLICANT'S PERFORMANCE ON PAST GRANTS (0 to -10 points)

- A. Describe any of your agency's ATP type grant failures during the past 5 years, and what changes your agency will take in order to deliver this project.

The City has received grants from the Federal Highway Safety Improvement Program, Measure A Regional Bicycle and Pedestrian Program Grants and Measure A Safe Routes to School, Proposition 84 grant funding, Community Development Block Grant (CDBG) funding, State Parks grant funding, Highway Bridge Program and others. The City has not had any grant failures to date. When awarded this ATP grant for construction funding for the Project, the City will adjust the priorities in the Capital Improvement Program and focus resources on this Project

Project name:

V. PROJECT PROGRAMMING REQUEST

Applicant must complete a Project Programming Request (PPR) and attach it as part of this application. The PPR and can be found at http://www.dot.ca.gov/hq/transprog/allocation/ppr_new_projects_9-12-13.xls

PPR Instructions can be found at <http://www.dot.ca.gov/hq/transprog/ocip/2012stip.htm>

Notes:

- Fund No. 1 must represent ATP funding being requested for program years 2014/2015 and 2015/2016 only.
- Non-infrastructure project funding must be identified as Con and indicated as “Non-infrastructure” in the Notes box of the Proposed Cost and Proposed Funding tables.
- Match funds must be identified as such in the Proposed Funding tables.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

General Instructions

<input checked="" type="checkbox"/> New Project					Date:	5/20/14
District	EA	Project ID	PPNO	MPO ID	TCRP No.	
05	4611U	0500000548	4611	SBCAG/6090		
County	Route/Corridor	PM Bk	PM Ahd	Project Sponsor/Lead Agency		
SB	Hollister/ SR 217	1.1	2.2	Goleta, City of		
SB	Ekwill Street			MPO	Element	
SB	Fowler Road			SBCAG	Capital Outlay	
Project Manager/Contact		Phone		E-mail Address		
Rosemarie Gaglione		(805) 961-7569		rgaglione@cityofgoleta.org		
Project Title						
Fowler Rd & Ekwill St Extension						
Location, Project Limits, Description, Scope of Work						<input type="checkbox"/> See page 2
In Goleta, from 0.1 mile north of San Jose Creek Bridge to Hollister Avenue. The larger roadway project will construct roundabouts at Hollister Avenue ramps with SR 217, extend Fowler Road from Fairview Avenue to Kellogg Avenue, and extend Ekwill Street to Kellogg Avenue. The ATP Grant scope of work includes constructing Class II bike lanes and sidewalks along the new roadway extensions, new and improved intersection crossings at existing intersections, roundabouts that include improved bicycle and pedestrian configurations, and a multi-use facility adjacent to the Old San Jose Creek.						
<input type="checkbox"/> Includes ADA Improvements <input checked="" type="checkbox"/> Includes Bike/Ped Improvements						
Component	Implementing Agency					
PA&ED	Goleta, City of					
PS&E	Goleta, City of					
Right of Way	Goleta, City of					
Construction	Goleta, City of					
Purpose and Need						<input type="checkbox"/> See page 2
The existing roadway system within Old Town Goleta has inadequate east-west circulation both North and South of Hollister Avenue and lacks direct access into Old Town Goleta, the Santa Barbara Airport and UCSB. Pedestrian and bicycle access is very limited. Old Town Goleta south of Hollister Avenue is not pedestrian or bicyclist friendly. There are no Class II bike facilities within the project vicinity and many existing streets do not have sidewalks. To address these deficiencies, the City's General Plan Transportation Element includes policies to increase pedestrian and bicyclist access. The purpose of the Ekwill-Fowler Road Extensions Project is to improve connectivity and access and to reduce congestion along Hollister Avenue.						
Project Benefits						<input type="checkbox"/> See page 2
The Project will provide enhanced vehicular, pedestrian and bicycle access into and within Old Town Goleta and to the Santa Barbara Airport, UCSB and recreational coastal destinations immediately south. The Project will also greatly improve circulation and operations at the Route 217/Hollister Avenue interchange.						
<input checked="" type="checkbox"/> Supports Sustainable Communities Strategy (SCS) Goals <input checked="" type="checkbox"/> Reduces Greenhouse Gas Emissions						
Project Milestone						Proposed
Project Study Report Approved						09/07/97
Begin Environmental (PA&ED) Phase						01/01/02
Circulate Draft Environmental Document				Document Type	EIR	09/01/11
Draft Project Report						09/01/11
End Environmental Phase (PA&ED Milestone)						01/04/12
Begin Design (PS&E) Phase						10/01/12
End Design Phase (Ready to List for Advertisement Milestone)						05/12/15
Begin Right of Way Phase						08/21/13
End Right of Way Phase (Right of Way Certification Milestone)						05/15/15
Begin Construction Phase (Contract Award Milestone)						09/30/15
End Construction Phase (Construction Contract Acceptance Milestone)						08/29/17
Begin Closeout Phase						08/29/17
End Closeout Phase (Closeout Report)						11/15/17

ADA Notice For individuals with sensory disabilities, this document is available in alternate formats. For information call (916) 654-6410 or TDD (916) 654-3880 or write Records and Forms Management, 1120 N Street, MS-89, Sacramento, CA 95814.

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

Date: 5/20/14

District	County	Route	EA	Project ID	PPNO	TCRP No.
05	SB , SB, SB	Hollister/ SR 217,	4611U	0500000548	4611	
Project Title: Fowler Rd & Ekwil St Extension						

Proposed Total Project Cost (\$1,000s)									Notes
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	
E&P (PA&ED)	2,022							2,022	
PS&E	1,422	631						2,053	
R/W SUP (CT)									
CON SUP (CT)									
R/W	5,409							5,409	
CON			14,387					14,387	
TOTAL	8,853	631	14,387					23,871	

Request

Fund No. 1:	ATP Funding								Program Code
Proposed Funding (\$1,000s)									20.30.720
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									STATE
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON			2,010					2,010	
TOTAL			2,010					2,010	

Fund No. 2:	LOCAL FUNDS - Development Impact Fees (LOCAL MATCH)								Program Code
Proposed Funding (\$1,000s)									
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									LOCAL
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON			260					260	
TOTAL			260					260	

Fund No. 3:	RIP - National Highway System (NH)								Program Code
Proposed Funding (\$1,000s)									
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									FEDERAL
PS&E	980							980	
R/W SUP (CT)									
CON SUP (CT)									
R/W	3,581							3,581	
CON			11,372					11,372	
TOTAL	4,561		11,372					15,933	

PROJECT PROGRAMMING REQUEST

DTP-0001 (Revised July 2013)

Date: 5/20/14

District	County	Route	EA	Project ID	PPNO	TCRP No.
05	SB , SB, SB	Hollister/ SR 217,	4611U	0500000548	4611	
Project Title: Fowler Rd & Ekwill St Extension						

Fund No. 4:	RIP - State Cash (ST-CASH)								Program Code
Proposed Funding (\$1,000s)									
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)	2,022							2,022	STATE
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL	2,022							2,022	

Fund No. 5:	Local Funds - Development Impact Fees								Program Code
Proposed Funding (\$1,000s)									
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									LOCAL
PS&E	442	631						1,073	
R/W SUP (CT)									
CON SUP (CT)									
R/W	1,828							1,828	
CON			745					745	
TOTAL	2,270	631	745					3,646	

Fund No. 6:									Program Code
Proposed Funding (\$1,000s)									
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									

Fund No. 7:									Program Code
Proposed Funding (\$1,000s)									
Component	Prior	14/15	15/16	16/17	17/18	18/19	19/20+	Total	Funding Agency
E&P (PA&ED)									
PS&E									
R/W SUP (CT)									
CON SUP (CT)									
R/W									
CON									
TOTAL									

Project name:

VI. ADDITIONAL INFORMATION

Only fill in those fields that are applicable to your project

FUNDING SUMMARY

ATP Funds being requested by Phase (to the nearest \$1000)

Amount

PE Phase (includes PA&ED and PS&E)	\$
Right-of-Way Phase	\$
Construction Phase-Infrastructure	\$
Construction Phase-Non-infrastructure	\$
Total for ALL Phases	\$

All Non-ATP fund types on this project* (to the nearest \$1000)

Amount

	\$
	\$
	\$
	\$
	\$
	\$

*Must indicate which funds are matching

Total Project Cost	\$
Project is Fully Funded	

ATP Work Specific Funding Breakdown (to the nearest \$1000)

Amount

Request for funding a Plan	\$
Request for Safe Routes to Schools Infrastructure work	\$
Request for Safe Routes to Schools Non-Infrastructure work	\$
Request for other Non-Infrastructure work (non-SRTS)	\$
Request for Recreational Trails work	\$

ALLOCATION/AUTHORIZATION REQUESTS SCHEDULE

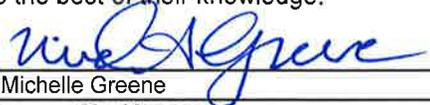
	Proposed Allocation Date	Proposed Authorization (E-76) Date
PA&ED or E&P		
PS&E		
Right-of-Way		
Construction		

All project costs MUST be accounted for on this form, including elements of the overall project that will be, or have been funded by other sources.

Project name: **EKWILL-FOWLER BICYCLE AND PEDESTRIAN IMPROVEMENT PROJECT**

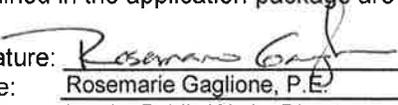
VIII. APPLICATION SIGNATURES

Applicant: The undersigned affirms that the statements contained in the application package are true and complete to the best of their knowledge.

Signature: 
Name: Michelle Greene
Title: Interim City Manager

Date: 5-19-14
Phone: (805) 961-7522
e-mail: mgreene@cityofgoleta.org

Local Agency Official (City Engineer or Public Works Director): The undersigned affirms that the statements contained in the application package are true and complete to the best of their knowledge.

Signature: 
Name: Rosemarie Gaglione, P.E.
Title: Interim Public Works Director

Date: 5/19/2014
Phone: (805) 961-7569
e-mail: rgaglione@cityofgoleta.org

School Official: The undersigned affirms that the school(s) benefited by this application is not on a school closure list.

Signature: _____
Name: _____
Title: _____

Date: _____
Phone: _____
e-mail: _____

Person to contact for questions:

Name: _____
Title: _____

Phone: _____
e-mail: _____

Caltrans District Traffic Operations Office Approval*

If the application's project proposes improvements on a freeway or state highway that affects the safety or operations of the facility, it is required that the proposed improvements be reviewed by the district traffic operations office and either a letter of support or acknowledgement from the traffic operations office be attached or the signature of the traffic personnel be secured below.

Signature: _____
Name: _____
Title: _____

Date: _____
Phone: _____
e-mail: _____

*Contact the District Local Assistance Engineer (DLAE) for the project to get Caltrans Traffic Ops contact information. DLAE contact information can be found at <http://www.dot.ca.gov/hq/LocalPrograms/dlae.htm>

Project name:

VIII. ADDITIONAL APPLICATION ATTACHMENTS

Check all attachments included with this application.

- Vicinity/Location Map- **REQUIRED for all IF Projects**
 - North Arrow
 - Label street names and highway route numbers
 - Scale

- Photos and/or Video of Existing Location- **REQUIRED for all IF Projects**
 - Minimum of one labeled color photo of the existing project location
 - Minimum photo size 3 x 5 inches
 - Optional video and/or time-lapse

- Preliminary Plans- **REQUIRED for Construction phase only**
 - Must include a north arrow
 - Label the scale of the drawing
 - Typical Cross sections where applicable with property or right-of-way lines
 - Label street names, highway route numbers and easements

- Detailed Engineer's Estimate- **REQUIRED for Construction phase only**
 - Estimate must be true and accurate. Applicant is responsible for verifying costs prior to submittal
 - Must show a breakdown of all bid items by unit and cost. Lump Sum may only be used per industry standards
 - Must identify all items that ATP will be funding
 - Contingency is limited to 10% of funds being requested
 - Evaluation required under the ATP guidelines is not a reimbursable item

- Documentation of the partnering maintenance agreement- Required with the application if an entity, other than the applicant, is going to assume responsibility for the operation and maintenance of the facility

- Documentation of the partnering implementation agreement-Required with the application if an entity, other than the applicant, is going to implement the project.

- Letters of Support from Caltrans (Required for projects on the State Highway System(SHS))

- Digital copy of or an online link to an approved plan (bicycle, pedestrian, safe routes to school, active transportation, general, recreation, trails, city/county or regional master plan(s), technical studies, and/or environmental studies (with environmental commitment record or list of mitigation measures), if applicable. Include/highlight portions that are applicable to the proposed project.

- Documentation of the public participation process (required)

- Letter of Support from impacted school- when the school isn't the applicant or partner on the application (required)

- Additional documentation, letters of support, etc (optional)

ATTACHMENT A



FIGURES



Figure 1 – Vicinity Map



Figure 2 - Location Map

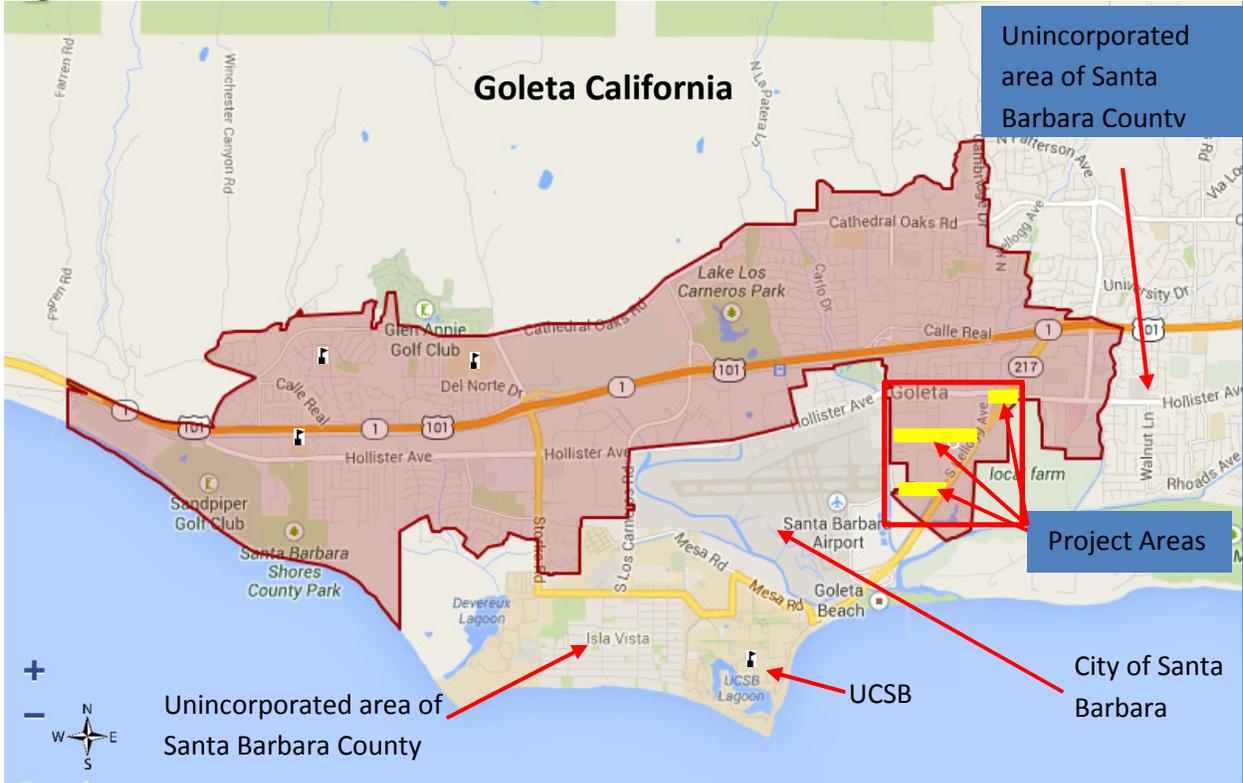


Figure 3 – City of Goleta, California Map



<p>PLANNING ■ DESIGN ■ CONSTRUCTION</p> <p>RBF CONSULTING</p> <p>500 YONGOC VALLEY ROAD, SUITE 270 WALNUT CREEK, CALIFORNIA 94596 925.900.1460 • FAX 925.900.1465 • www.RBF.com</p>		<p>PROJECT MAP</p> <p>JUNE 2006</p> <p>SCALE: 1:5000</p>
<p>DESIGNED BY:</p>	<p>DRAWN BY:</p>	<p>CHECKED BY:</p>
<p>JN 30100607</p> <p>EXHIBIT A</p> <p>SHEET: 1 OF 1</p>		

Figure 4 – Project Design Exhibit

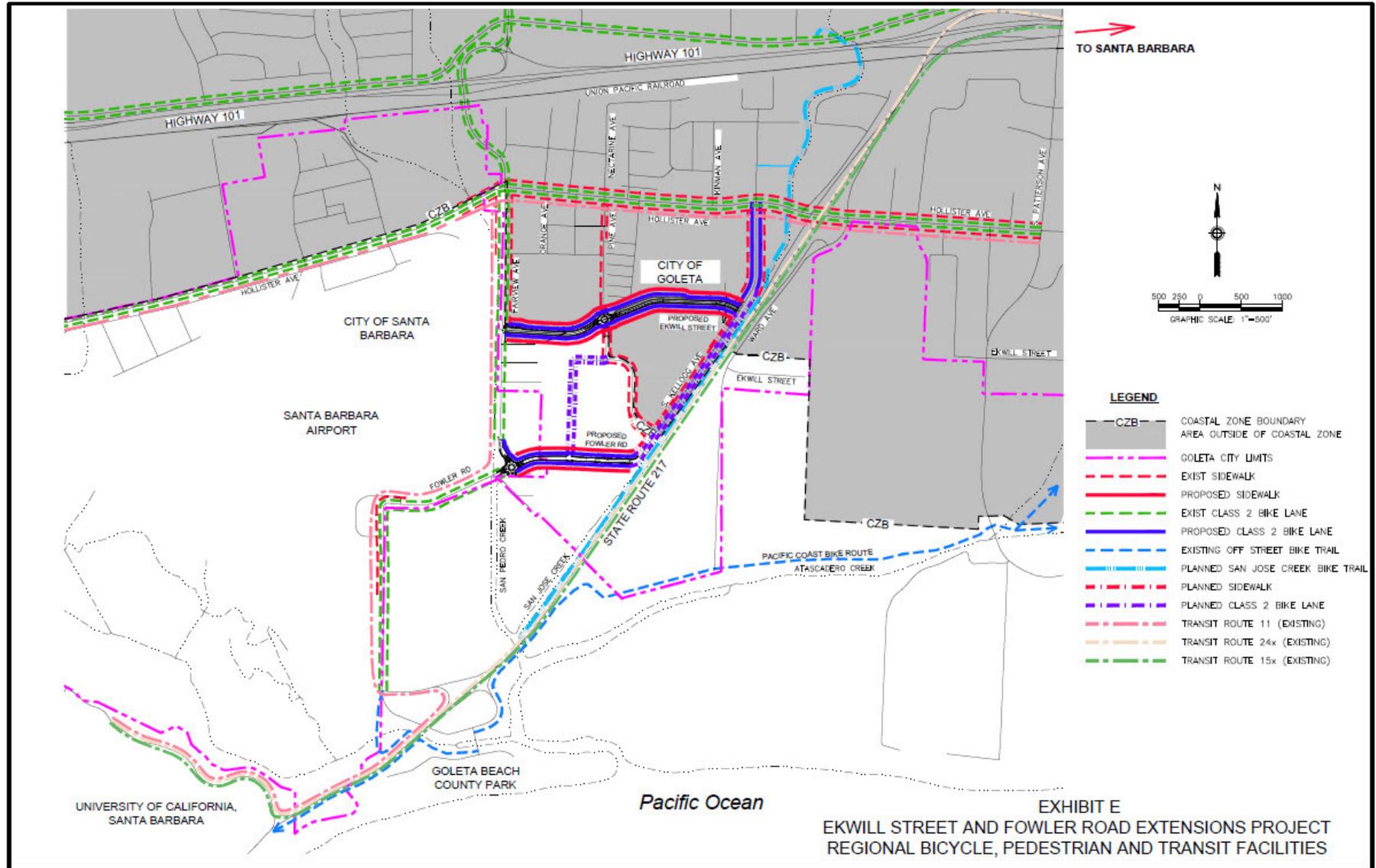


Figure 5 – Project Area Bicycle, Pedestrian, and Transit Facilities

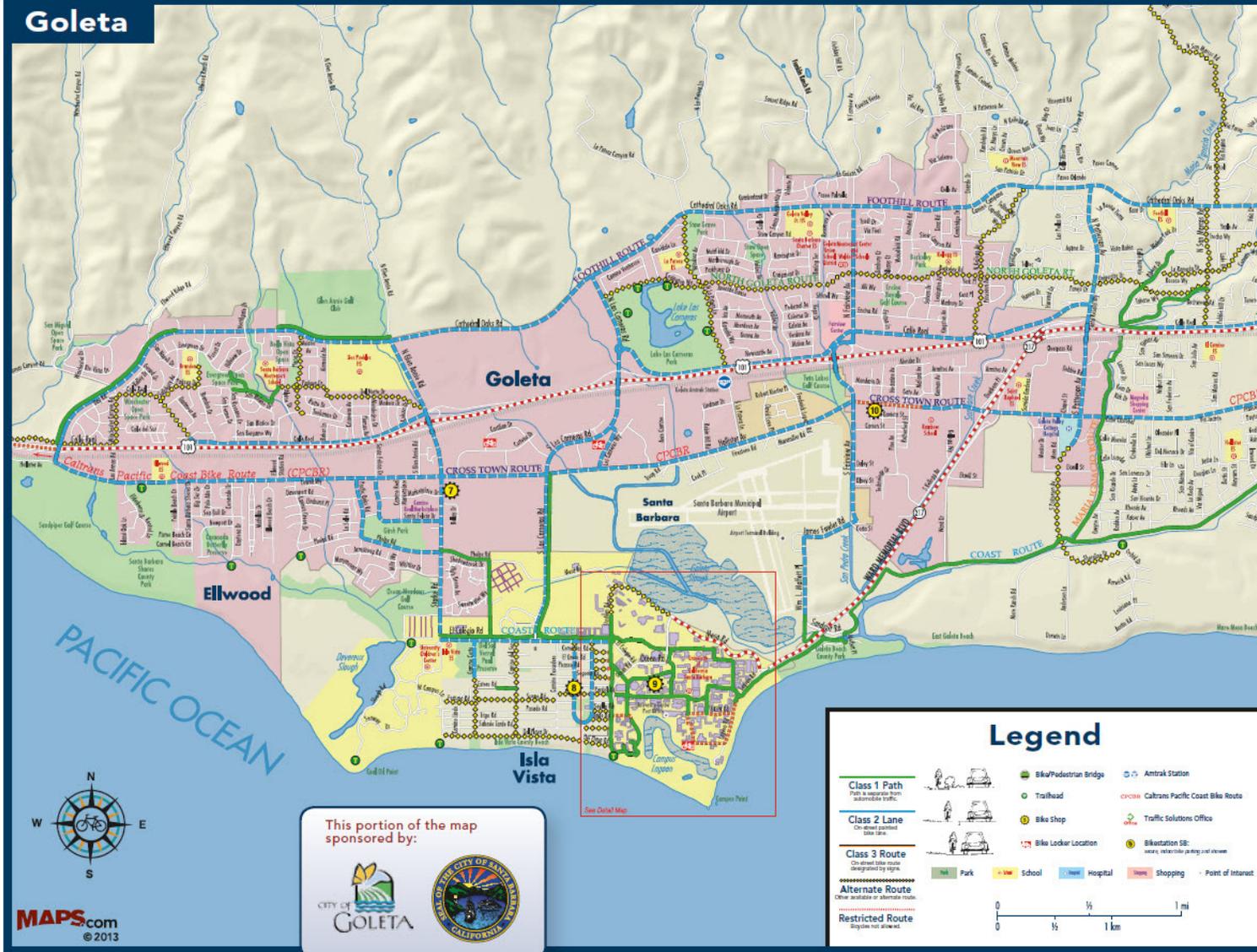


Figure 6 – Citywide Bikeways Map

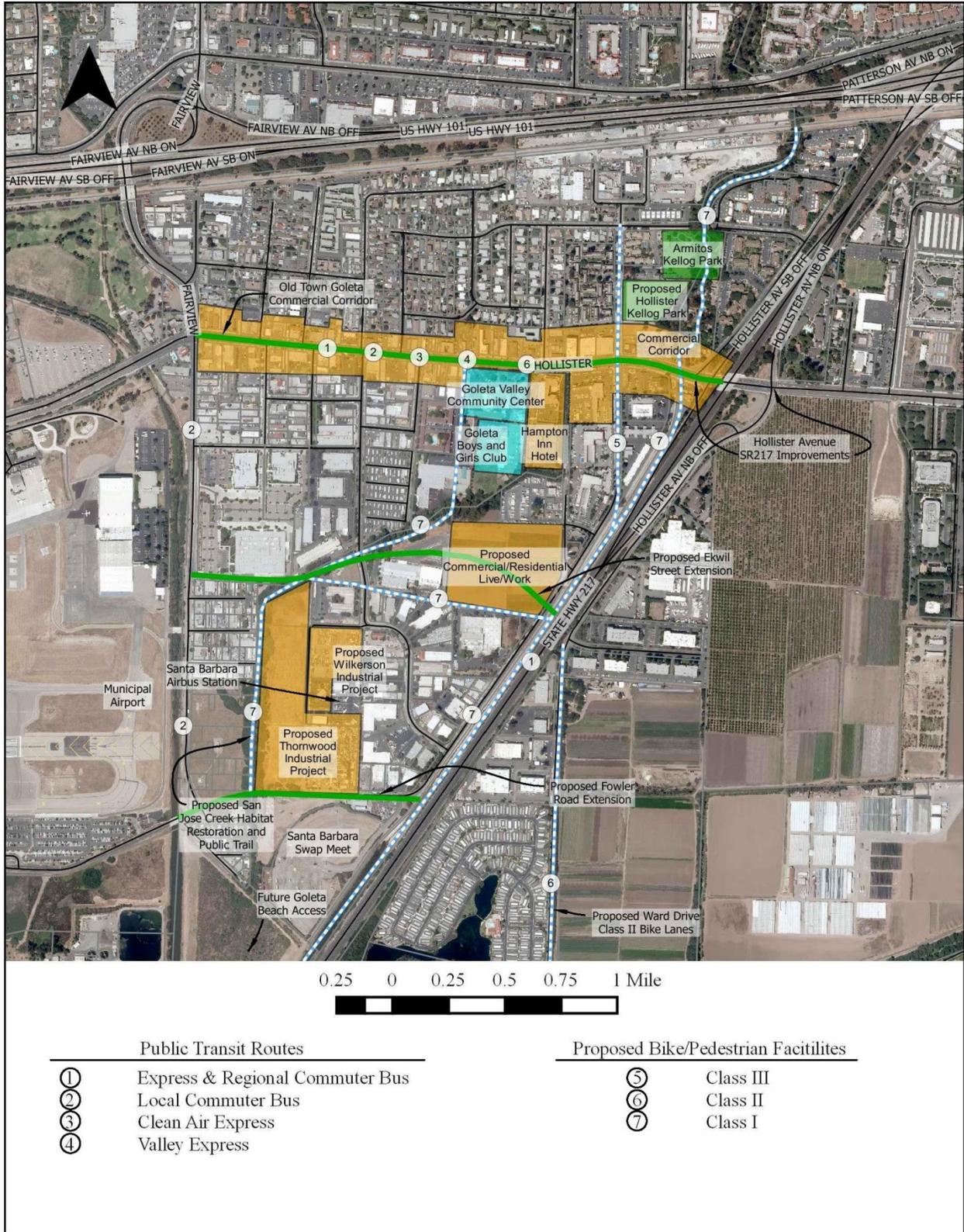
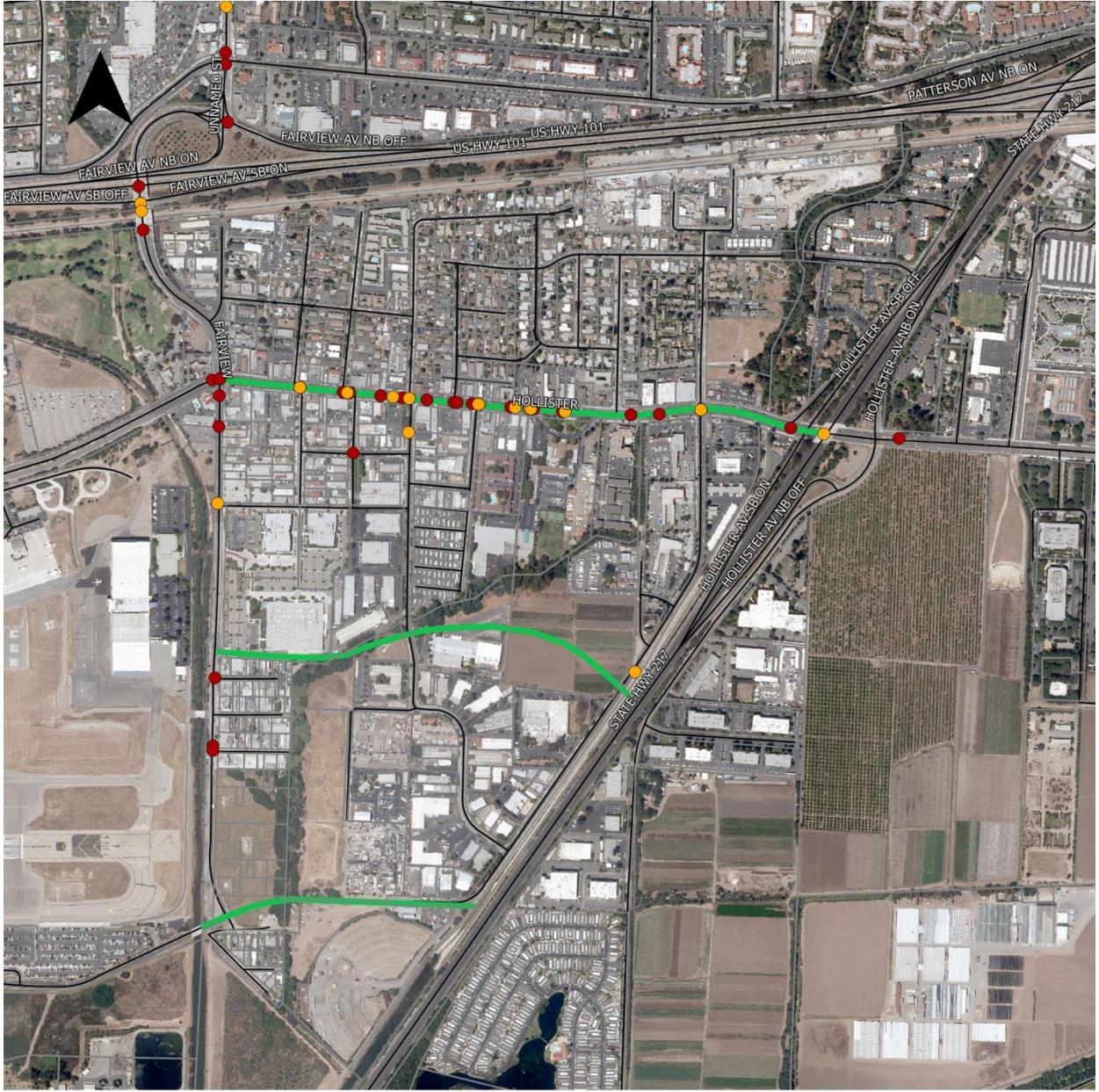


Figure 7 – Project Area Transit, Bike, and Facility Uses and Destinations



- Pedestrian Accidents
- Bicycle Accidents

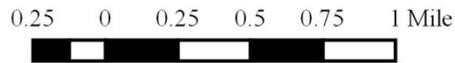


Figure 8 – Accident Location

ATTACHMENT B

Ekwill Street Extension Photos



Photo 1 – ATK at Pine and future corner of Ekwill Street



Photo 2 – Future Fairview - Ekwill intersection



Photo 3 – Looking EB from Fairview at Future Ekwil Street



Photo 4 – Future Ekwil Street to go alongside ATK Parking Lot



Photo 5 – Future Ekwil alignment adjacent to parking lot

Fowler Road Extension Photos



Photo 6 – Area adjacent of future Fowler-Fairview Roundabout 2



Photo 7 – Area adjacent of future Fowler-Fairview Roundabout



Photo 8 – Future Fowler and Fairview Roundabout looking SE 3



Photo 9 – Businesses use future Fowler as parking lot or junk yard

Hollister Avenue Class I Bike Path Safe Routes to School



Photo 10 – Looking EB at Future Fowler Road towards Creek Crossing



Photo 11 - Businesses using future Fowler as a parking lot and junk yard 2



Photo 12 – SB on Technology Drive



Photo 13 – NB on Fairview toward future roundabout with Fowler

Hollister Avenue Class I Bike Path Safe Routes to School



Photo 14 – SB on Fairview toward future roundabout with Fowler

Hollister Avenue/SR 217 Roundabout Intersection Photos



Photo 15 – 217 Bridge over Hollister



Photo 16 – Hollister at Dearborn looking east



Photo 17 – Ward Drive at HWY 217 off ramp



Photo 18 – Ward Drive NB to Hollister Avenue



Photo 19 – On Dearborn looking south towards 217 on-ramp



Photo 20 – HWY 217 SB off ramp and Ward Drive on north side of Hollister



Photo 21 – Hollister at HWY 217

ATTACHMENT C
- Ekwil Street

CITY OF GOLETA
PUBLIC WORKS DEPARTMENT

PROJECT PLANS FOR CONSTRUCTION ON
CITY STREET
EKWILL STREET EXTENSION
IN GOLETA

TO BE SUPPLEMENTED BY CALTRANS STANDARD PLANS DATED 2010
AND COUNTY OF SANTA BARBARA STANDARD DETAILS DATED SEPTEMBER 2011

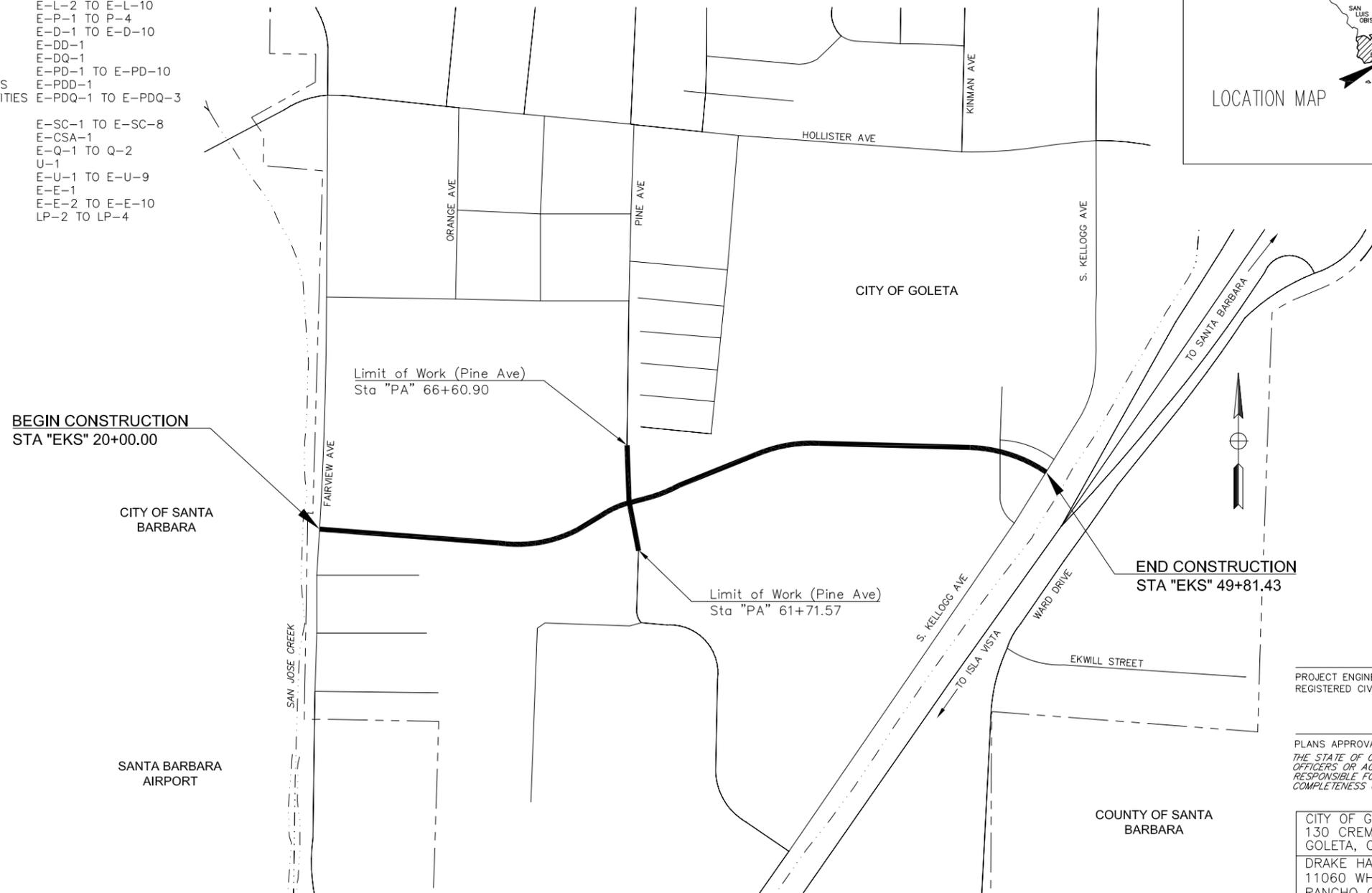
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			1	78



APPROVED AS TO IMPACT ON STATE FACILITIES AND CONFORMANCE WITH APPLICABLE STATE STANDARDS AND PRACTICES AND THAT TECHNICAL OVERSIGHT WAS PERFORMED.

INDEX OF PLANS

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BEGIN CONSTRUCTION
STA "EKS" 20+00.00

Limit of Work (Pine Ave)
Sta "PA" 66+60.90

Limit of Work (Pine Ave)
Sta "PA" 61+71.57

END CONSTRUCTION
STA "EKS" 49+81.43

NO SCALE
65% SUBMITTAL NOT FOR CONSTRUCTION

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

PROJECT ENGINEER _____ DATE _____
REGISTERED CIVIL ENGINEER



PLANS APPROVAL DATE _____
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

CITY OF GOLETA, DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN & ASSOCIATES
11060 WHITE ROCK ROAD, SUITE 200
RANCHO CORDOVA, CA 95670

CONTRACT No.	05-4611U4
PROJECT ID	0500000548



DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	Local	N/A	1	87

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

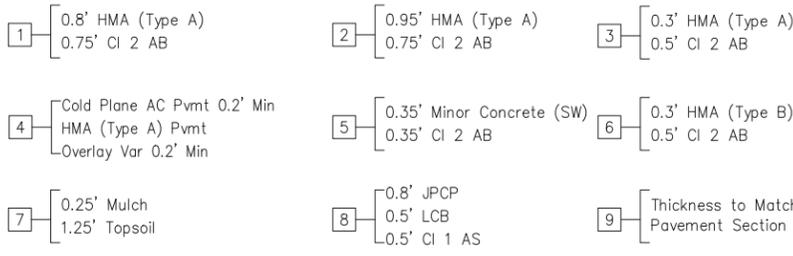
MNS ENGINEERS INC
 201 N. Calle Cesar Chavez, Suite 300
 Santa Barbara, CA 93110
 805.892.8921 Phone

ENGINEERING SURVEYING
 CONSTRUCTION MANAGEMENT

REGISTERED PROFESSIONAL ENGINEER
 ADAM S. CHASE
 No. 74043
 Exp. 06-30-15
 CIVIL
 STATE OF CALIFORNIA

The State of California or its officers or agents or MNS Engineers, Inc. or City of Goleta shall not be responsible for the accuracy or completeness of electronic copies of this plan.

Typical Pavement Sections



Existing Structural Sections



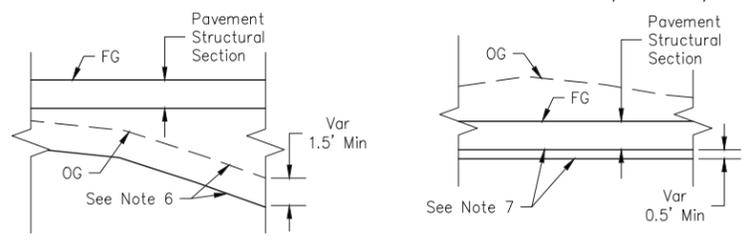
General Notes

- All dimensions for the measurement of length are in feet, unless otherwise noted.
- The project abbreviations for EKWILL STREET/PINE AVENUE/KELLOGG WAY/FAIRVIEW AVENUE Improvements are per Caltrans Std Plans A10A and A10B.
- The Contractor is to obtain an R-Value at the subgrade shall have a Registered Geotechnical or Civil Engineer use Caltrans test method No. 301 and as referenced by the California Department of Transportation Highway Design Manual to either determine a new structural pavement section or verify the structural pavement sections currently provided.
- Dimensions of the pavement structural sections are subject to tolerances specified in the standard specifications.
- See LIGHTING PLANS for conduit information not shown.
- Overexcavate 1.5' min below original ground in fill areas; overexcavation should extend horizontally a minimum distance of 18" from fill edges.
- Overexcavate 0.5' min below pavement structural sections in cut areas; overexcavation should extend horizontally a minimum distance of 12" from cut edges.
- Median Curb and/or Gutter per County of Santa Barbara Std Plan 4-035.
- Sidewalk per County of Santa Barbara Std Plan 5-040.
- Modified 3" Mountable Curb and Gutter per Caltrans Std Plan A87A Type D-6 see CONSTRUCTION DETAILS.
- Modified Concrete Block Slough Wall with Fence detail per RETAINING WALL sheets.
- See CONSTRUCTION DETAILS for landscaping height restrictions.

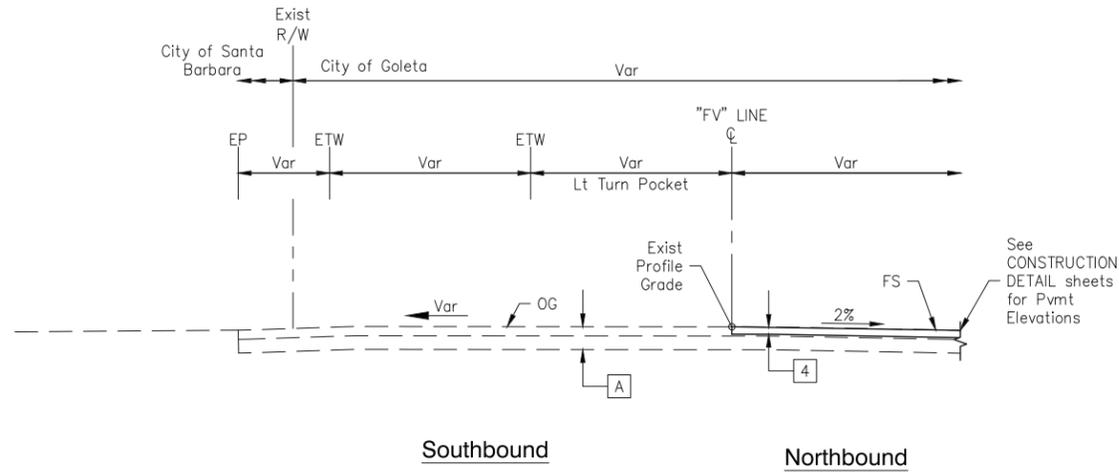
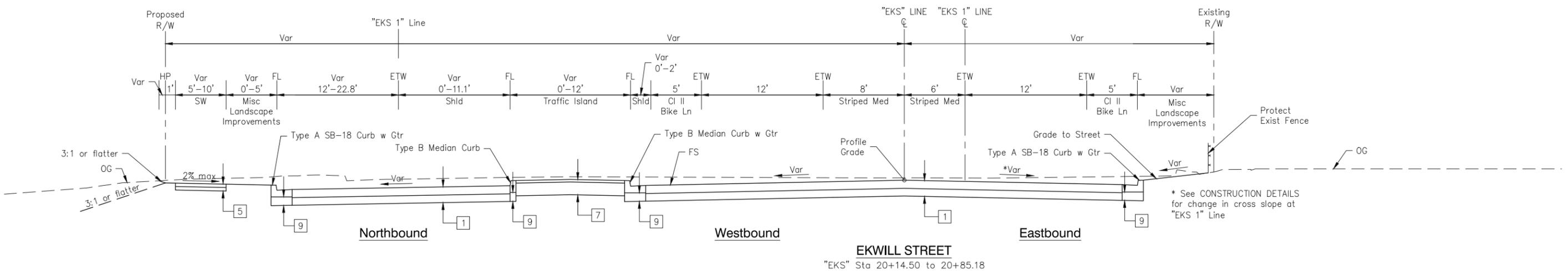
Design Designations

EKWILL STREET (Western Segment)				EKWILL STREET (Eastern Segment)			
2013 ADT = 4,900	D = 52.4%	2013 ADT = 3,290	D = 51.1%	2035 ADT = 5,360	T = 3%	2035 ADT = 3,600	T = 3%
DHV = 536	V = 30	DHV = 360	V = 30	ESAL = 482,805	TI (20) = 8	ESAL = 316,227	TI (20) = 8
PINE AVENUE (Northern Segment)				PINE AVENUE (Southern Segment)			
2013 ADT = 1,590	D = 51.7%	2013 ADT = 1,960	D = 52.3%	2035 ADT = 1,740	T = 3%	2035 ADT = 2,140	T = 3%
DHV = 174	V = 25	DHV = 214	V = 25	ESAL = 154,638	TI (20) = 8	ESAL = 288,591	TI (20) = 8

EKWILL STREET at PINE AVENUE (Roundabout - Circulatory Roadway)
 V = 15
 TI (20) = 9



Fill Areas
Overexcavation Details
Cut Areas



EKWILL STREET
TYPICAL CROSS SECTIONS

65% SUBMITTAL NOT FOR CONSTRUCTION

NOT TO SCALE E-X-1

REVISOR: Adam Chase
 PROJECT ENGINEER: Adam Chase
 DEPARTMENT OF PUBLIC WORKS

DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	Local	N/A	5	87

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

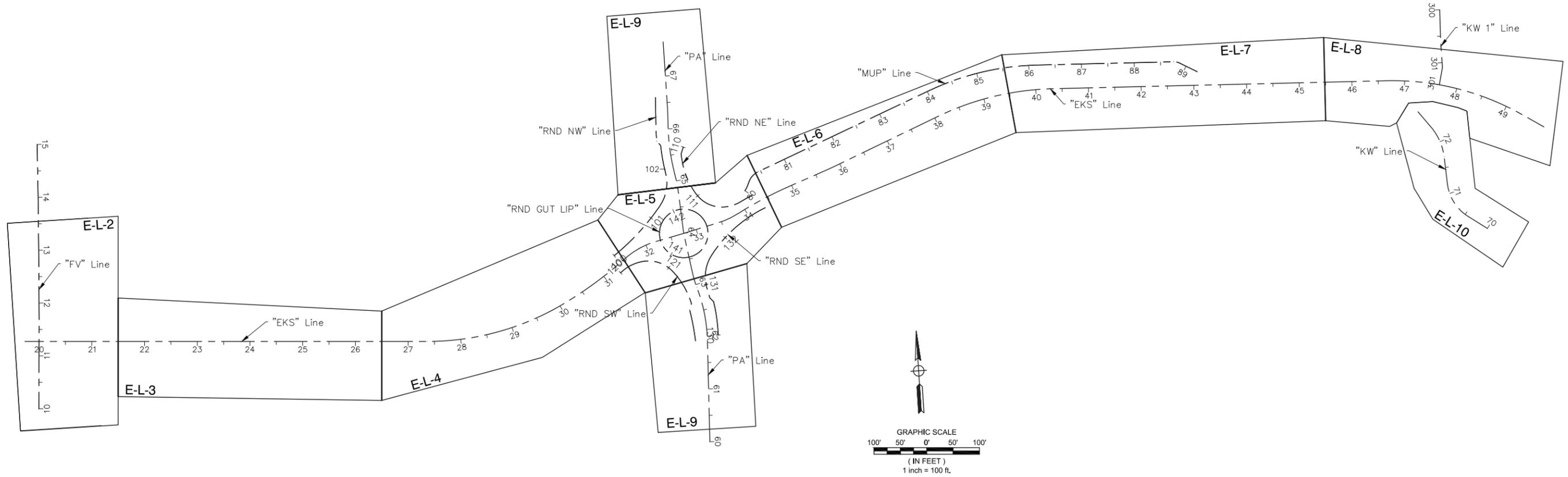
MNS ENGINEERS INC
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ENGINEERING SURVEYING
 CONSTRUCTION MANAGEMENT

REGISTERED PROFESSIONAL ENGINEER
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 No. 74043
 Exp. 06-30-15
 CIVIL
 STATE OF CALIFORNIA

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CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS	PROJECT ENGINEER	CALCULATED-DESIGNED BY	REVISOR
	-----	CHECKED BY	-----
		Adorn Chase	-----
			DATE REVISOR



65% SUBMITTAL NOT FOR CONSTRUCTION

EKWILL STREET
 KEY MAP AND LINE INDEX
 SCALE: 1" = 100'
 E-K-1

DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	LOCAL	N/A	6	87

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LEGEND

-  Cold Plane AC Pvmnt (0.2' Min)
AC Pvmnt Overlay (0.2' Min)
-  Raised Truncated Domes at Access Ramps
-  Construction Line Survey Control Point
- C --- Top of Cut
- F --- Toe of Fill
- x--- Chain Link Fence
- >--- Direction of Ditch Flow
- Sawcut AC Pavement
- Existing Right of Way
- Proposed Right of Way

GENERAL NOTES

- Project Construction Lines are as follows:
 "FV" LINE for FAIRVIEW AVENUE
 "EKS" LINE for EKWILL STREET
 "PA" LINE for PINE AVENUE
 "KW" LINE for KELLOGG WAY
 "MUP" LINE for separated MULTI-USE PATH
- Secondary Construction Lines are as follows:
 "RND NW" LINE for Roundabout Northwest Quadrant Curb Return
 "RND NE" LINE for Roundabout Northeast Quadrant Curb Return
 "RND SE" LINE for Roundabout Southeast Quadrant Curb Return
 "RND SW" LINE for Roundabout Southwest Quadrant Curb Return
 "RND GUT LIP" LINE for Roundabout Gutter Lip
 "EKS 1" LINE for layout and Intersection Grading per CONSTRUCTION DETAILS
 "EKS 2" LINE for Right Turn Lane to FAIRVIEW AVE layout and Intersection Grading per CONSTRUCTION DETAILS
 "KW 1" LINE for Northerly Area of KELLOGG WAY
- All layout data for curbs are referenced to the bottom (flowline) face of curb unless otherwise noted.
- See CONSTRUCTION DETAILS for Intersection grading, Multi-Use Path grading, Driveways, Pedestrian and Bicycle Ramps, Splitter and Traffic Islands, and Truck Apron Details.
- See DRAINAGE PLANS for contour grading.
- See CONSTRUCTION DETAILS for conform and sawcut layout.
- All work shall be performed in accordance with Caltrans Std Plans and Specifications 2010, including all revisions and amendments and the special provisions.
- The Contractor shall notify the Underground Service Alert (U.S.A.) one call program 811 and the City of Goleta at least two working days in advance of beginning any underground work.
- The Contractor shall expose and verify the location, elevation and condition of existing facilities prior to beginning any excavation or other underground work in the vicinity of the improvements.
- The Contractor shall comply with CAL/OSHA regulations regarding safety of excavation. Excavations shall be adequately shored, braced and sheeted to protect against injury, and prevent disturbance to existing facilities that are to remain and be protected in place.
- Any wall, fences, structures and/or appurtenances adjacent to this project shall be protected in place. If grading operations damage or adversely affect said items in any way, the Contractor is responsible for working out an acceptable solution to the satisfaction of the affected property owner(s).
- Contractor shall demolish existing site improvements that conflict with the proposed work. Prior to starting demolition, Contractor shall mark limits of demolition and the items to be demolished as shown per plans. Demolition layout shall be reviewed with the City Inspector. Following City approval, the improvements identified for demolition shall be completely removed. Features removed shall be salvaged or disposed as identified on the plan or as directed by the City public works inspector.
- A thorough search shall be made for all abandoned man-made facilities such as tanks, pipelines and conduits. Should such facilities be encountered, the Contractor shall stop work and notify the City Engineer.
- Contractor shall minimize construction activities within coastal boundary or environmental sensitive areas as shown per the WATER POLLUTION CONTROL plans with temporary fencing.

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS	PROJECT ENGINEER	Adam Chase	REVISOR	
	CALCULATED-DESIGNED BY	Adam Chase	DATE	
	CHECKED BY		DATE	

65% SUBMITTAL NOT FOR CONSTRUCTION

EKWILL STREET
 LAYOUT NOTES AND LEGEND

E-L-1



DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	LOCAL	N/A	7	87

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

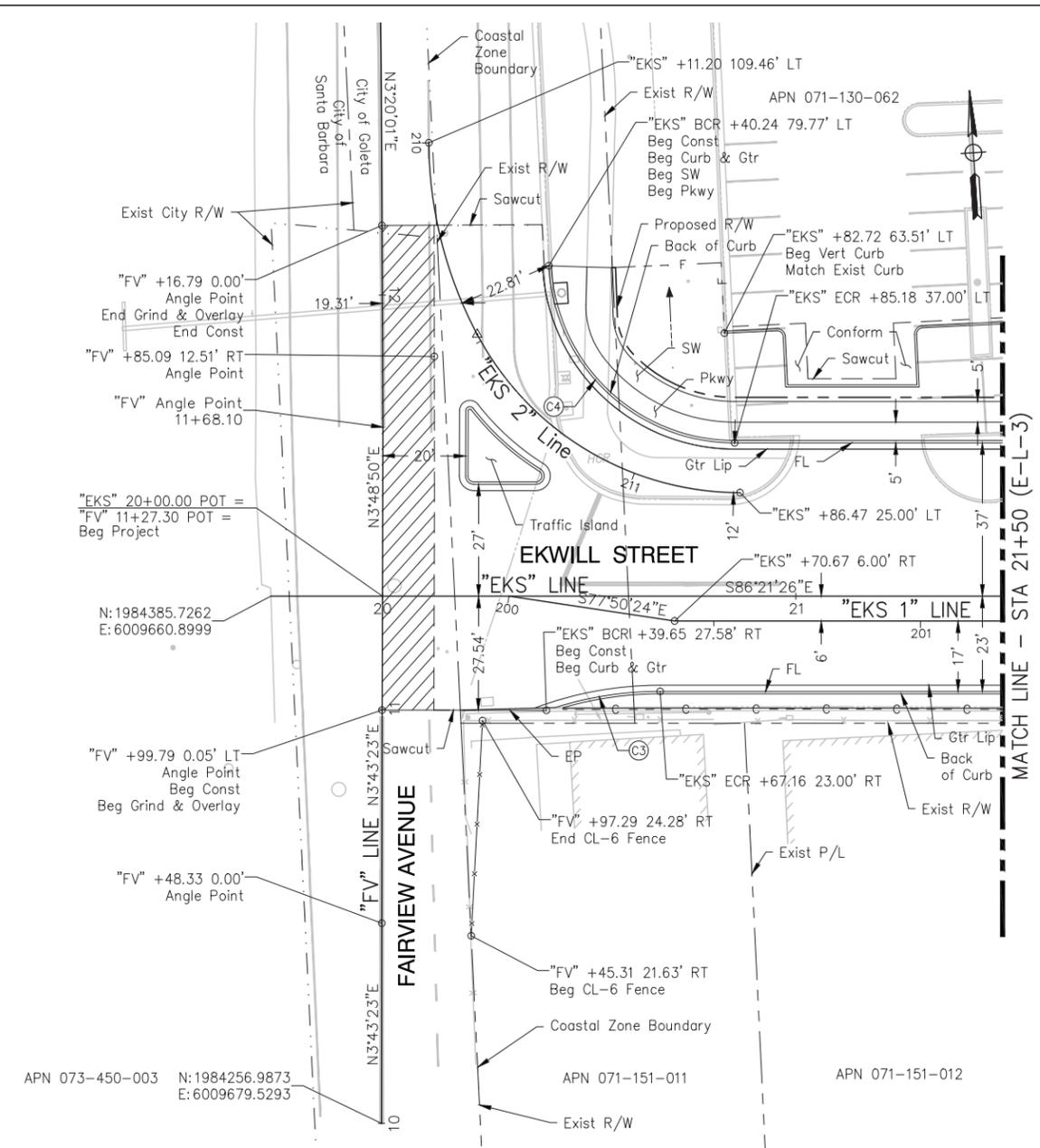
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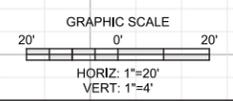
REGISTERED PROFESSIONAL ENGINEER
 ADAM S. CHASE
 No. 74043
 Exp. 06-30-15
 CIVIL
 STATE OF CALIFORNIA

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Curve Data				
No.	R	Δ	T	L
C4	45.00'	87.16'	42.82	68.45'



65% SUBMITTAL NOT FOR CONSTRUCTION



EKWILL STREET LAYOUT

SCALE: 1" = 20' E-L-2

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS	PROJECT ENGINEER	Adom Chose
	CALCULATED-DESIGNED BY	Adom Chose
REVISIONS	REVISOR	DATE
	REVISION	DATE

LAST REVISION DATE PLOTTED = Aug 08, 2013 00:00:00 TIME PLOTTED = 4:41 pm

DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	LOCAL	N/A	9	87

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

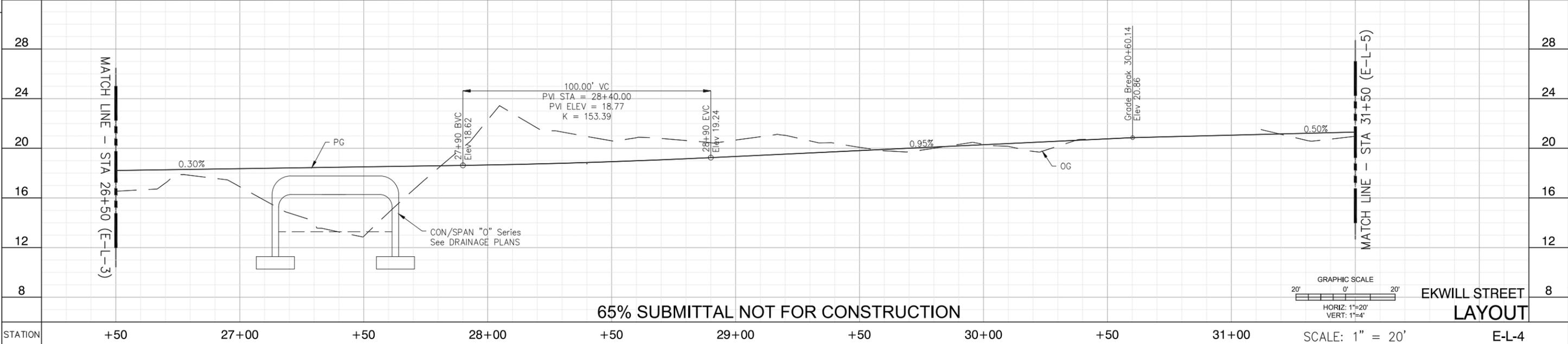
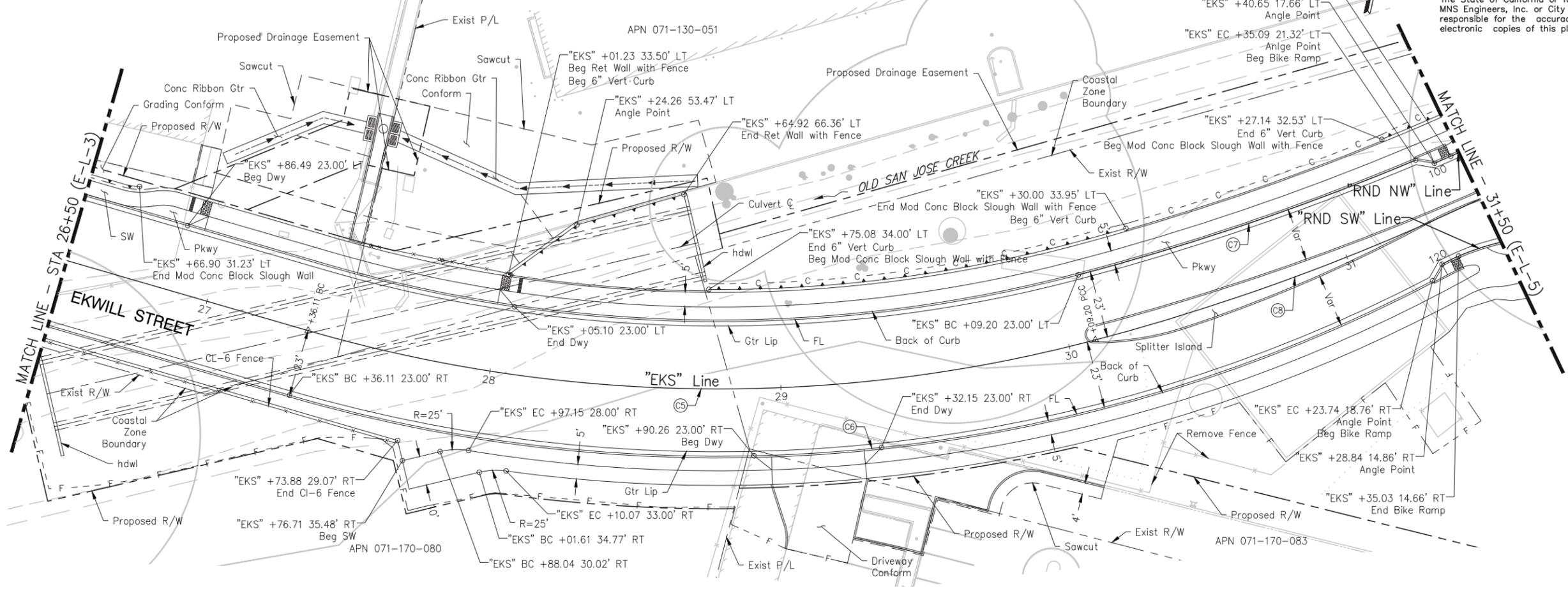
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Curve Data				
No.	R	Δ	T	L
C5	506.00'	30.92	139.96	273.10'
C6	529.00'	43.69	212.08	403.41'
C7	883.00'	7.93	61.19	122.19'
C8	758.94'	10.83	71.93	143.43'



REVISOR: _____ DATE: _____

DESIGNER: Adam Chase

CHECKED BY: _____

PROJECT ENGINEER: _____

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

LAST REVISION DATE PLOTTED = Aug 08, 2013
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DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
05	SB	LOCAL	N/A	10	87

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

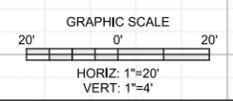
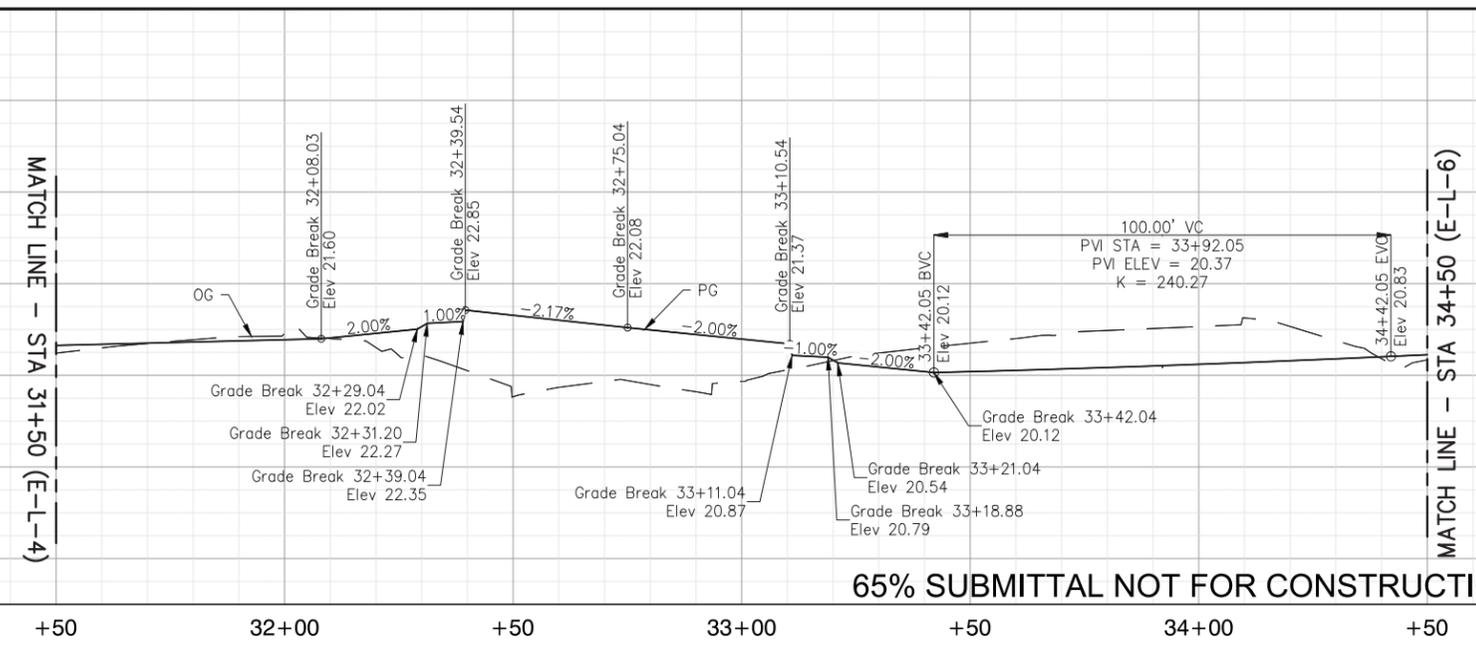
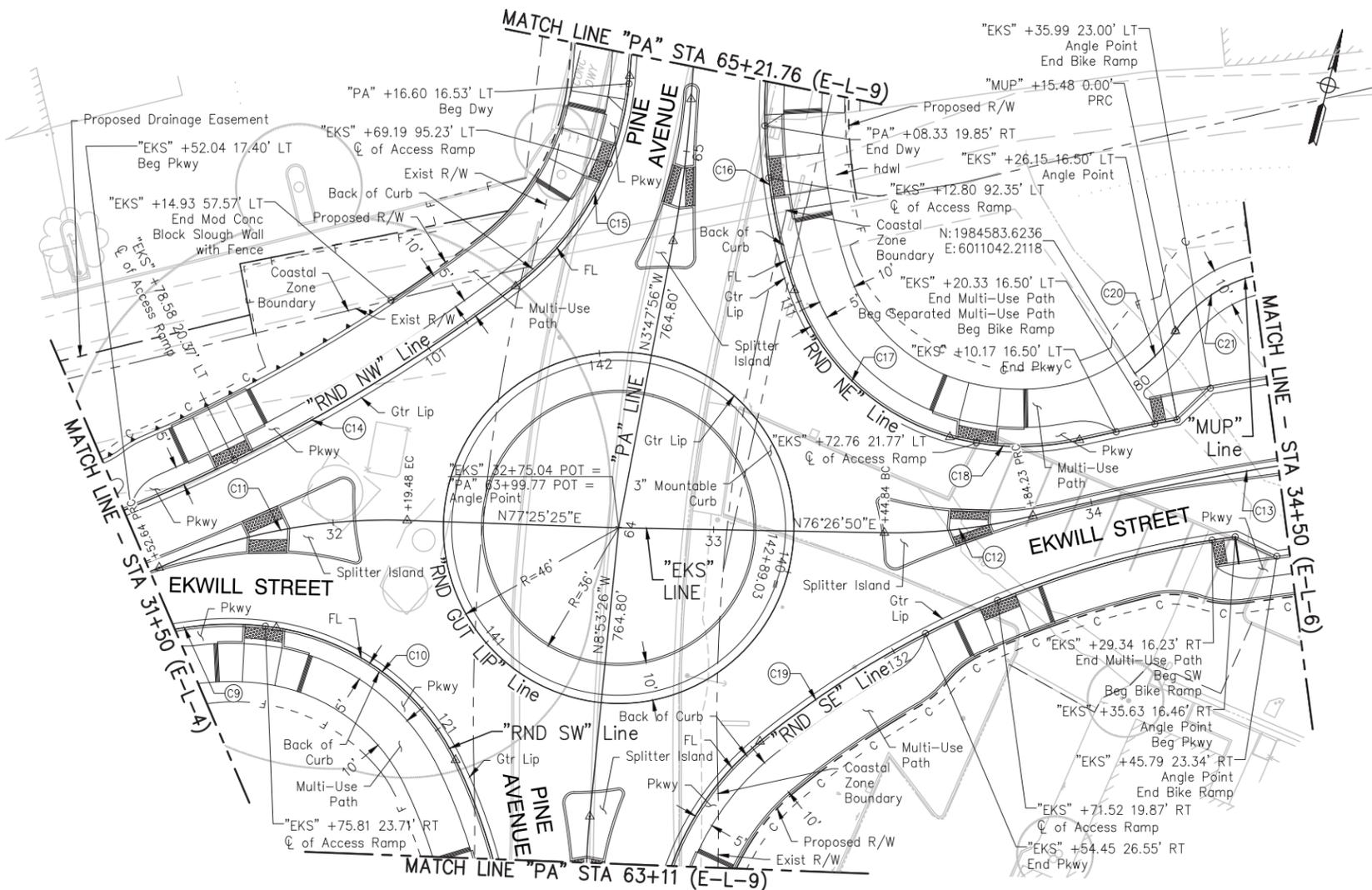
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Curve Data				
No.	R	Δ	T	L
C9	80.00'	34.73	25.02	48.50'
C10	63.00'	55.29	33.00	60.80'
C11	150.00'	25.53	33.99	66.84'
C12	150.00'	15.04	19.81	39.39'
C13	747.00'	8.81	57.52	114.81'
C14	450.00'	16.29	64.42	127.97'
C15	76.50'	48.41	34.39	64.64'
C16	145.00'	26.57	34.23	67.24'
C17	60.00'	55.75	31.73	58.38'
C18	70.00'	28.14	17.55	34.38'
C19	250.00'	31.56	70.66	137.73'
C20	45.00'	19.71	7.82	15.48'
C21	35.00'	49.00	15.95	29.93'



EKWIL STREET LAYOUT

SCALE: 1" = 20' E-L-5

PROJECT ENGINEER	ADAM CHASE
DESIGNED BY	ADAM CHASE
CHECKED BY	---
REVISOR	---
DATE	---

DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	LOCAL	N/A	11	87

REGISTERED CIVIL ENGINEER _____ DATE _____

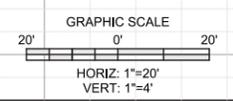
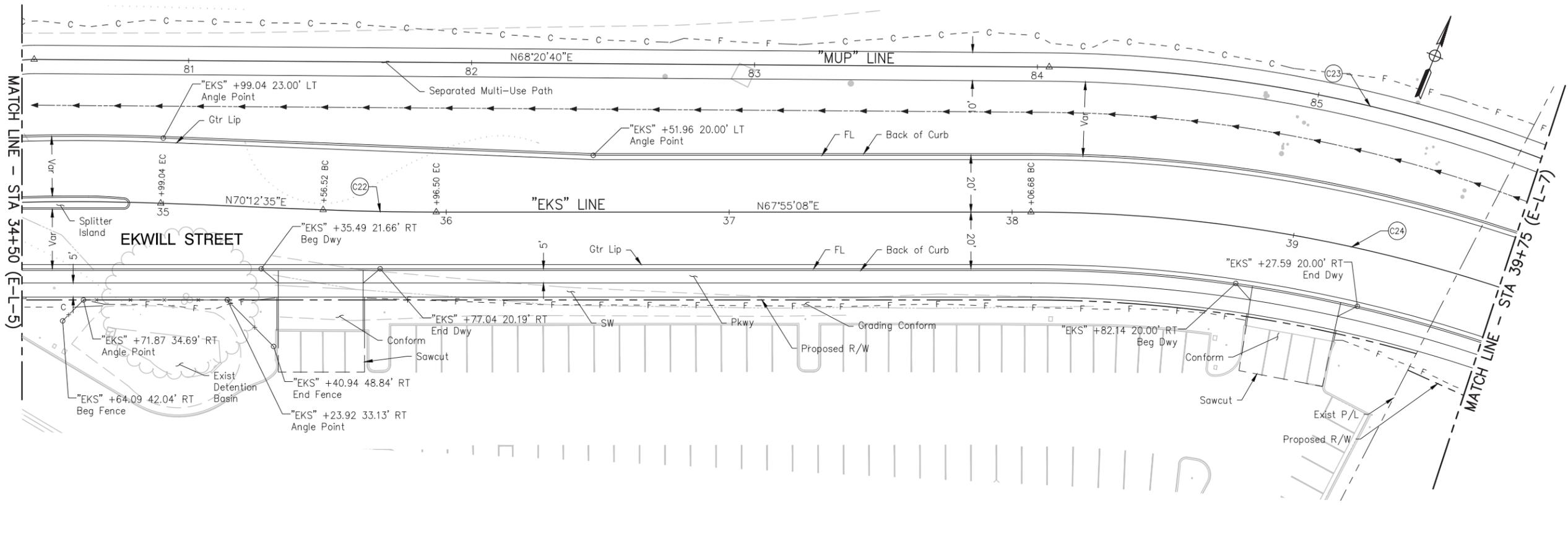
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Curve Data				
No.	R	Δ	T	L
C22	1000.00'	2.29	19.99	39.98'
C23	505.00'	23.93	107.03	210.94'
C24	530.00'	24.36	114.39	225.31'



65% SUBMITTAL NOT FOR CONSTRUCTION

EKWILL STREET LAYOUT

SCALE: 1" = 20' E-L-6

REVISOR: _____ DATE: _____

DESIGNED BY: _____ CHECKED BY: _____

PROJECT ENGINEER: _____

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	LOCAL	N/A	12	87

REGISTERED CIVIL ENGINEER _____ DATE _____

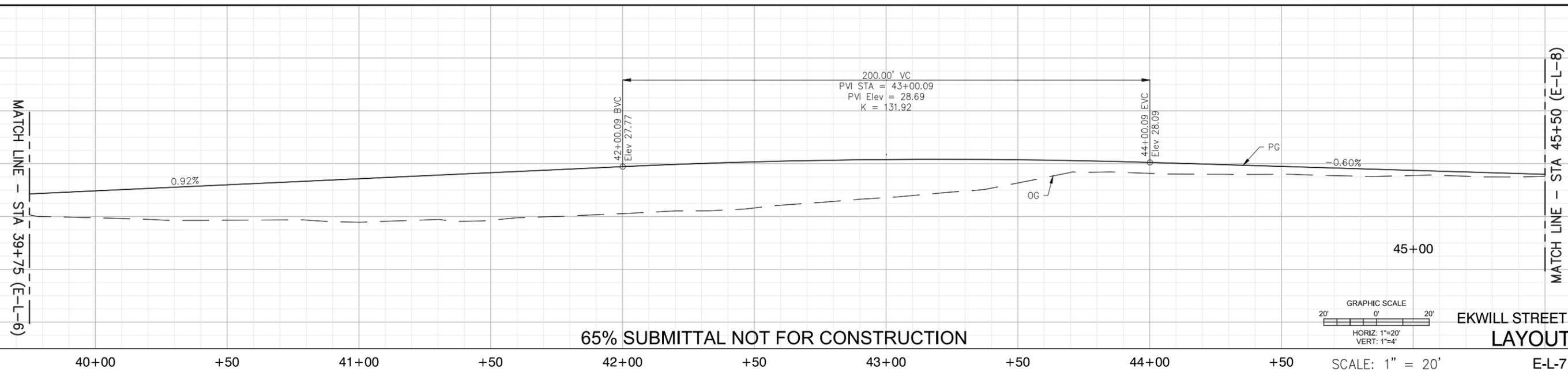
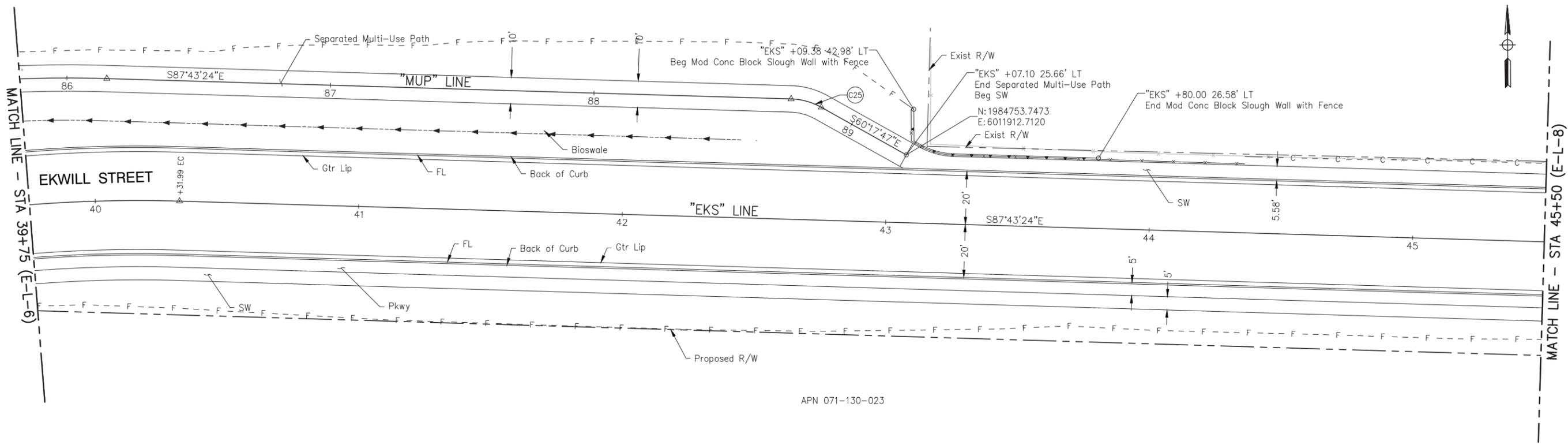
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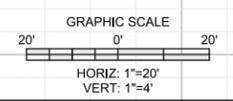
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EKWILL STREET LAYOUT

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS	PROJECT ENGINEER	CALCULATED/DESIGNED BY	ADAM CHASE	REVISOR	DATE
	---	CHECKED BY	---	---	---

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	LOCAL	N/A	13	87

REGISTERED CIVIL ENGINEER _____ DATE _____

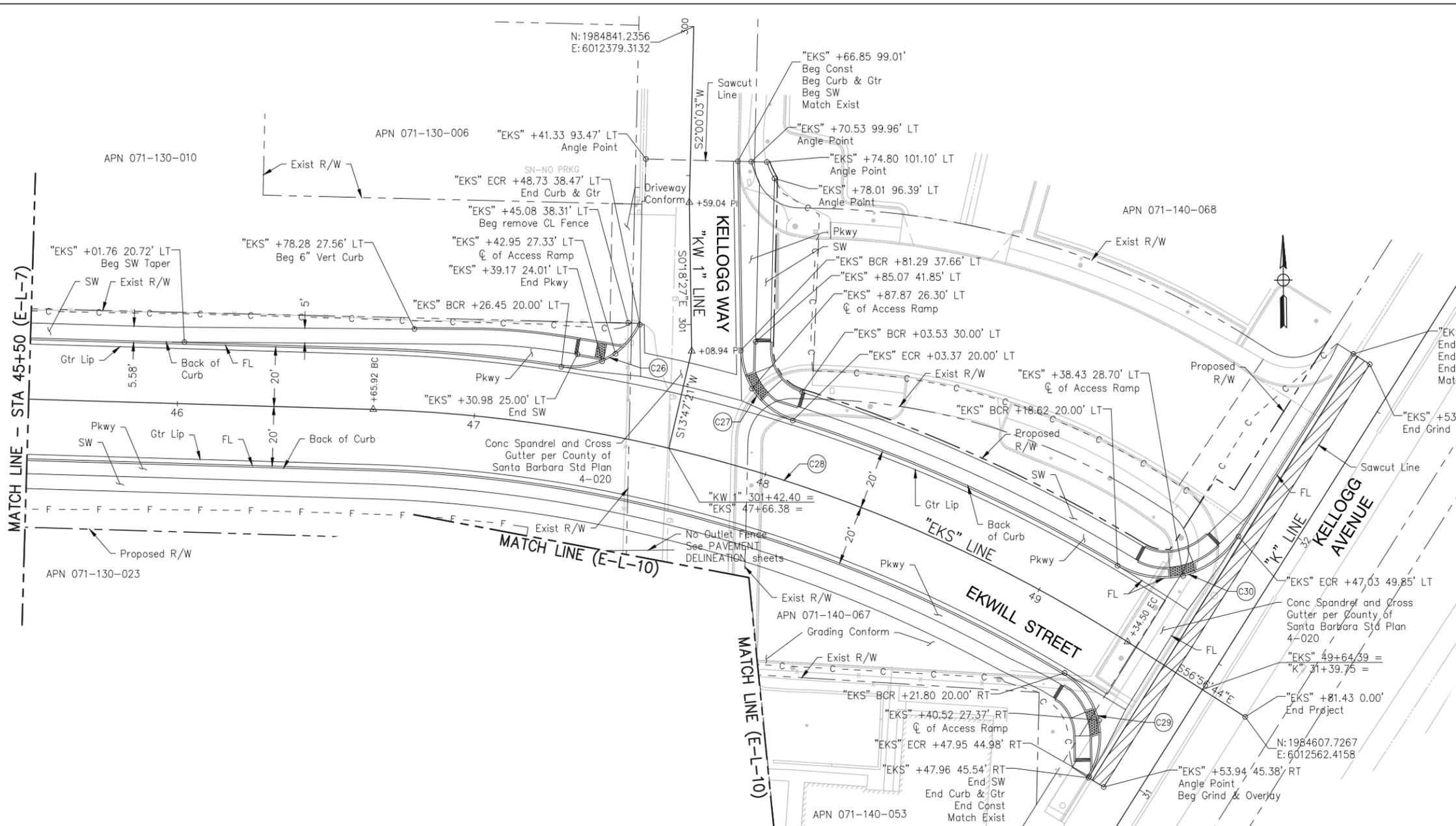
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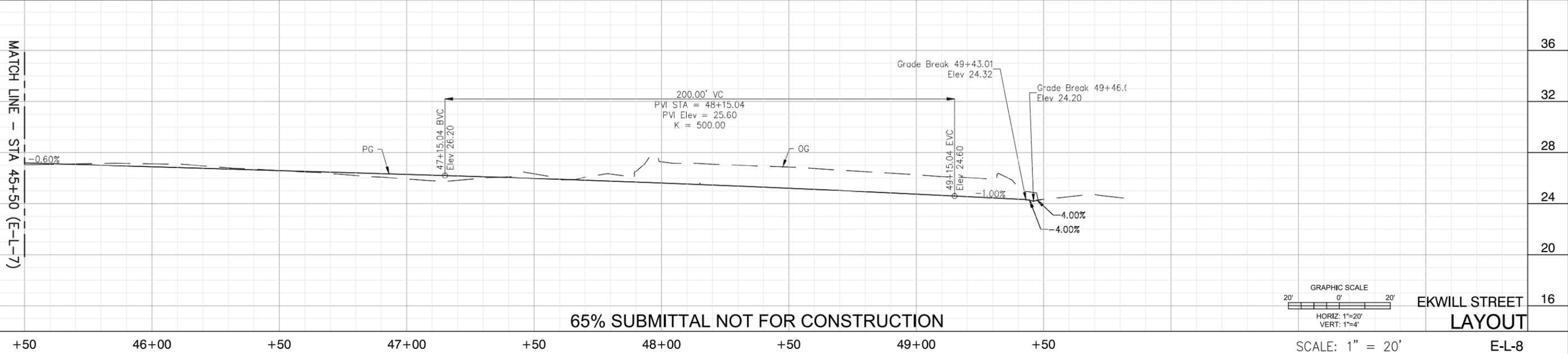
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Curve Data				
No.	R	Δ	T	L
C28	500.00'	30.78	137.62	268.59'



GRAPHIC SCALE
 20' 0' 20'

HORIZ: 1"=20'
 VERT: 1"=4'

EA 000000

REVISOR: _____ DATE: _____

DESIGNED BY: _____ CHECKED BY: _____

PROJECT ENGINEER: _____

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

65% SUBMITTAL NOT FOR CONSTRUCTION

LAST REVISION DATE PLOTTED = Aug 08, 2013
 00-00-00 TIME PLOTTED = 4:41 pm

REVISIONS: REVISED BY, DATE REVISED, CALCULATED-DESIGNED BY, CHECKED BY, PROJECT ENGINEER, CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

Curve Data				
No.	R	Δ	T	L
C37	150.00'	27.73	37.02	72.60'
C38	300.00'	19.62	51.88	102.75'
C39	200.00'	8.50	14.86	29.67'
C40	318.50'	11.47	31.98	63.76'

Curve Data				
No.	R	Δ	T	L
C31	413.50'	4.14	14.95	29.88'
C32	413.50'	1.39	5.01	10.02'
C33	413.50'	1.94	7.00	14.00'
C34	420.00'	4.30	15.76	31.50'
C35	278.14'	3.30	8.00	16.00'
C36	500.00'	3.41	14.89	29.78'

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	Local	N/A	14	87

REGISTERED CIVIL ENGINEER DATE

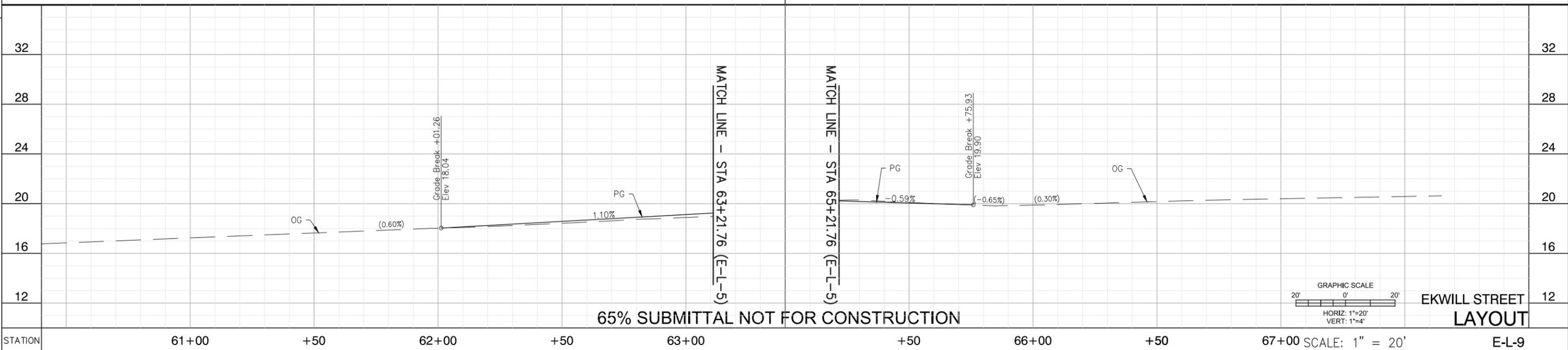
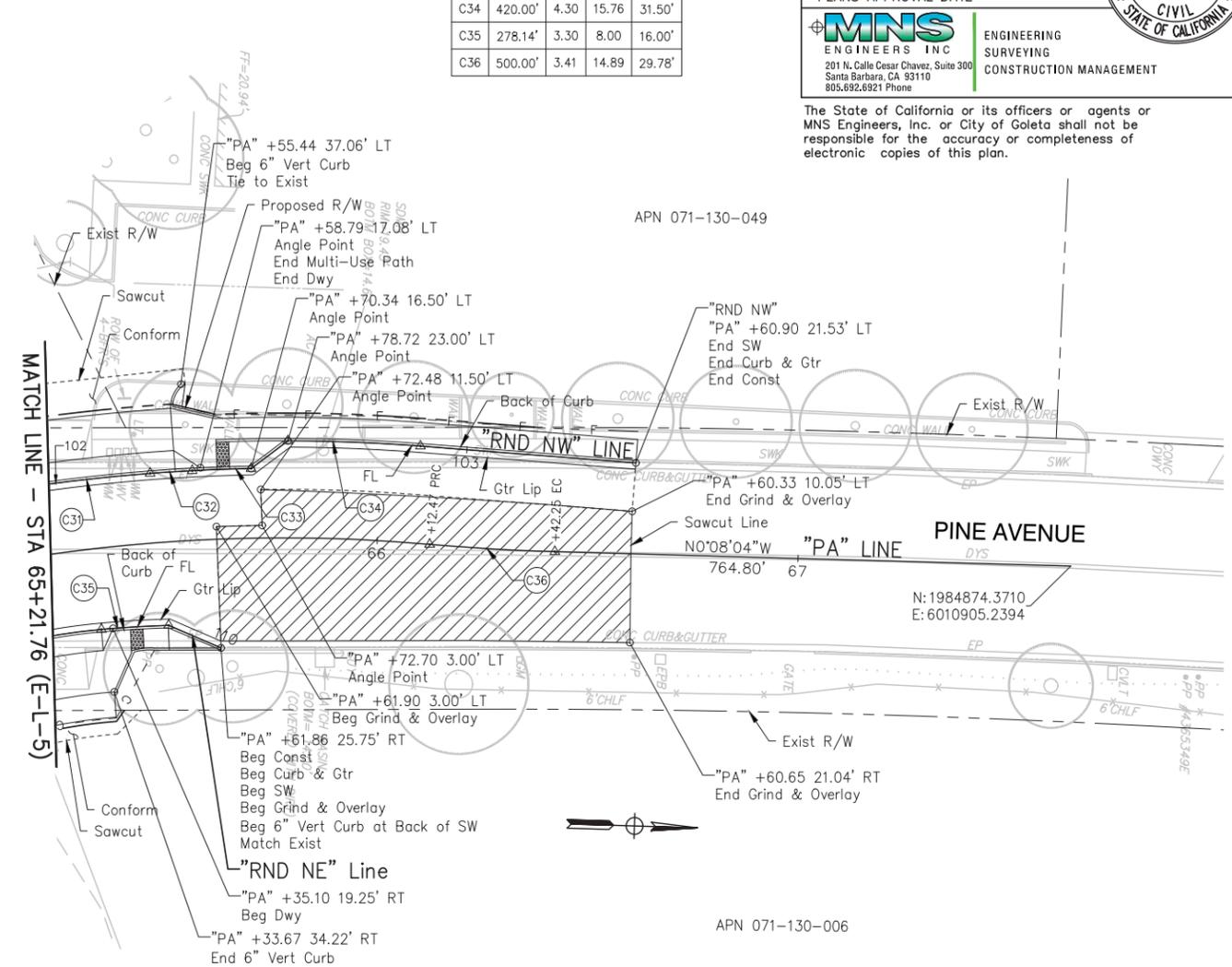
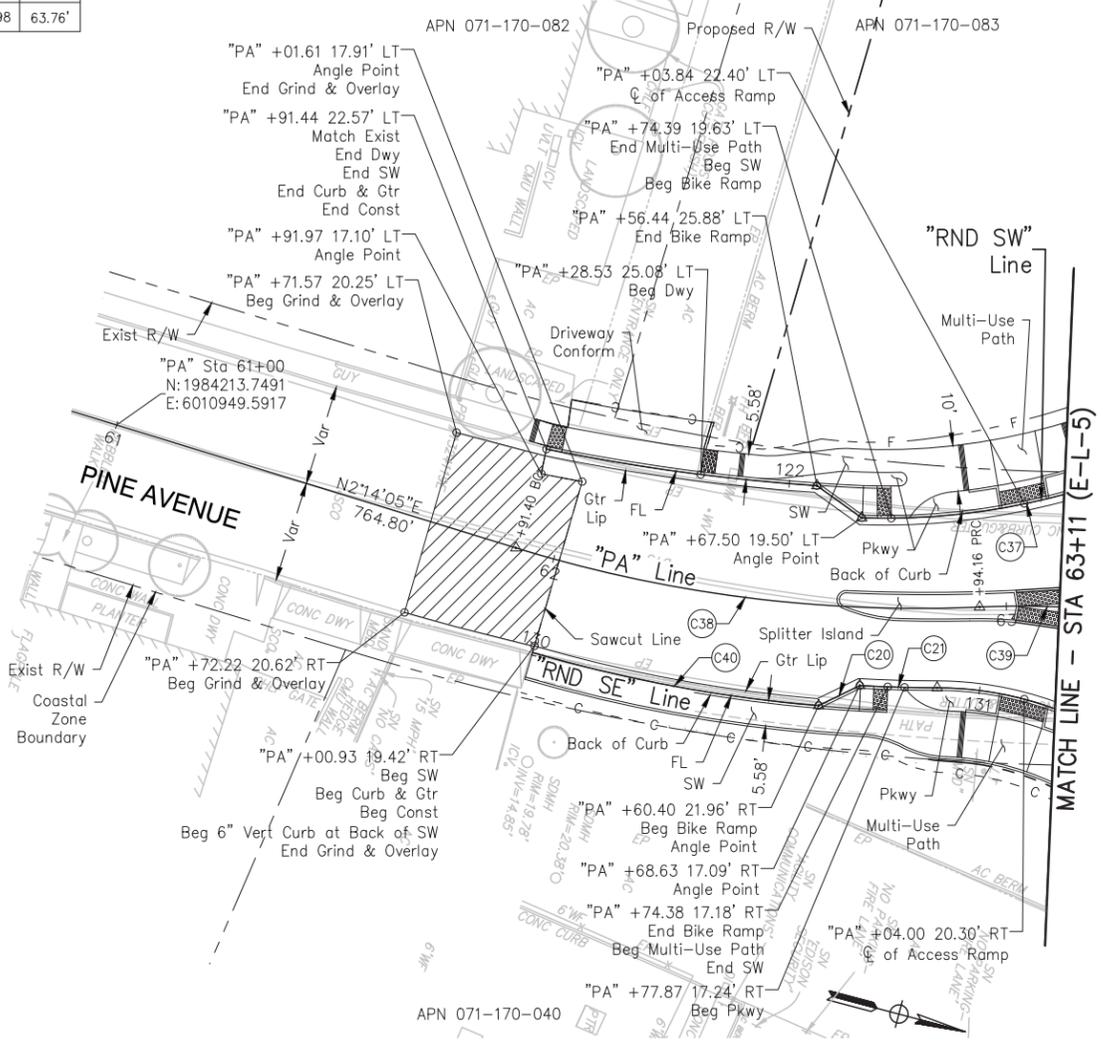
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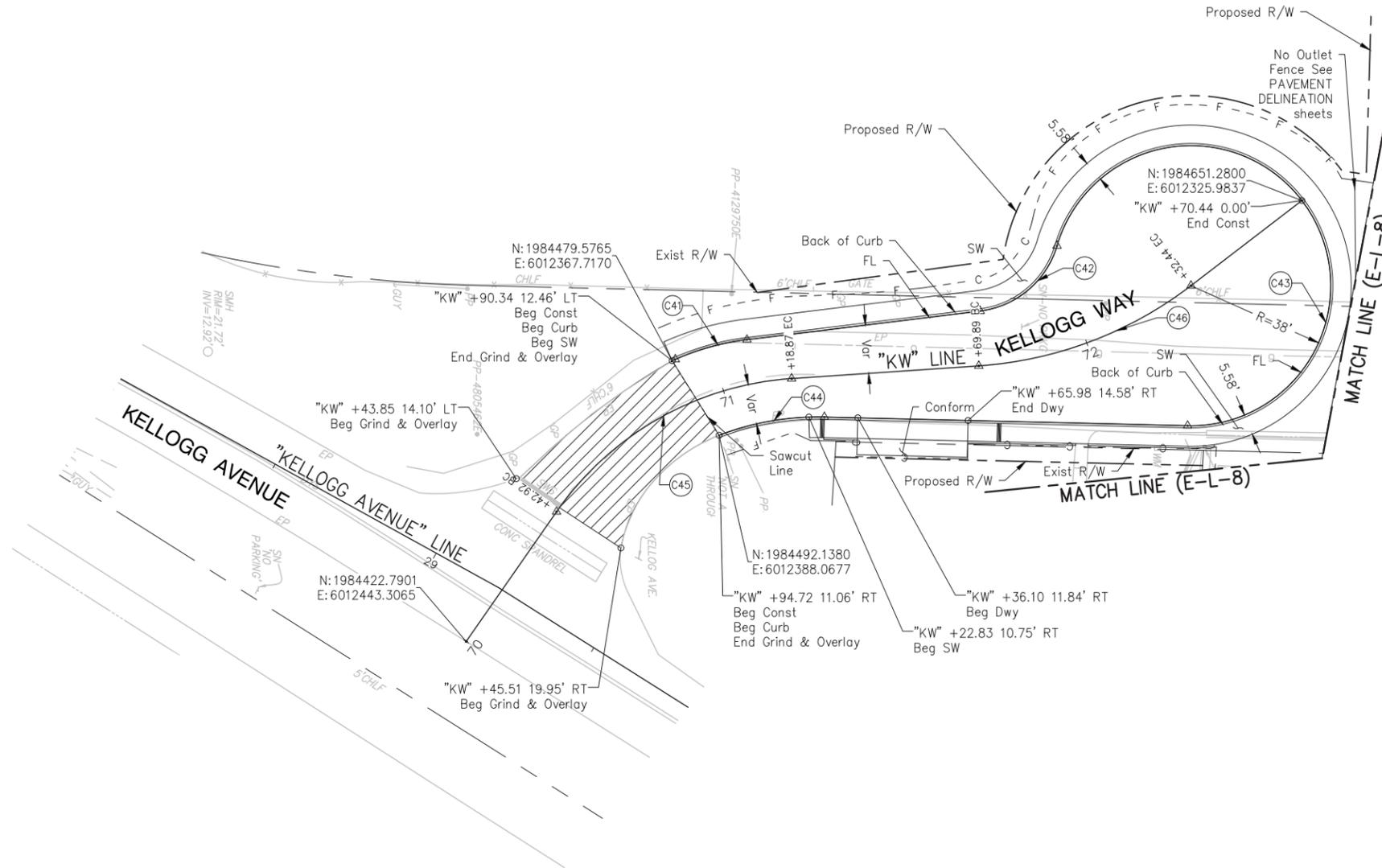


65% SUBMITTAL NOT FOR CONSTRUCTION

EKWILL STREET LAYOUT

LAST REVISION DATE PLOTTED = Aug 08, 2013
 00-00-00 TIME PLOTTED = 4:41 pm

Curve Data				
No.	R	Δ	T	L
C41	70.00'	16.60	10.21	20.29'
C42	25.00'	66.46	16.38	29.00'
C43	38.00'	254.82	49.68	169.01'
C44	75.00'	22.35	14.82	29.26'
C45	85.00'	51.19	40.72	75.95'
C46	106.80'	33.56	32.20	62.56'



Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
05	SB	Local	N/A	15	87

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

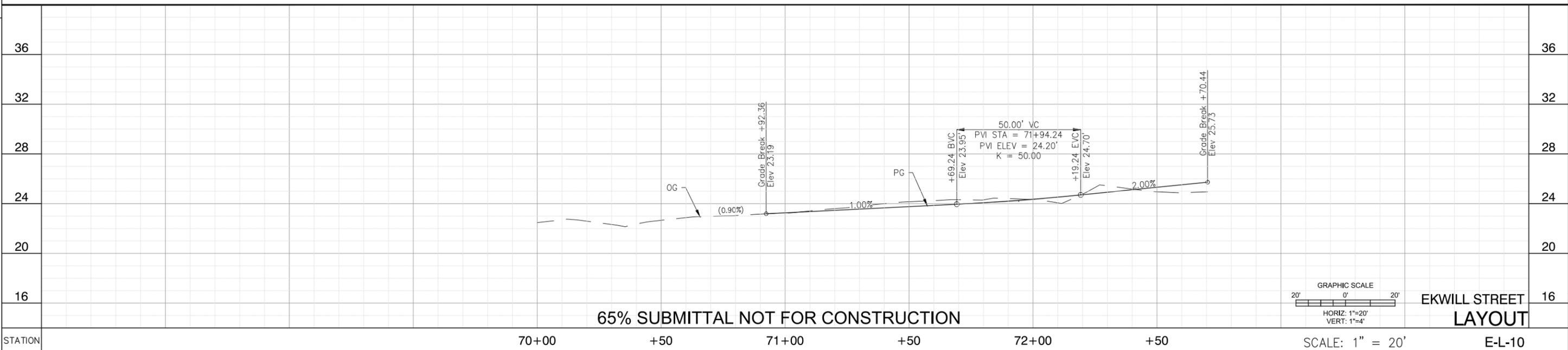
MNS ENGINEERS INC
 201 N. Calle Cesar Chavez, Suite 300
 Santa Barbara, CA 93110
 805.892.6921 Phone

ENGINEERING SURVEYING
 CONSTRUCTION MANAGEMENT

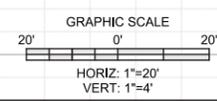
REGISTERED PROFESSIONAL ENGINEER
 ADAM S. CHASE
 No. 74043
 Exp. 06-30-15
 CIVIL
 STATE OF CALIFORNIA

The State of California or its officers or agents or MNS Engineers, Inc. or City of Goleta shall not be responsible for the accuracy or completeness of electronic copies of this plan.

REVISOR: ADAM CHASE
 PROJECT ENGINEER: ADAM CHASE
 CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS



65% SUBMITTAL NOT FOR CONSTRUCTION



EKWILL STREET LAYOUT

SCALE: 1" = 20' E-L-10

ATTACHMENT C
Fowler Road

**CITY OF GOLETA
PUBLIC WORKS DEPARTMENT**

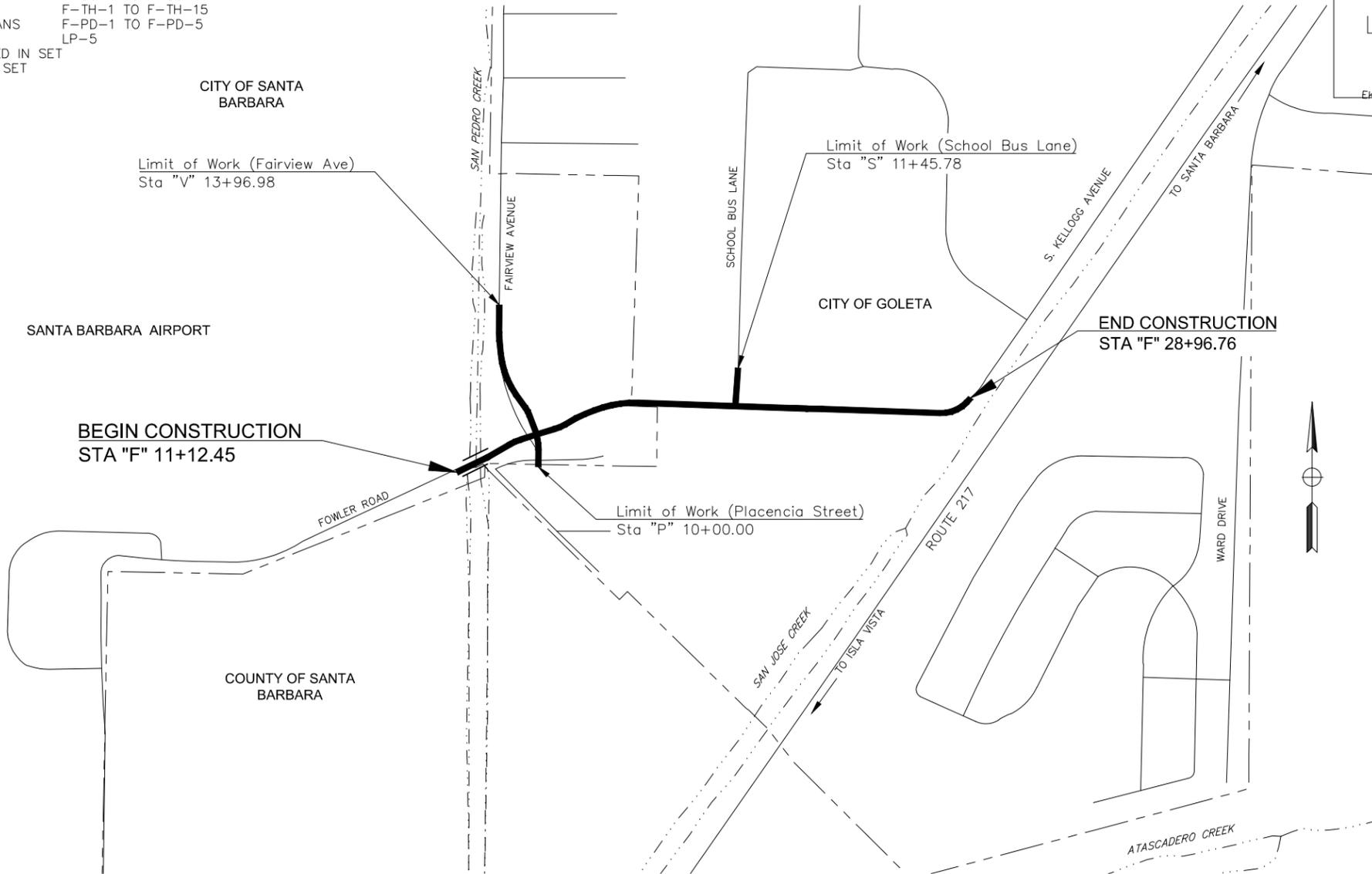
**PROJECT PLANS FOR CONSTRUCTION ON
COUNTY STREET AND CITY STREET
FOWLER ROAD EXTENSION
IN SANTA BARBARA AND GOLETA**

TO BE SUPPLEMENTED BY CALTRANS STANDARD PLANS DATED 2010
AND COUNTY OF SANTA BARBARA STANDARD DETAILS DATED SEPTEMBER 2011



INDEX OF PLANS

- | | | |
|-------|---|--------------------|
| 1 | TITLE SHEET - FOWLER ROAD EXTENSION PROJECT | F-T-1 |
| 2-4 | TYPICAL CROSS SECTIONS | F-X-1 TO F-X-3 |
| 5 | KEY MAP AND LINE INDEX | F-K-1 |
| 6 | LAYOUT NOTES AND LEGENDS | F-L-1 |
| 7-11 | LAYOUTS | F-L-2 TO F-L-6 |
| 12-13 | SUPPLEMENTAL PROFILES | F-P-1 TO F-P-2 |
| 14-17 | CONSTRUCTION DETAILS | F-CD-1 TO F-CD-4 |
| 18-25 | WATER POLLUTION CONTROL | F-WPC-1 TO F-WPC-8 |
| 26-30 | DRAINAGE PLANS | F-D-1 TO F-D-5 |
| 31-35 | UTILITY PLANS | F-U-1 TO F-U-5 |
| 36-51 | STAGE CONSTRUCTION | F-SC-1 TO F-SC-16 |
| 52-66 | TRAFFIC HANDLING PLANS | F-TH-1 TO F-TH-15 |
| 67-71 | PAVEMENT DELINEATION AND SIGN PLANS | F-PD-1 TO F-PD-5 |
| XX | PRELIMINARY LANDSCAPE PLAN | LP-5 |
| XX-XX | TRAFFIC SIGNAL PLAN - NOT INCLUDED IN SET | |
| XX-XX | STREET LIGHTING - NOT INCLUDED IN SET | |



APPROVED AS TO IMPACT ON STATE FACILITIES AND CONFORMANCE WITH APPLICABLE STATE STANDARDS AND PRACTICES AND THAT TECHNICAL OVERSIGHT WAS PERFORMED.

DATE SIGNED

LICENSE Exp DATE

REGISTRATION No.

CALTRANS DESIGN OVERSIGHT APPROVAL

CONSULTANT DESIGN ENGINEER
CRAIG DRAKE

PROJECT ENGINEER _____ DATE _____
REGISTERED CIVIL ENGINEER

PLANS APPROVAL DATE _____
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.



CITY OF GOLETA, DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN & ASSOCIATES
11060 WHITE ROCK ROAD, SUITE 200
RANCHO CORDOVA, CA 95670

CONTRACT No. 05-4611U4
PROJECT ID 0500000548

NO SCALE
65% SUBMITTAL NOT FOR CONSTRUCTION

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	Santa Barbara				

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

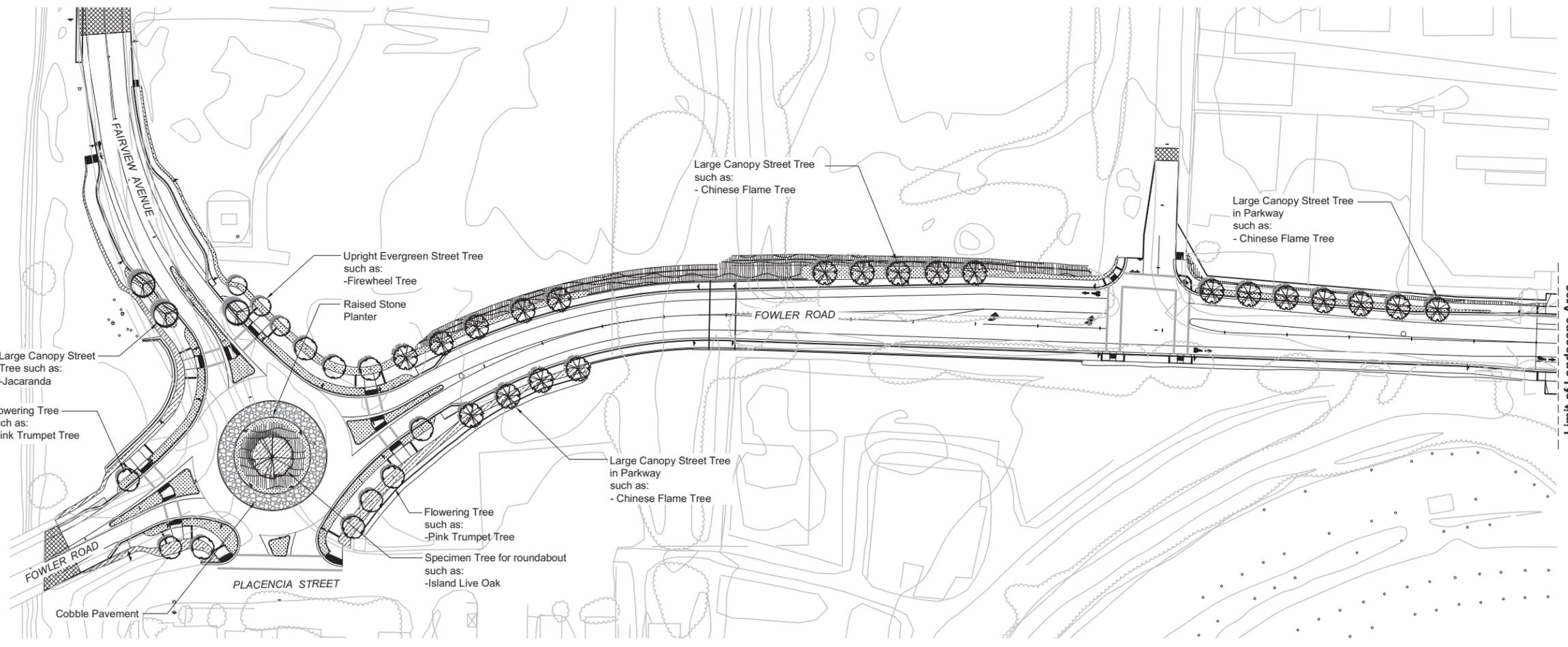


dh drake haglan AND ASSOCIATES
 11060 White Rock Road, Suite 200
 Rancho Cordova, CA 95670

DAVID R. BLACK & ASSOCIATES
 LANDSCAPE ARCHITECTURE
 URBAN DESIGN
 1718 PAMPAS AVENUE
 SANTA BARBARA, CALIFORNIA 93101
 (805) 898-8717
 FAX 805-898-0373
 e-mail: dblack@blackda.com

CONCEPTUAL PLANT LEGEND

- Upright Street Trees**
 Stenocarpus sinuatus / Firewheel Tree
- Large Canopy Street Trees**
 Jacaranda mimosifolia / Jacaranda
 Koelreuteria bipinnata / Chinese Flame Tree
- Flowering Accent Trees**
 Tabebuia impetiginosa / Pink Trumpet Tree
- Specimen Trees for Roundabout**
 Jacaranda mimosifolia / Jacaranda
 Platanus racemosa / California Sycamore
 Quercus tomentella / Island Live Oak
- Perennials / Grasses 18" height**
 Arctostaphylos 'Pacific Mist' / Manzanita 'Pacific Mist'
 Carex divulsa / Berkeley Sedge
 Ceanothus griseus horizontalis 'Yankee Point' / California Lilac
 Helictotrichon sempervirens / Blue Oat Grass
 Lomandra confertifolia 'Little Con' / Small Mat Rush
 Lomandra longifolia 'Breeze' / Lomandra
- Perennials (30 - 18 inches in height)**
 Ceanothus griseus horizontalis 'Yankee Point' / California Lilac
 Festuca californica / California Fescue
 Leymus triticoides 'Canyon Prince' / Canyon Prince Wild Rye
 Muhlenbergia capillans / Hairy Awn Muhly
 Muhlenbergia rigens / Deer Grass
- Shrubs (30 inches - 3 ft in height)**
 Agave attenuata / Agave
 Arctostaphylos 'John Dourley' / Manzanita
 Lomandra 'Gary's Green' / Mat Rush
 Salvia apiana / White Sage
 Salvia clevelandii 'Winifred Gillman' / Cleveland Sage
 Salvia greggii 'Alba' / Autumn Sage
 Salvia greggii 'Furmans Red' / Autumn Sage
- Shrubs (3-6 ft in height)**
 Arctostaphylos densiflora 'Howard McMinn' / Howard McMinn Manzanita
 Arctostaphylos 'Sunset' / Manzanita
 Heteromeles arbutifolia / Toyon
 Rhamnus californica / California Coffee Berry
 Rhus integrifolia / Lemonade Berry



SCALE: 1" = 50'-0"

Preliminary Landscape Plan - Fowler Road Extension
 Ekwil Street and Fowler Road Extensions Project

REVISIONS: 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100
 PROJECT ENGINEER
 CALCULATED - DESIGNED BY
 CHECKED BY
 REVISOR BY
 DATE REVISOR
 CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

BORDER LAST REVISED 8/7/13

RELATIVE BORDER SCALE IS IN INCHES



USERNAME = work station
 DWG FILE = FOWLER Concept_8-27-13.dwg

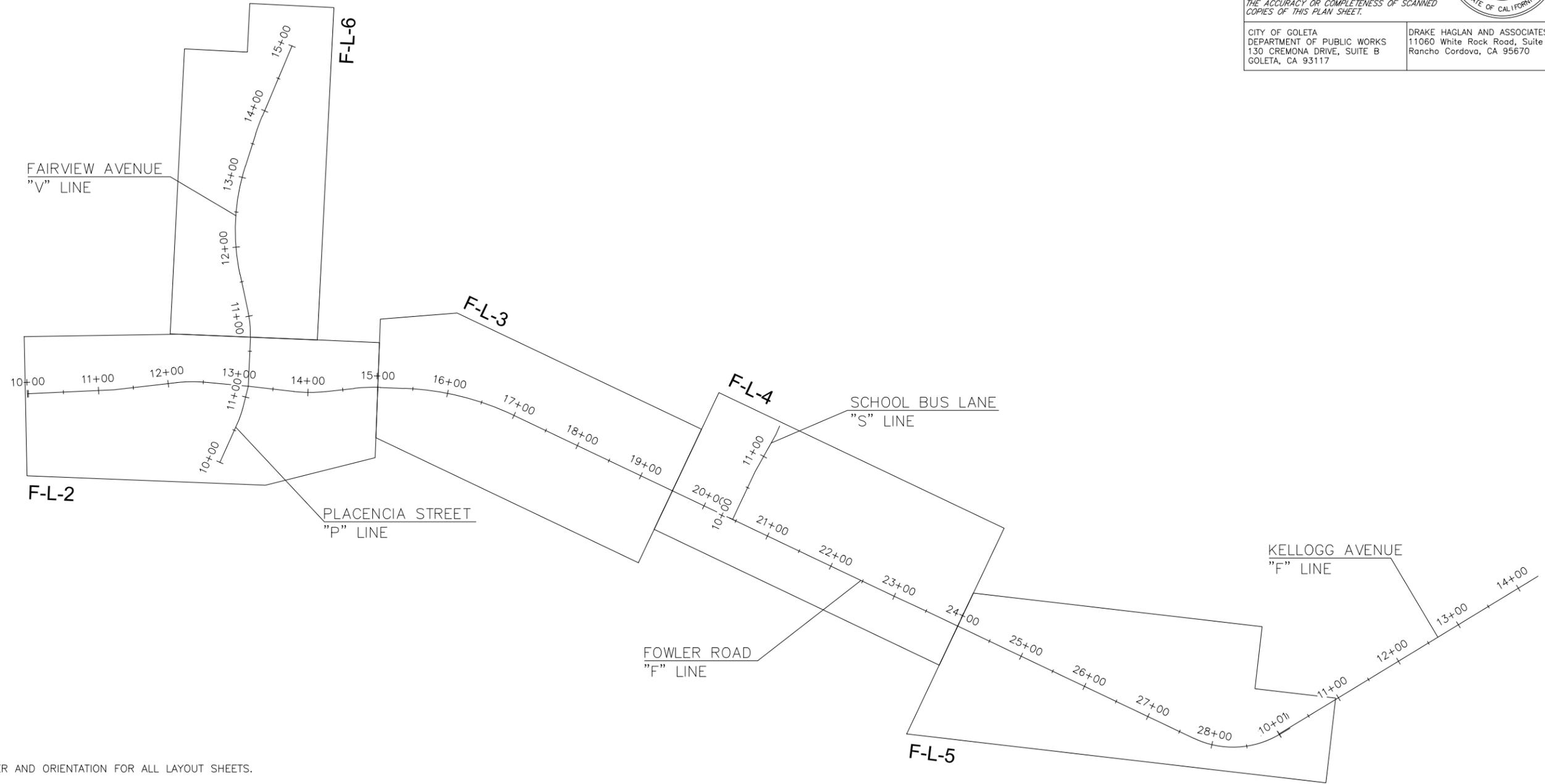
CU 00000

EA 000000

8-28-13 **LP-5**

LAST REVISION 00-00-00
 Aug 28, 2013
 TIME PLOTTED = 10:34 am

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			5	71
REGISTERED CIVIL ENGINEER			DATE		
PLANS APPROVAL DATE					
<small>THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
<small>CITY OF GOLETA DEPARTMENT OF PUBLIC WORKS 130 CREMONA DRIVE, SUITE B GOLETA, CA 93117</small>			<small>DRAKE HAGLAN AND ASSOCIATES 11060 White Rock Road, Suite 200 Rancho Cordova, CA 95670</small>		



CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR
	DAVE MELIS	CHECKED BY	
			DATE REVISED

65% SUBMITTAL NOT FOR CONSTRUCTION

KEY MAP AND LINE INDEX
NO SCALE
F-K-1

BORDER LAST REVISED 7/2/2010

USERNAME => acastlekeane
DGN FILE => FOWLER_EF-K-1.dwg

RELATIVE BORDER SCALE
IS IN INCHES



UNIT 0000

PROJECT NUMBER & PHASE

4611U0

DATE PLOTTED => Oct 04, 2013
TIME PLOTTED => 3:09 pm
LAST REVISION
00-00-00

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			7	71

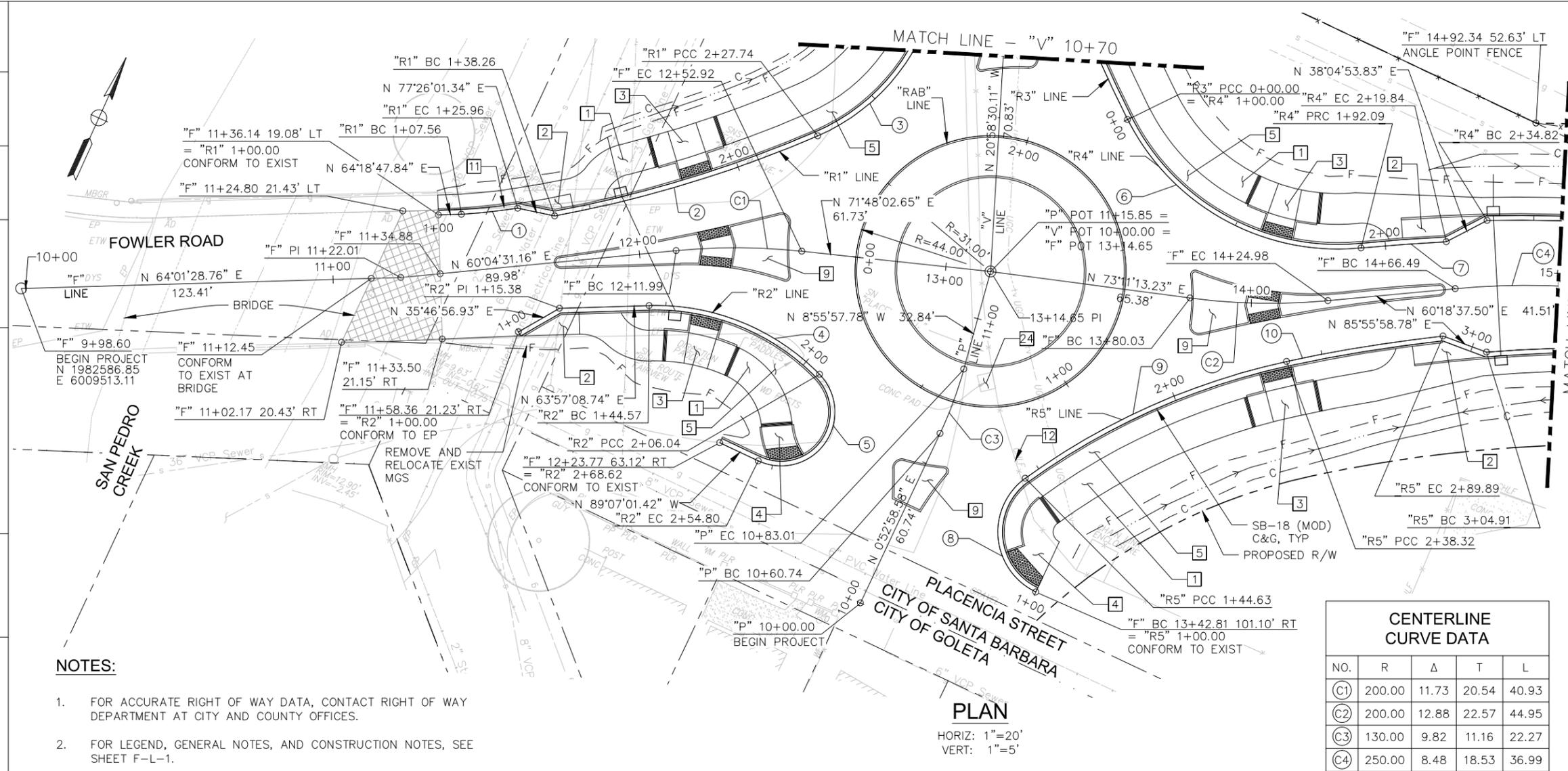
REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670



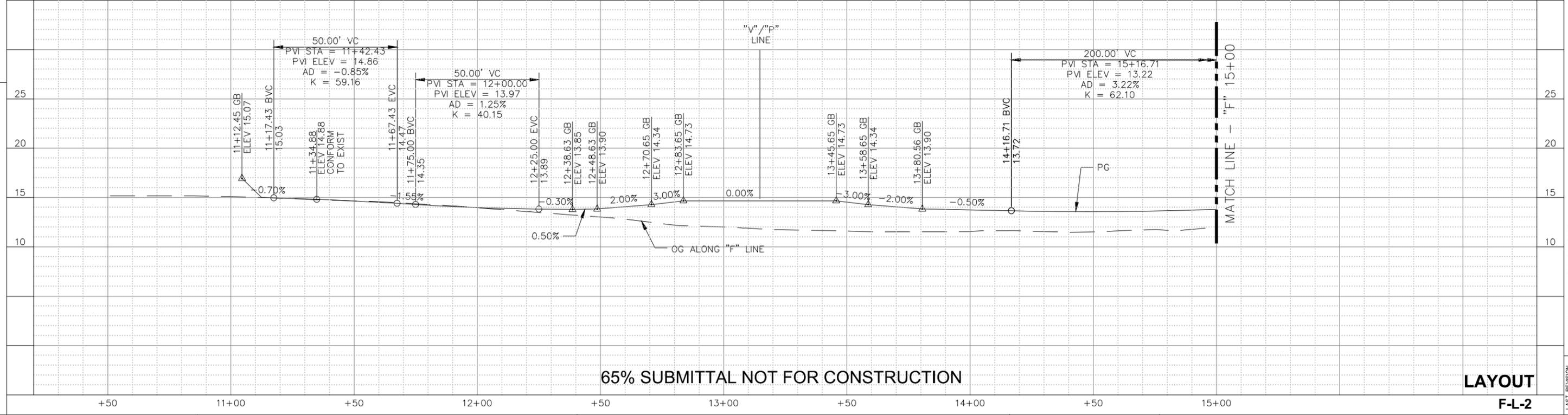
- NOTES:**
- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY DEPARTMENT AT CITY AND COUNTY OFFICES.
 - FOR LEGEND, GENERAL NOTES, AND CONSTRUCTION NOTES, SEE SHEET F-L-1.

CENTERLINE CURVE DATA

NO.	R	Δ	T	L
①	195.50	5.39	9.21	18.40
②	401.50	12.77	44.93	89.48
③	66.50	82.70	58.52	95.98
④	74.50	47.28	32.61	61.47
⑤	17.50	159.66	97.53	48.76
⑥	76.50	68.97	52.55	92.09
⑦	512.00	3.11	13.88	27.75
⑧	21.50	118.94	36.45	44.63
⑨	227.25	23.62	47.52	93.69
⑩	488.00	6.05	25.80	51.56
①	200.00	11.73	20.54	40.93
②	200.00	12.88	22.57	44.95
③	130.00	9.82	11.16	22.27
④	250.00	8.48	18.53	36.99

LIP OF GUTTER CURVE DATA

NO.	R	Δ	T	L
①	195.50	5.39	9.21	18.40
②	401.50	12.77	44.93	89.48
③	66.50	82.70	58.52	95.98
④	74.50	47.28	32.61	61.47
⑤	17.50	159.66	97.53	48.76
⑥	76.50	68.97	52.55	92.09
⑦	512.00	3.11	13.88	27.75
⑧	21.50	118.94	36.45	44.63
⑨	227.25	23.62	47.52	93.69
⑩	488.00	6.05	25.80	51.56



65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
F-L-2

DATE PLOTTED => Oct 04, 2013
TIME PLOTTED => 3:10 pm

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			8	71

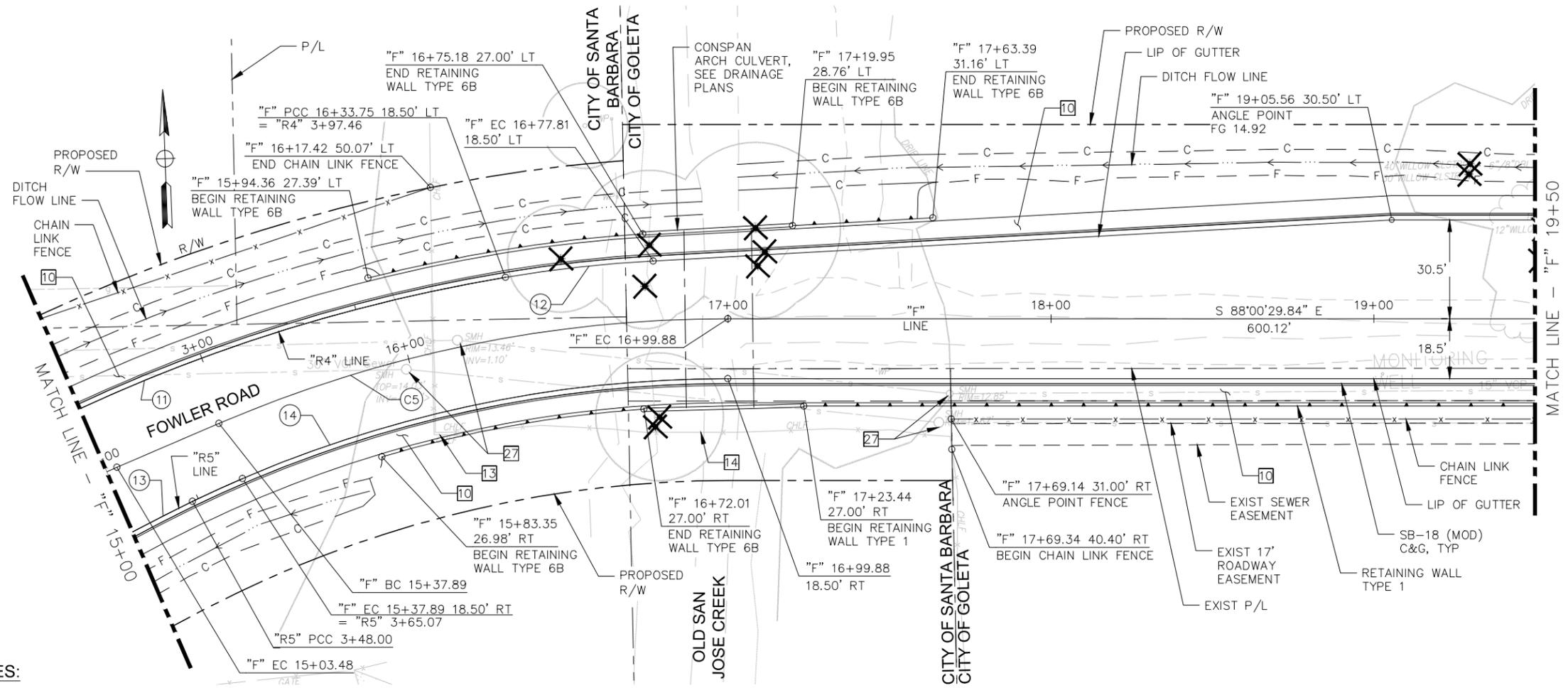
REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670



CENTERLINE CURVE DATA

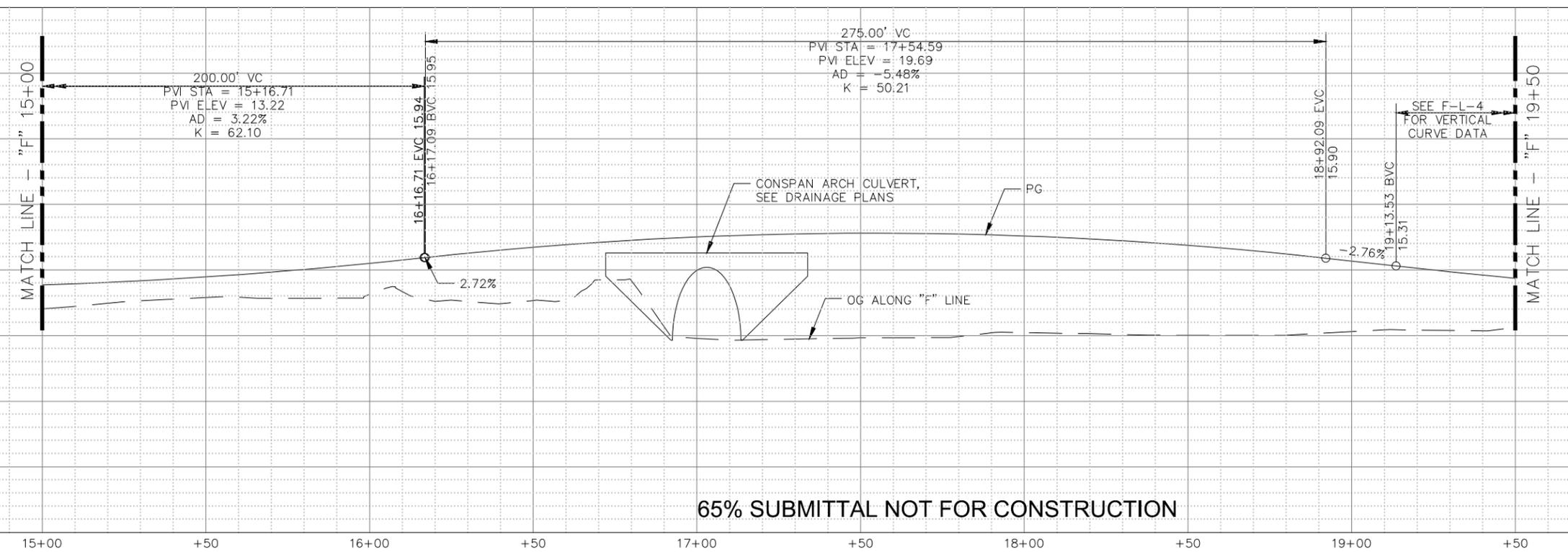
NO.	R	Δ	T	L
(C5)	400.00	23.20	82.12	161.99

LIP OF GUTTER CURVE DATA

NO.	R	Δ	T	L
(11)	518.50	17.97	81.99	162.64
(12)	418.50	6.31	23.07	46.09
(13)	481.50	5.13	21.56	43.10
(14)	381.50	25.77	87.26	171.56

- NOTES:**
- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY DEPARTMENT AT CITY AND COUNTY OFFICES.
 - FOR LEGEND, GENERAL NOTES, AND CONSTRUCTION NOTES, SEE SHEET F-L-1.

PLAN
HORIZ: 1"=20'
VERT: 1"=5'



65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
F-L-3

REVISOR: _____ DATE: _____

DESIGNED BY: _____ CHECKED BY: _____

CONSULTANT FUNCTIONAL SUPERVISOR: DAVE MELIS

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

DATE PLOTTED => Oct 04, 2013
TIME PLOTTED => 3:11 pm

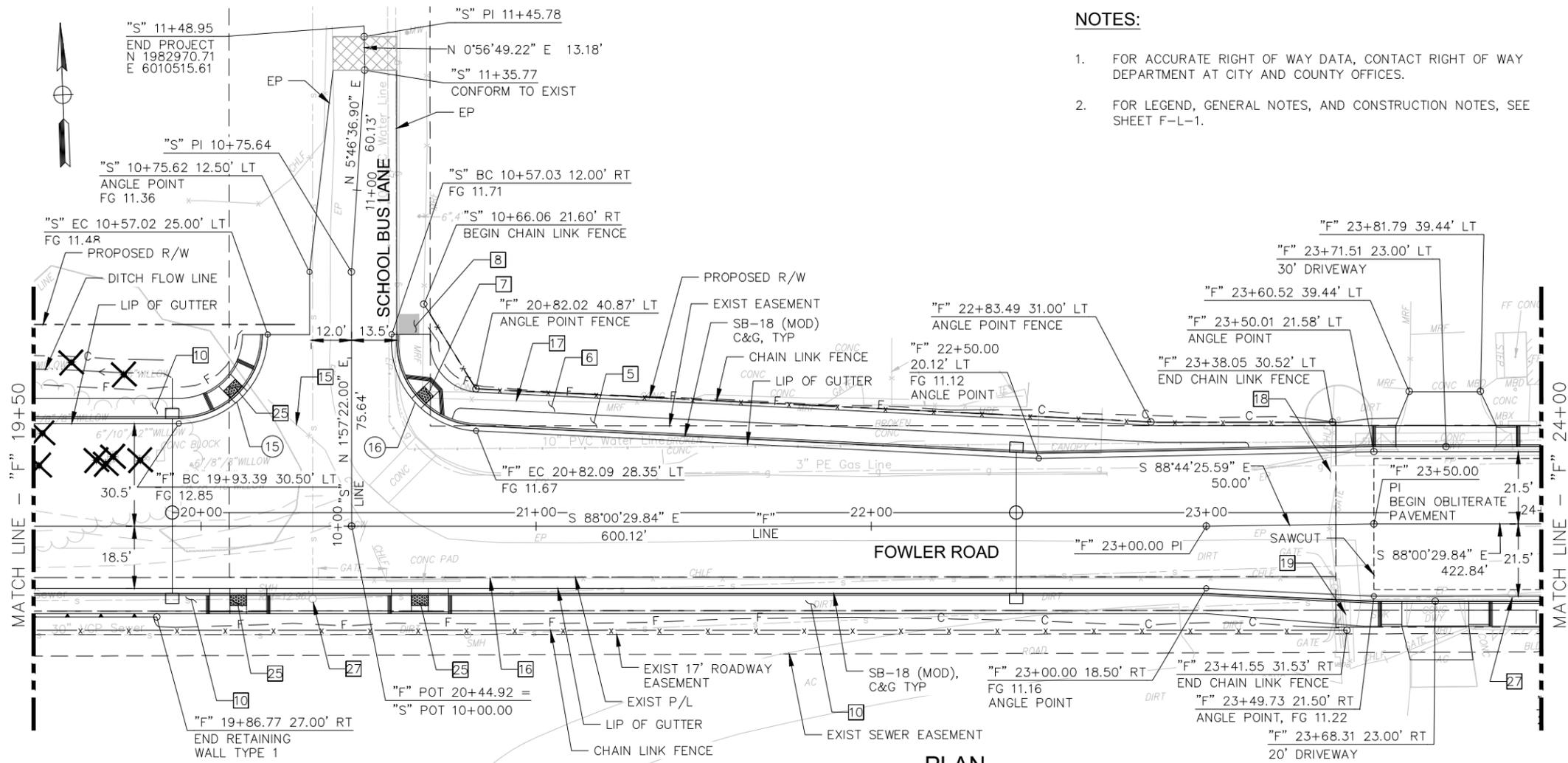
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			9	71

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

CITY OF GOLETA DEPARTMENT OF PUBLIC WORKS 130 CREMONA DRIVE, SUITE B GOLETA, CA 93117	DRAKE HAGLAN AND ASSOCIATES 11060 White Rock Road, Suite 200 Rancho Cordova, CA 95670
--	---

NOTES:

- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY DEPARTMENT AT CITY AND COUNTY OFFICES.
- FOR LEGEND, GENERAL NOTES, AND CONSTRUCTION NOTES, SEE SHEET F-L-1.

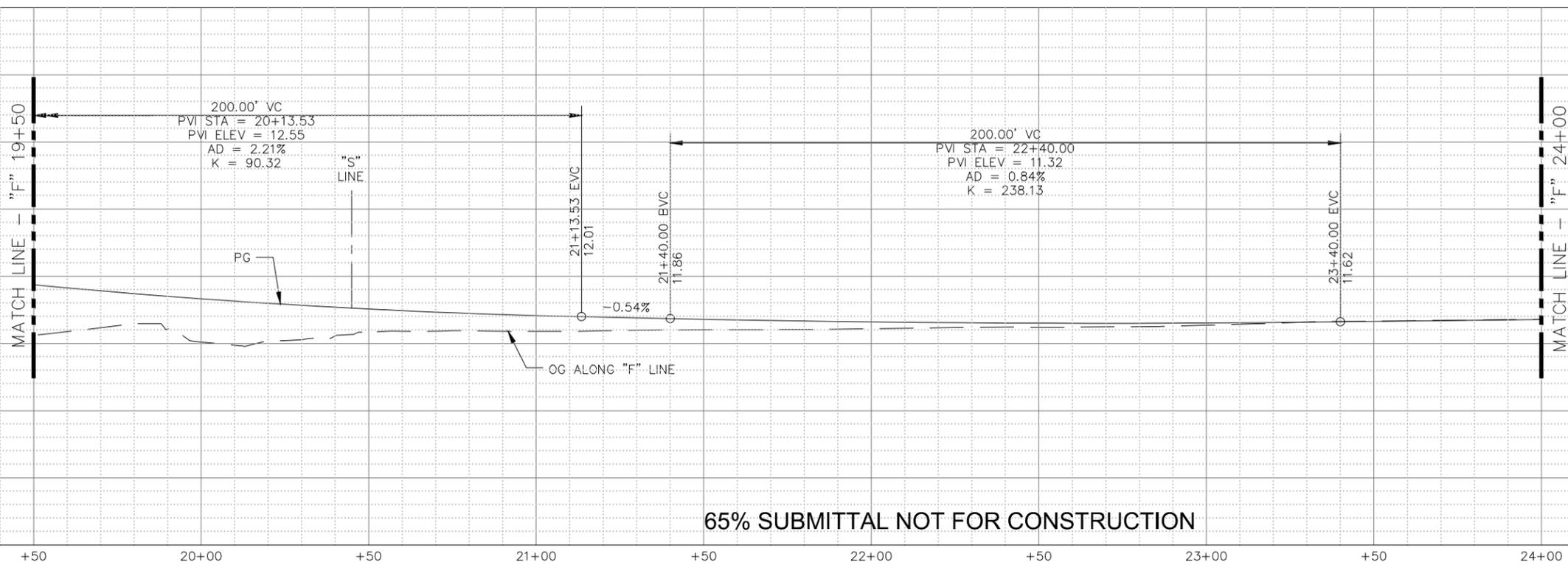


LIP OF GUTTER CURVE DATA

NO.	R	Δ	T	L
15	26.50	90.04	26.52	41.64
16	26.50	87.16	25.22	40.31

PLAN

HORIZ: 1"=20'
VERT: 1"=5'



65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT

F-L-4

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			10	71

REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

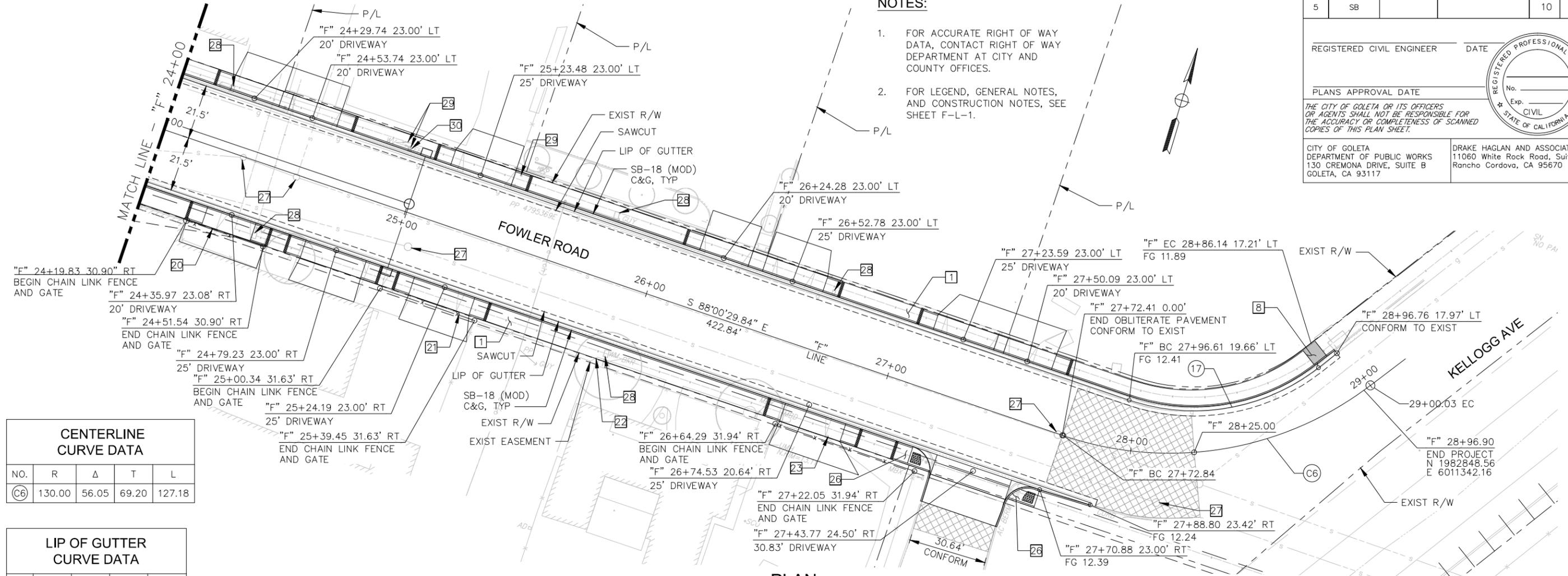
THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670

NOTES:

- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY DEPARTMENT AT CITY AND COUNTY OFFICES.
- FOR LEGEND, GENERAL NOTES, AND CONSTRUCTION NOTES, SEE SHEET F-L-1.



CENTERLINE CURVE DATA

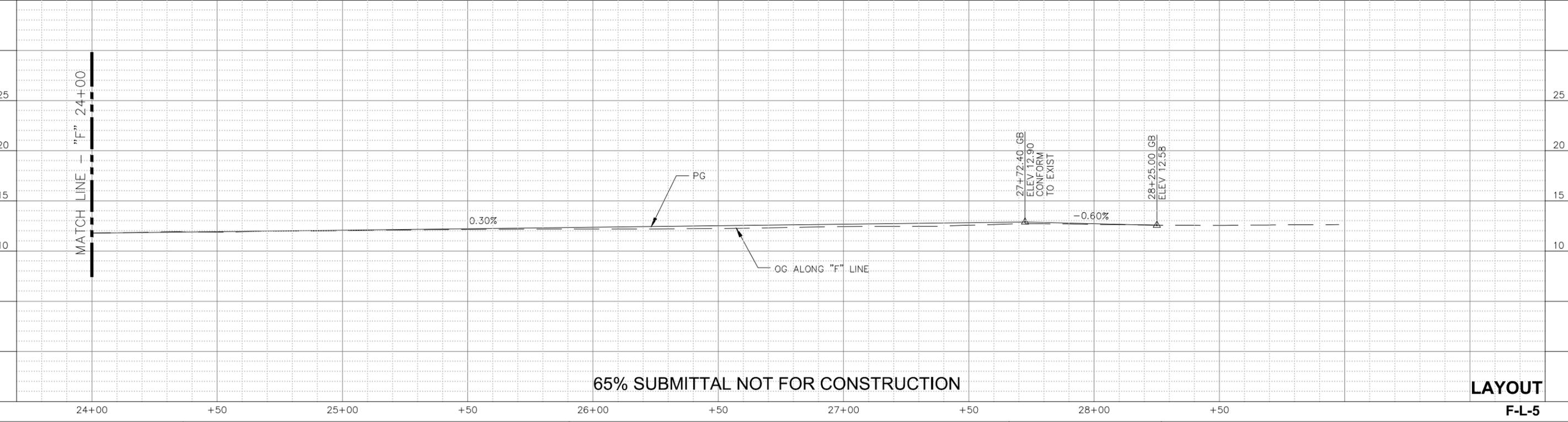
NO.	R	Δ	T	L
(C6)	130.00	56.05	69.20	127.18

LIP OF GUTTER CURVE DATA

NO.	R	Δ	T	L
(L7)	79.08	56.90	42.85	78.54

PLAN

HORIZ: 1"=20'
VERT: 1"=5'



65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
F-L-5

REVISOR: _____ DATE: _____

CHECKED BY: _____

DESIGNED BY: _____

CONSULTANT FUNCTIONAL SUPERVISOR: DAVE MELIS

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

DATE PLOTTED => Oct 04, 2013
TIME PLOTTED => 3:11 pm

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			11	71

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

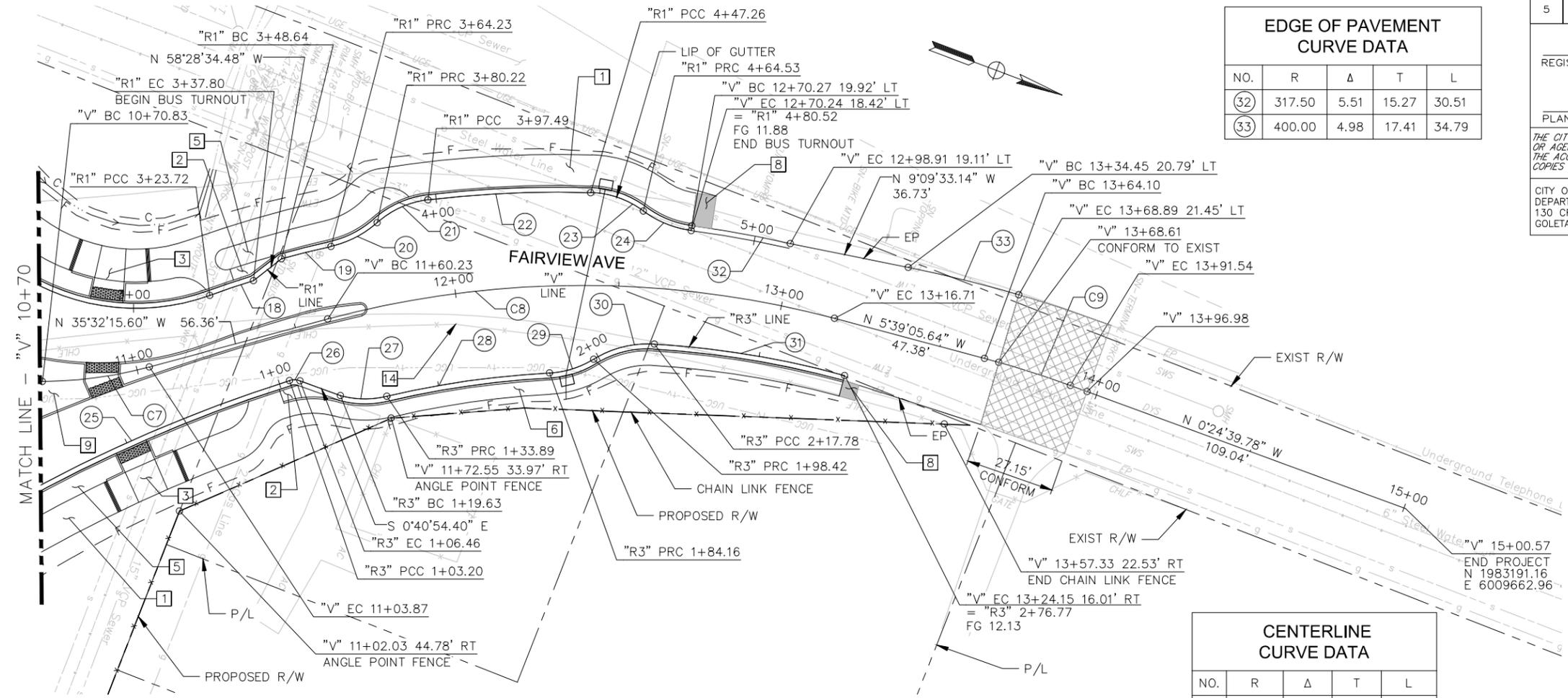
CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670

NO.	R	Δ	T	L
(32)	317.50	5.51	15.27	30.51
(33)	400.00	4.98	17.41	34.79

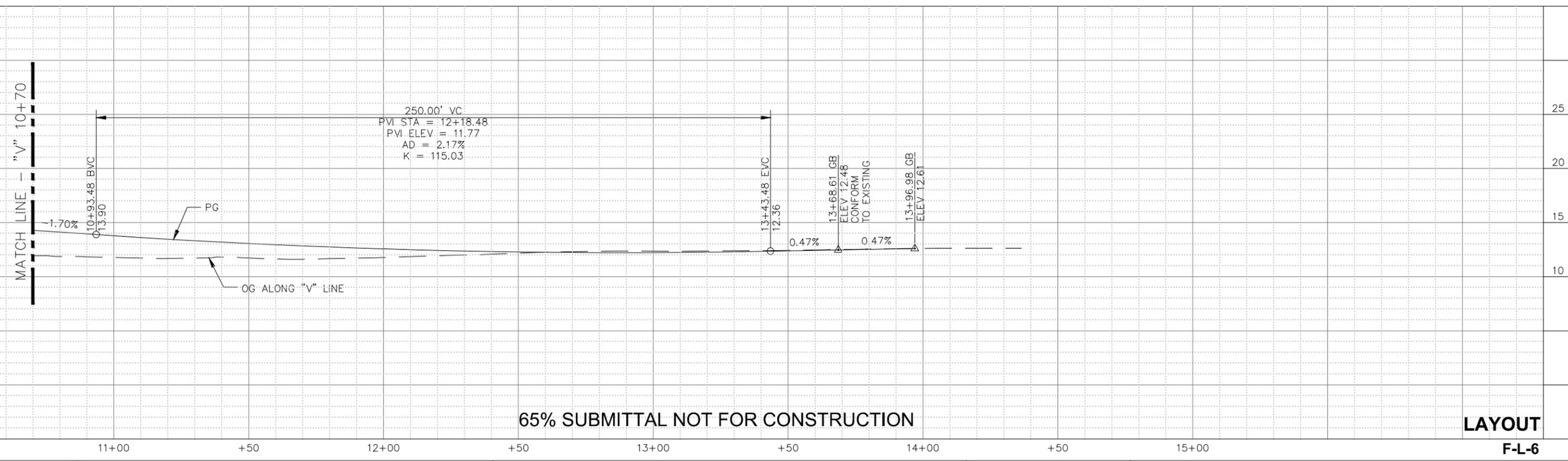
NO.	R	Δ	T	L
(18)	312.00	2.59	7.04	14.08
(19)	316.00	2.83	7.80	15.60
(20)	31.50	29.08	8.17	15.99
(21)	28.50	34.72	8.91	17.27
(22)	325.00	8.77	24.93	49.77
(23)	28.50	34.72	8.91	17.27
(24)	31.50	29.08	8.17	15.99
(25)	451.50	13.10	51.83	103.20
(26)	5.00	37.31	1.69	3.26
(27)	28.50	28.67	7.28	14.26
(28)	275.00	10.47	25.21	50.27
(29)	28.50	28.67	7.28	14.26
(30)	31.50	35.21	10.00	19.36
(31)	283.61	8.28	20.52	40.96

NO.	R	Δ	T	L
(C7)	130.00	14.56	16.61	33.04
(C8)	300.00	29.89	80.07	156.48
(C9)	300.00	5.24	13.73	27.44



- NOTES:**
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 - FOR LEGEND, GENERAL NOTES, AND CONSTRUCTION NOTES, SEE SHEET F-L-1.

PLAN
 HORIZ: 1"=20'
 VERT: 1"=5'



65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
F-L-6

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LAST REVISION DATE PLOTTED => Oct 04, 2013 TIME PLOTTED => 3:12 pm

ATTACHMENT C
Hollister Avenue/SR 217

**CITY OF GOLETA
PUBLIC WORKS DEPARTMENT**

**PROJECT PLANS FOR CONSTRUCTION ON
STATE HIGHWAY, COUNTY STREET, AND CITY STREET
HOLLISTER AVENUE/ROUTE 217 INTERCHANGE
EKWILL STREET AND FOWLER ROAD EXTENSION
IN SANTA BARBARA COUNTY
IN SANTA BARBARA AND GOLETA
AT VARIOUS LOCATIONS**

TO BE SUPPLEMENTED BY CALTRANS STANDARD PLANS DATED 2010
AND COUNTY OF SANTA BARBARA STANDARD DETAILS DATED SEPTEMBER 2011

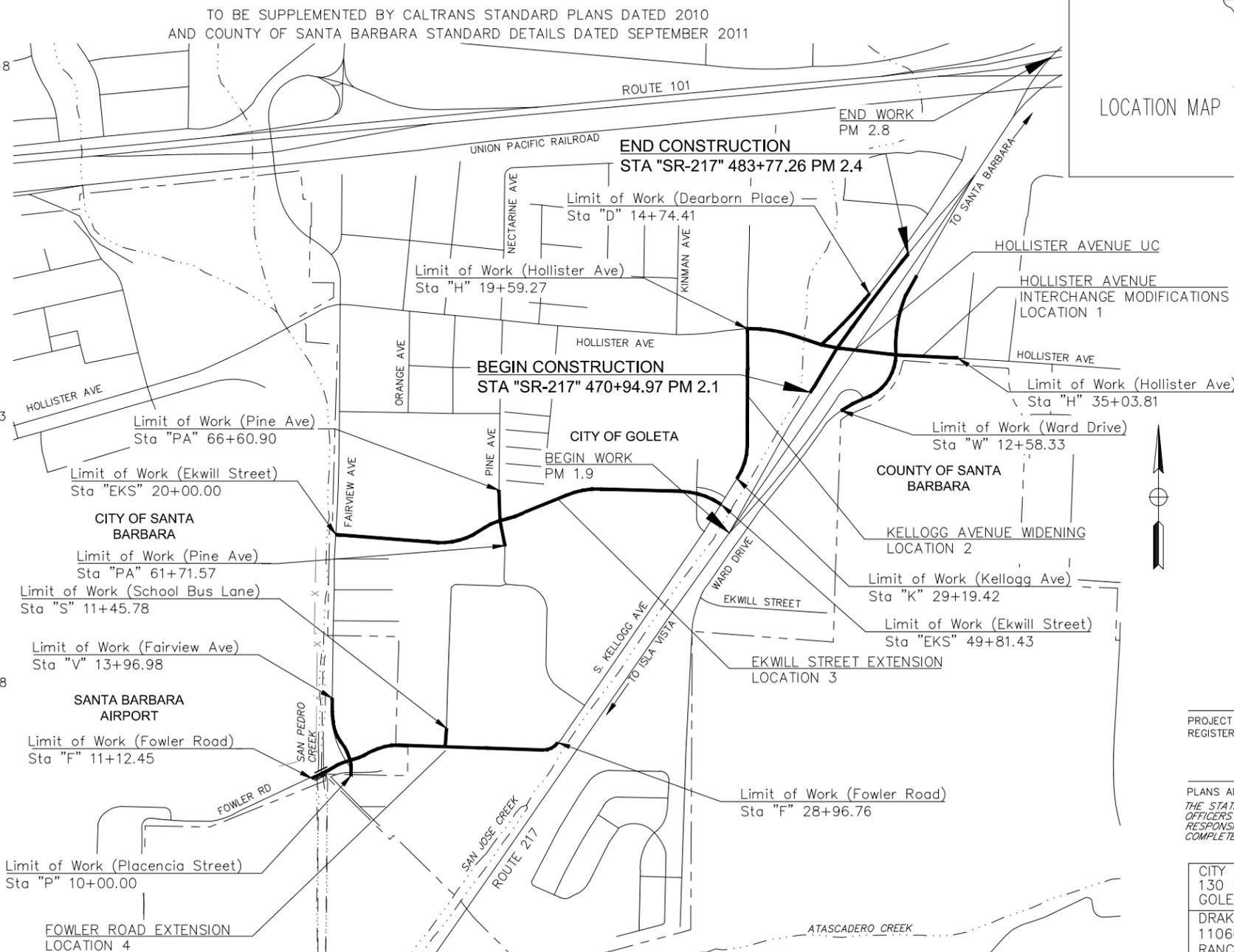


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24-28	PROFILE AND SUPERELEVATION DIAGRAMS	H-PS-1 TO H-PS-5
29-32	SUPPLEMENTAL PROFILES	H-SP-1 TO H-SP-4
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XX-XX	STREET LIGHTING - NOT INCLUDED IN SET	



65% SUBMITTAL NOT FOR CONSTRUCTION

PROJECT ENGINEER _____ DATE _____
REGISTERED CIVIL ENGINEER



PLANS APPROVAL DATE _____
THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

CITY OF GOLETA, DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN & ASSOCIATES
11060 WHITE ROCK ROAD, SUITE 200
RANCHO CORDOVA, CA 95670

CONTRACT No. **05-4611U4**

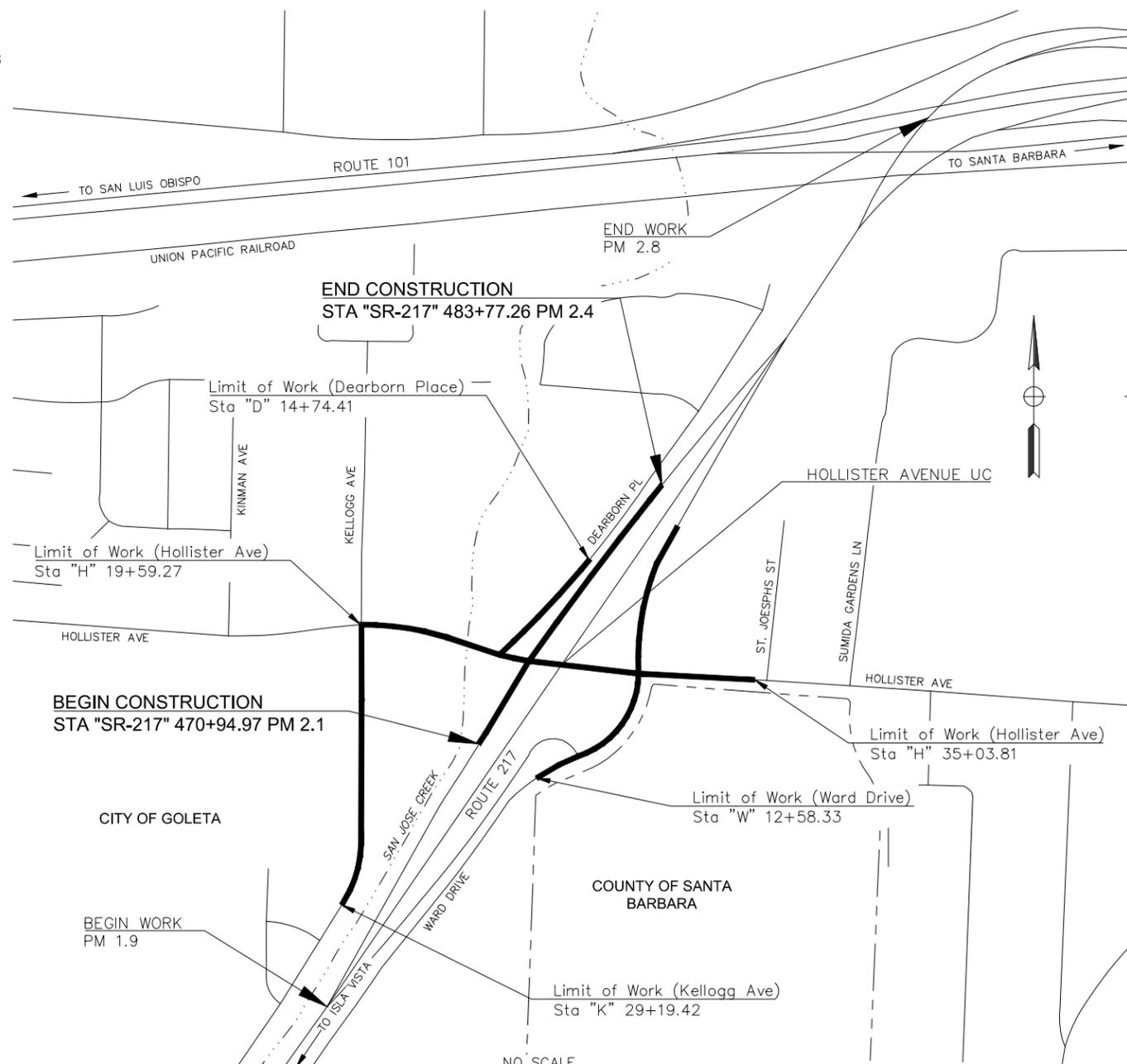
PROJECT ID **0500000548**

CITY OF GOLETA
PUBLIC WORKS DEPARTMENT
PROJECT PLANS FOR CONSTRUCTION ON
STATE HIGHWAY, COUNTY STREET, AND CITY STREET
HOLLISTER AVENUE/ROUTE 217 INTERCHANGE
IN SANTA BARBARA COUNTY
IN GOLETA

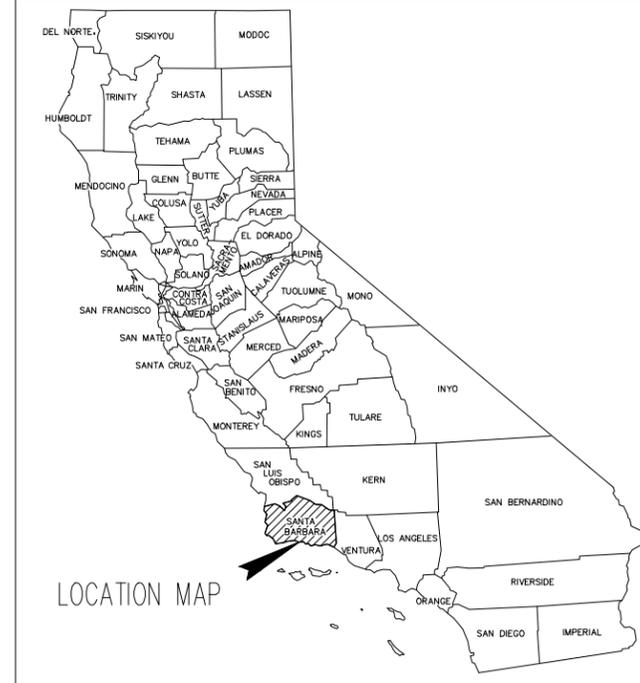
TO BE SUPPLEMENTED BY CALTRANS STANDARD PLANS DATED 2010
 AND COUNTY OF SANTA BARBARA STANDARD DETAILS DATED SEPTEMBER 2011

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Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB	217	2.1/2.4	1	143



APPROVED AS TO IMPACT ON STATE FACILITIES AND CONFORMANCE WITH APPLICABLE STATE STANDARDS AND PRACTICES AND THAT TECHNICAL OVERSIGHT WAS PERFORMED.

DATE SIGNED

LICENSE Exp DATE

REGISTRATION No.

CALTRANS DESIGN OVERSIGHT APPROVAL

CONSULTANT DESIGN ENGINEER

CRAIG DRAKE

PROJECT ENGINEER _____ DATE _____
 REGISTERED CIVIL ENGINEER



PLANS APPROVAL DATE _____
 THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

CITY OF GOLETA, DEPARTMENT OF PUBLIC WORKS
 130 CREMONA DRIVE, SUITE B
 GOLETA, CA 93117

DRAKE HAGLAN & ASSOCIATES
 11060 WHITE ROCK ROAD, SUITE 200
 RANCHO CORDOVA, CA 95670

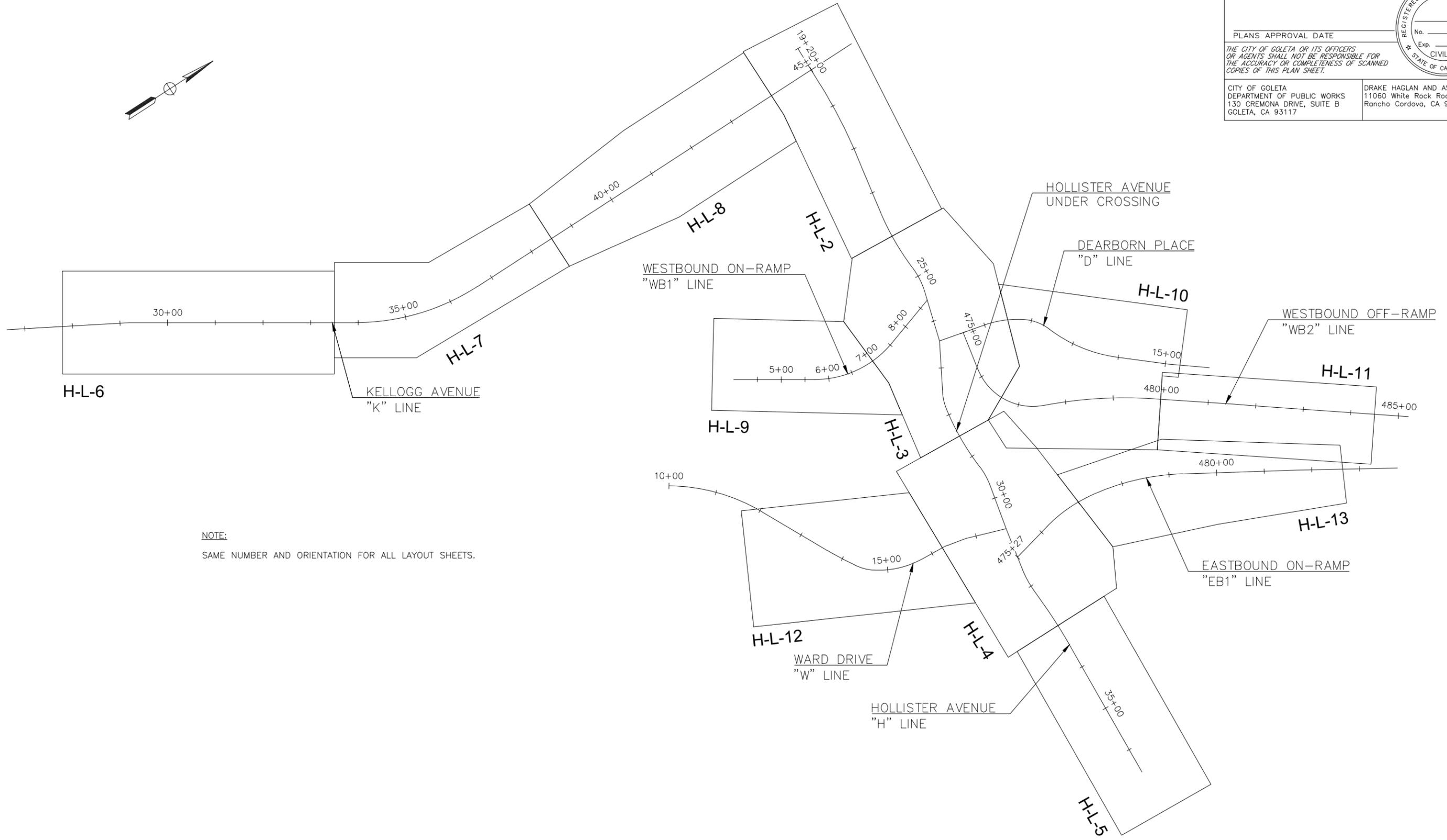
CONTRACT No. 05-4611U4

PROJECT ID 0500000548

THE CONTRACTOR SHALL POSSESS THE CLASS (OR CLASSES) OF LICENSE AS SPECIFIED IN THE "NOTICE TO BIDDERS."

65% SUBMITTAL NOT FOR CONSTRUCTION

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			7	143
REGISTERED CIVIL ENGINEER			DATE		
PLANS APPROVAL DATE			No.		
			Exp.		
			CIVIL		
			STATE OF CALIFORNIA		
CITY OF GOLETA DEPARTMENT OF PUBLIC WORKS 130 CREMONA DRIVE, SUITE B GOLETA, CA 93117			DRAKE HAGLAN AND ASSOCIATES 11060 White Rock Road, Suite 200 Rancho Cordova, CA 95670		



NOTE:
SAME NUMBER AND ORIENTATION FOR ALL LAYOUT SHEETS.

65% SUBMITTAL NOT FOR CONSTRUCTION

KEY MAP AND LINE INDEX
NO SCALE

K-1



NOTE:
FOR ACCURATE RIGHT OF WAY DATA,
CONTACT RIGHT OF WAY ENGINEERING AT
CALTRANS, CITY, AND COUNTY OFFICES.

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			9	143

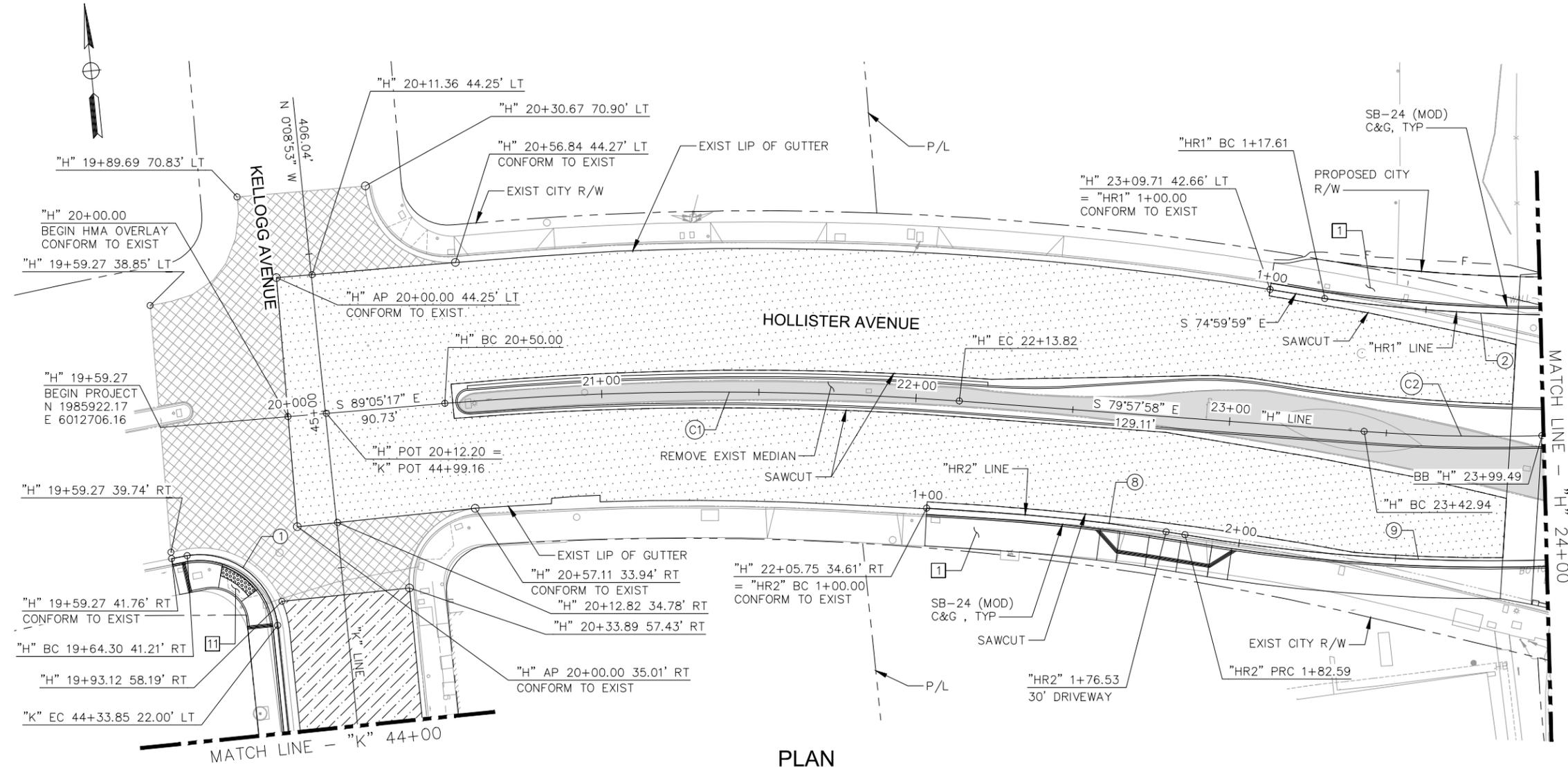
REGISTERED CIVIL ENGINEER _____ DATE _____

PLANS APPROVAL DATE _____

THE CITY OF GOLETA OR ITS OFFICERS
OR AGENTS SHALL NOT BE RESPONSIBLE FOR
THE ACCURACY OR COMPLETENESS OF SCANNED
COPIES OF THIS PLAN SHEET.

CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670

**CENTERLINE
CURVE DATA**

NO.	R	Δ	T	L
ⓐ	1029.00	9°07'19"	82.09	163.82
ⓑ	545.00	14°28'46"	69.23	137.73

**LIP OF GUTTER
CURVE DATA**

NO.	R	Δ	T	L
①	25.00	95.06	27.31	41.48
②	521.47	14.22	65.04	129.40
⑧	602.00	6.09	32.01	63.96
⑨	584.50	12.98	66.48	132.39

PLAN
HORIZ: 1"=20'

65% SUBMITTAL NOT FOR CONSTRUCTION

**LAYOUT
H-L-2**

REVISOR: _____ DATE: _____

CHECKED BY: _____

DESIGNED BY: _____

CONSULTANT FUNCTIONAL SUPERVISOR: **DAVE MELIS**

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			10	143

REGISTERED CIVIL ENGINEER _____ DATE _____

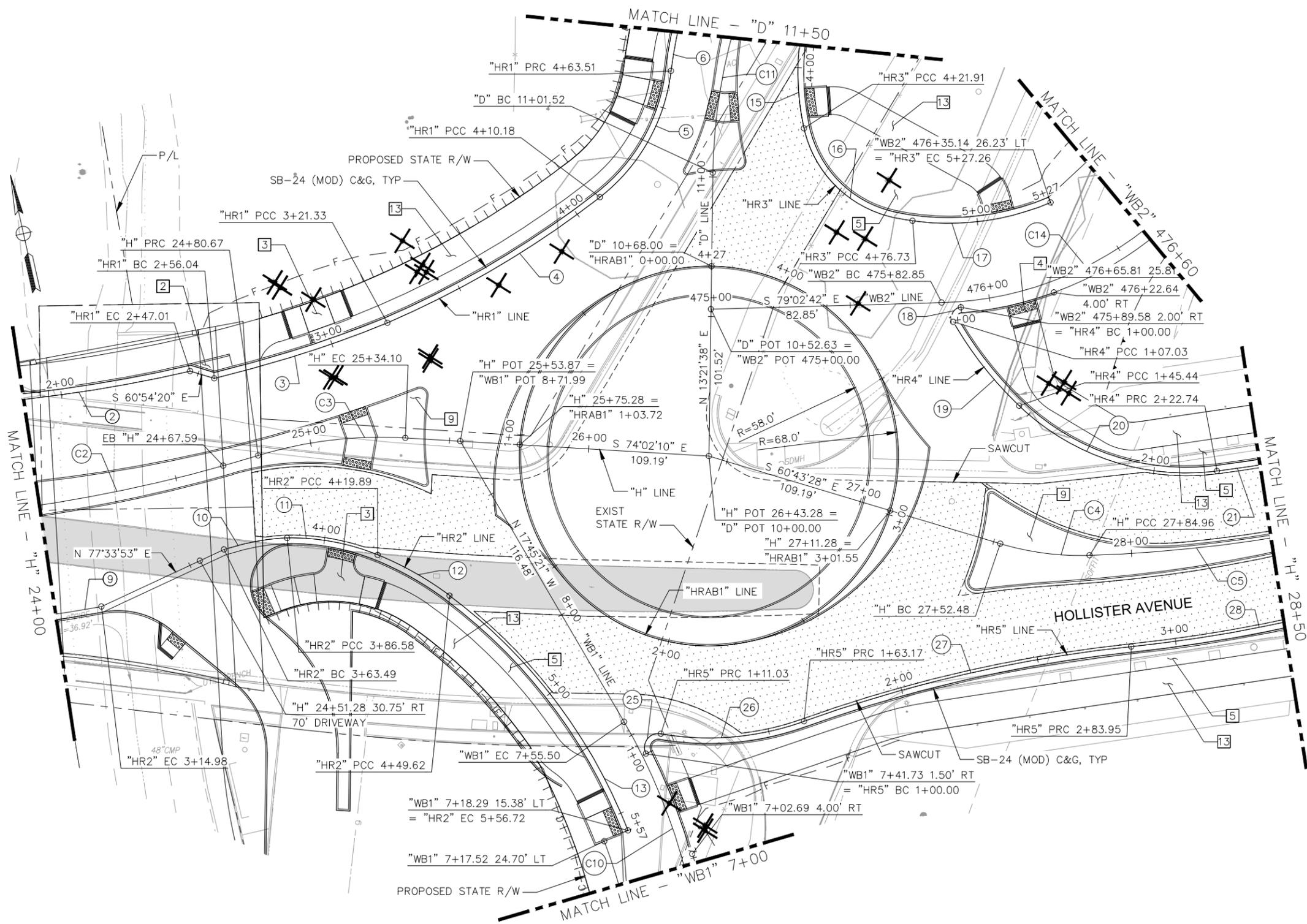
PLANS APPROVAL DATE _____

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CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670

NOTE:
FOR ACCURATE RIGHT OF WAY DATA,
CONTACT RIGHT OF WAY ENGINEERING AT
CALTRANS, CITY, AND COUNTY OFFICES.



CENTERLINE CURVE DATA

NO.	R	Δ	T	L
C2	545.00	14°28'46"	69.23	137.73
C3	150.00	20°24'34"	27.00	53.43
C4	100.00	18°36'33"	16.38	32.48
C5	527.50	15°24'54"	71.39	141.92
C10	210.00	50°36'41"	99.29	185.50
C11	200.00	19°30'47"	34.39	68.11
C14	124.00	79°00'15"	102.23	170.98

LIP OF GUTTER CURVE DATA

NO.	R	Δ	T	L
2	521.47	14.22	65.04	129.40
3	393.00	5.42	18.59	37.16
4	352.00	5.73	17.62	35.20
5	67.25	47.70	29.73	55.99
6	134.00	51.14	64.11	119.60
9	584.50	12.98	66.48	132.39
10	78.00	13.11	8.96	17.84
11	82.00	23.28	16.89	33.31
12	117.00	14.56	14.95	29.73
13	192.00	20.12	34.06	67.42
15	92.00	67.55	61.53	108.47
16	42.00	64.66	26.58	47.40
17	94.00	35.32	29.93	57.95
18	3.50	115.12	5.51	7.03
19	132.00	17.00	19.73	39.16
20	87.00	50.41	40.95	76.55
21	501.00	9.67	42.36	84.53
25	5.00	126.39	9.90	11.03
26	101.00	29.58	26.66	52.14
27	501.92	13.79	60.68	120.78
28	524.00	13.26	60.92	121.30

PLAN

HORIZ: 1"=20'

65% SUBMITTAL NOT FOR CONSTRUCTION

**LAYOUT
H-L-3**

REVISOR: _____ DATE: _____

CHECKED BY: _____

DESIGNED BY: _____

CONSULTANT SUPERVISOR: DAVE MELIS

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			11	143

REGISTERED CIVIL ENGINEER _____ DATE _____

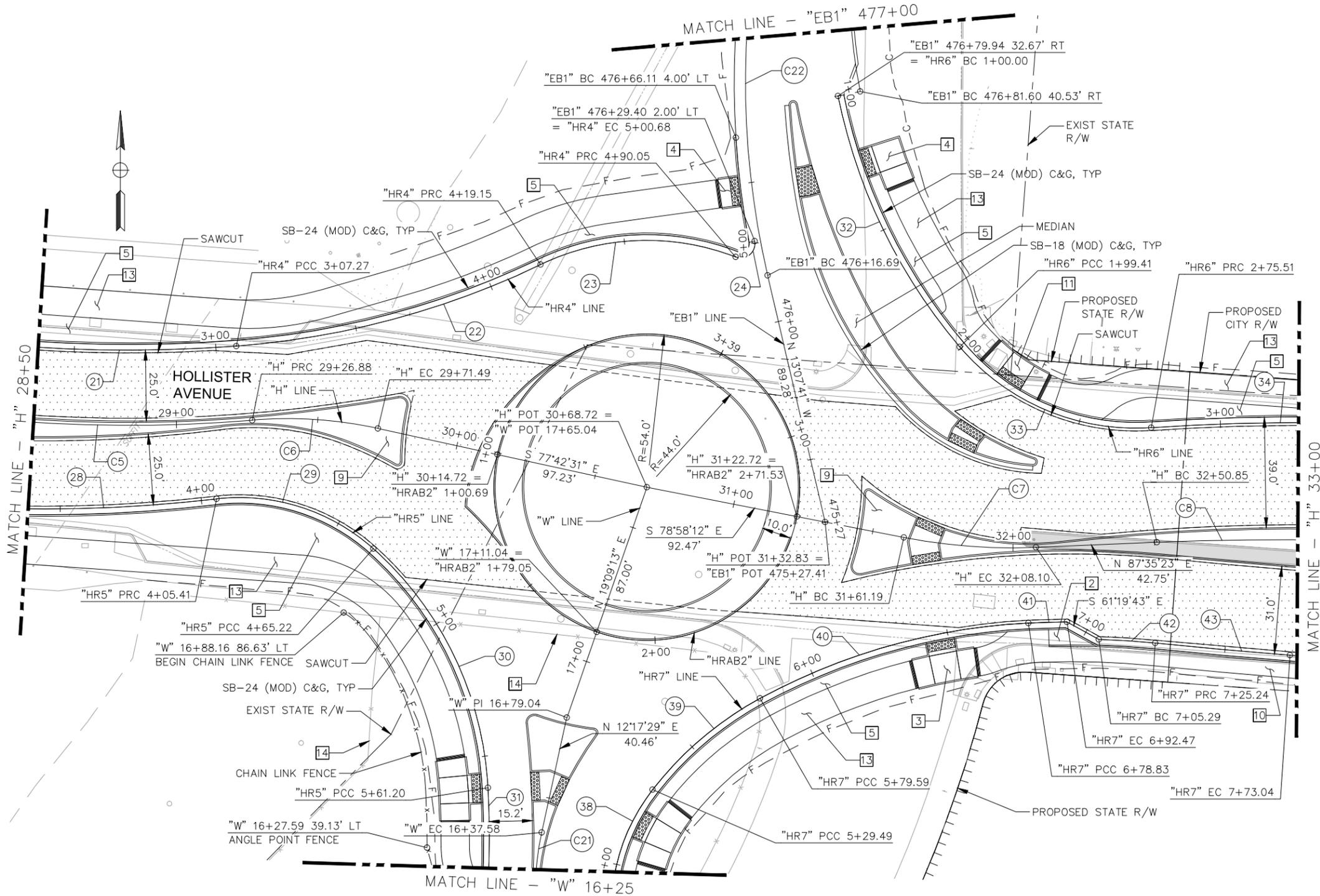
PLANS APPROVAL DATE _____

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CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670

NOTE:
FOR ACCURATE RIGHT OF WAY DATA,
CONTACT RIGHT OF WAY ENGINEERING AT
CALTRANS, CITY, AND COUNTY OFFICES.



NO.	R	Δ	T	L
C5	527.50	15°24'54"	71.39	141.92
C6	150.00	17°02'24"	22.47	44.61
C7	200.00	13°26'25"	23.57	46.92
C8	1000.00	5°30'14"	48.07	96.06
C21	100.00	8°48'59"	7.71	15.39
C22	300.00	20°36'39"	54.55	107.92

NO.	R	Δ	T	L
22	302.00	21.23	56.59	111.88
23	87.00	46.70	37.55	70.90
24	5.00	121.82	8.99	10.63
28	524.00	13.26	60.92	121.30
29	71.00	48.31	31.84	59.87
30	121.50	45.31	50.71	96.08
31	336.50	8.95	26.33	52.55
32	196.50	28.99	50.79	99.41
33	81.50	53.50	41.08	76.10
34	786.00	7.55	51.88	103.62
38	69.50	42.50	27.02	51.55
39	121.50	23.63	25.41	50.10
40	222.00	25.61	50.46	99.24
41	306.00	2.55	6.82	13.64
42	300.00	3.81	9.98	19.95
43	2730.00	1.00	23.90	47.80

PLAN

HORIZ: 1"=20'

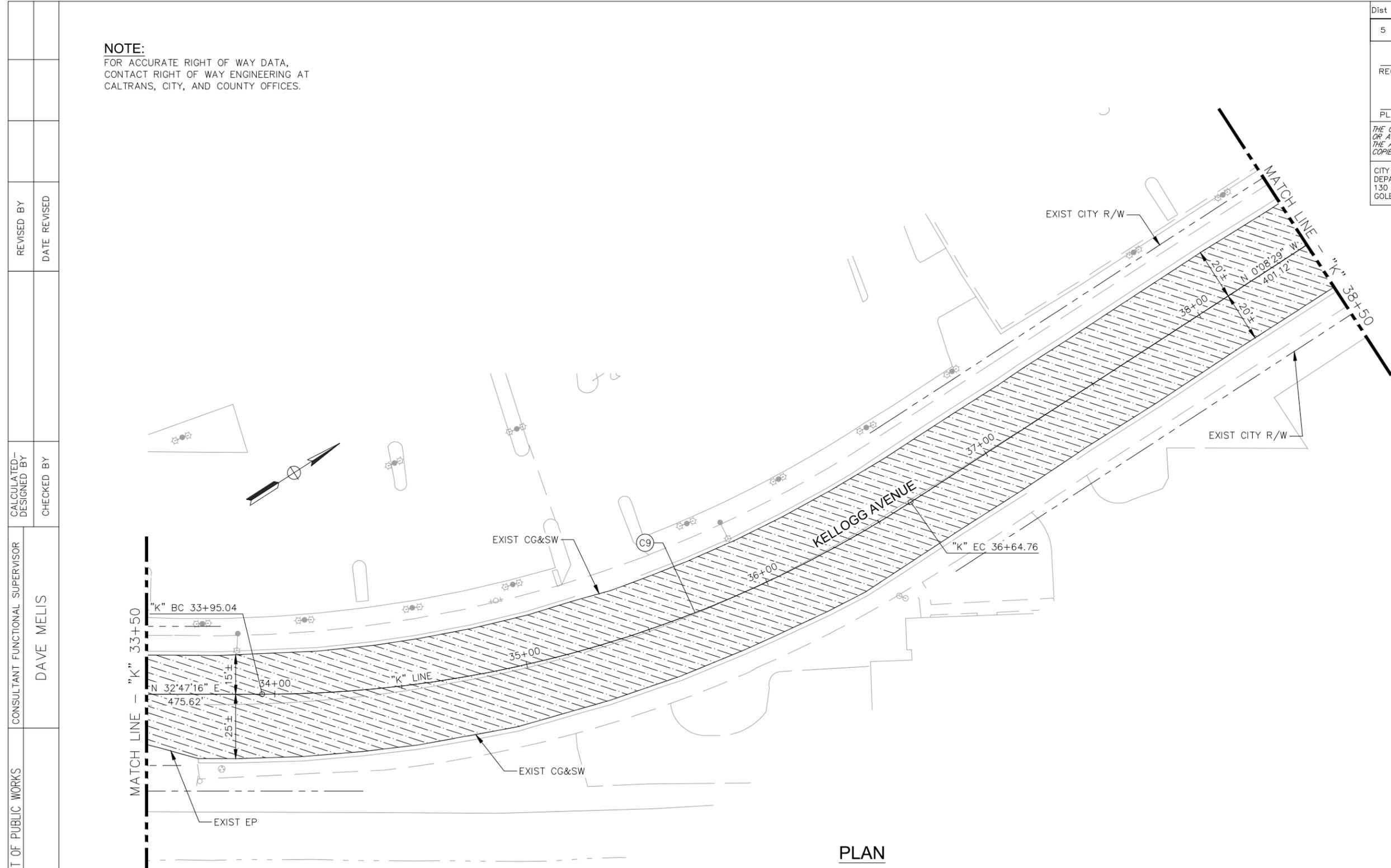
65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
H-L-4

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			14	143
REGISTERED CIVIL ENGINEER			DATE		
PLANS APPROVAL DATE					
<small>THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
CITY OF GOLETA DEPARTMENT OF PUBLIC WORKS 130 CREMONA DRIVE, SUITE B GOLETA, CA 93117			DRAKE HAGLAN AND ASSOCIATES 11060 White Rock Road, Suite 200 Rancho Cordova, CA 95670		



NOTE:
 FOR ACCURATE RIGHT OF WAY DATA,
 CONTACT RIGHT OF WAY ENGINEERING AT
 CALTRANS, CITY, AND COUNTY OFFICES.



CENTERLINE CURVE DATA				
NO.	R	Δ	T	L
(C9)	469.31	32.93	138.70	269.72

PLAN
 HORIZ: 1"=20'

65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
H-L-7

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS	CONSULTANT FUNCTIONAL SUPERVISOR	CALCULATED-DESIGNED BY	REVISOR
	DAVE MELIS	CHECKED BY	DATE REVISED

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			17	143

REGISTERED CIVIL ENGINEER _____ DATE _____

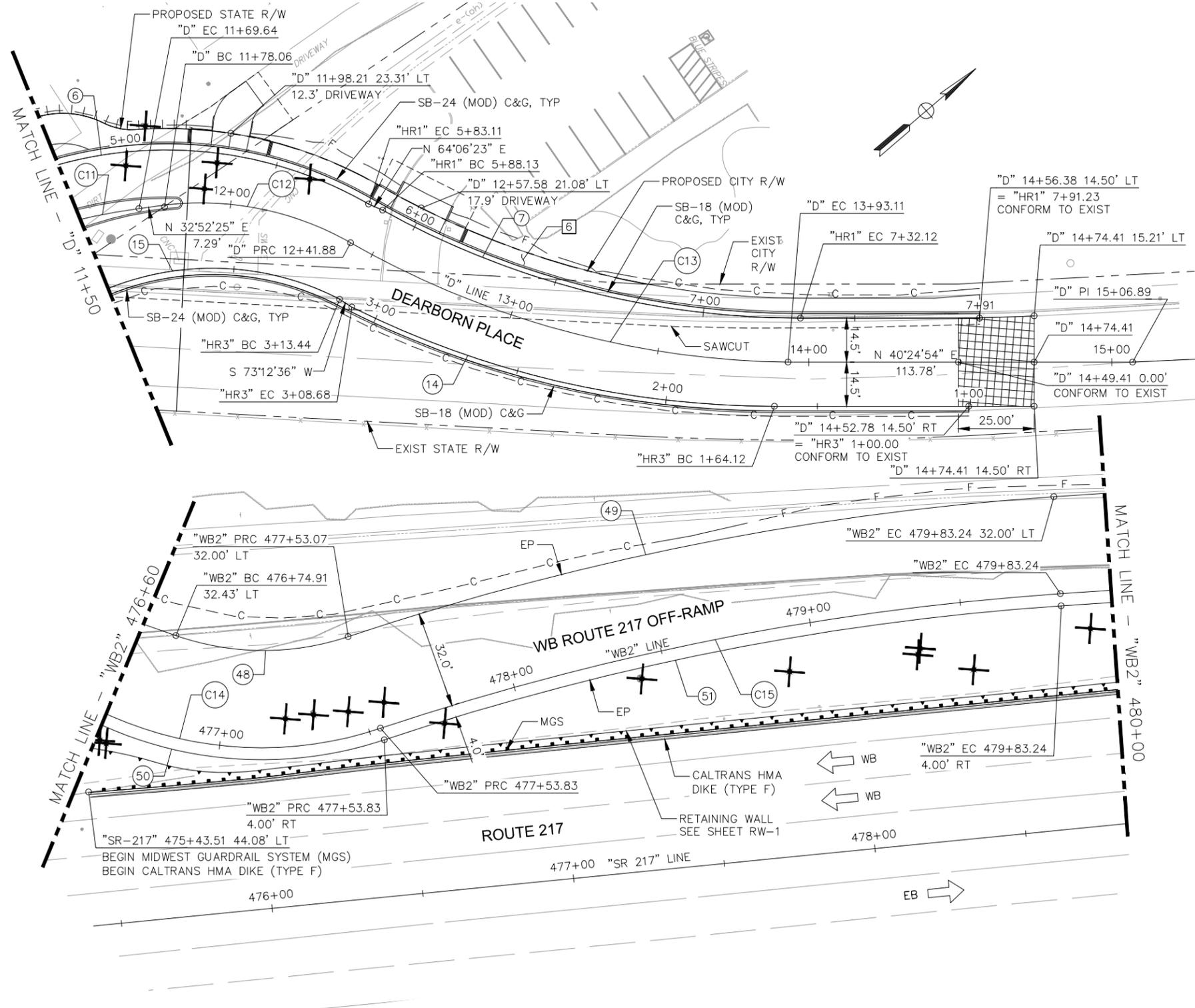
PLANS APPROVAL DATE _____

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CITY OF GOLETA
DEPARTMENT OF PUBLIC WORKS
130 CREMONA DRIVE, SUITE B
GOLETA, CA 93117

DRAKE HAGLAN AND ASSOCIATES
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670

NOTE:
FOR ACCURATE RIGHT OF WAY DATA,
CONTACT RIGHT OF WAY ENGINEERING AT
CALTRANS, CITY, AND COUNTY OFFICES.



NO.	R	Δ	T	L
(C11)	200.00	19°30'47"	34.39	68.11
(C12)	100.00	37°12'45"	33.67	64.95
(C13)	300.30	28°51'15"	77.25	151.23
(C14)	124.00	79°00'15"	102.23	170.98
(C15)	900.00	14°36'16"	115.33	229.41

NO.	R	Δ	T	L
(6)	134.00	51.14	64.11	119.60
(7)	285.56	28.89	73.56	143.98
(14)	314.54	26.33	73.58	144.56
(15)	92.00	67.55	61.53	108.47

NO.	R	Δ	T	L
(48)	88.00	37°43'28"	30.06	57.94
(49)	932.00	14°38'22"	119.72	238.13
(50)	128.00	60°37'14"	74.83	135.43
(51)	896.00	14°36'16"	114.82	228.39

PLAN

HORIZ: 1"=20'

65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
H-L-10

REVISOR: _____ DATE: _____

DESIGNED BY: _____ CHECKED BY: _____

CONSULTANT FUNCTIONAL SUPERVISOR: DAVE MELIS

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

BORDER LAST REVISED 7/2/2010

USERNAME => acastlekeane
DGN FILE => HOLLISTER_EF-L-9-WB RAMPS.dwg

RELATIVE BORDER SCALE IS IN INCHES

UNIT 0000

PROJECT NUMBER & PHASE

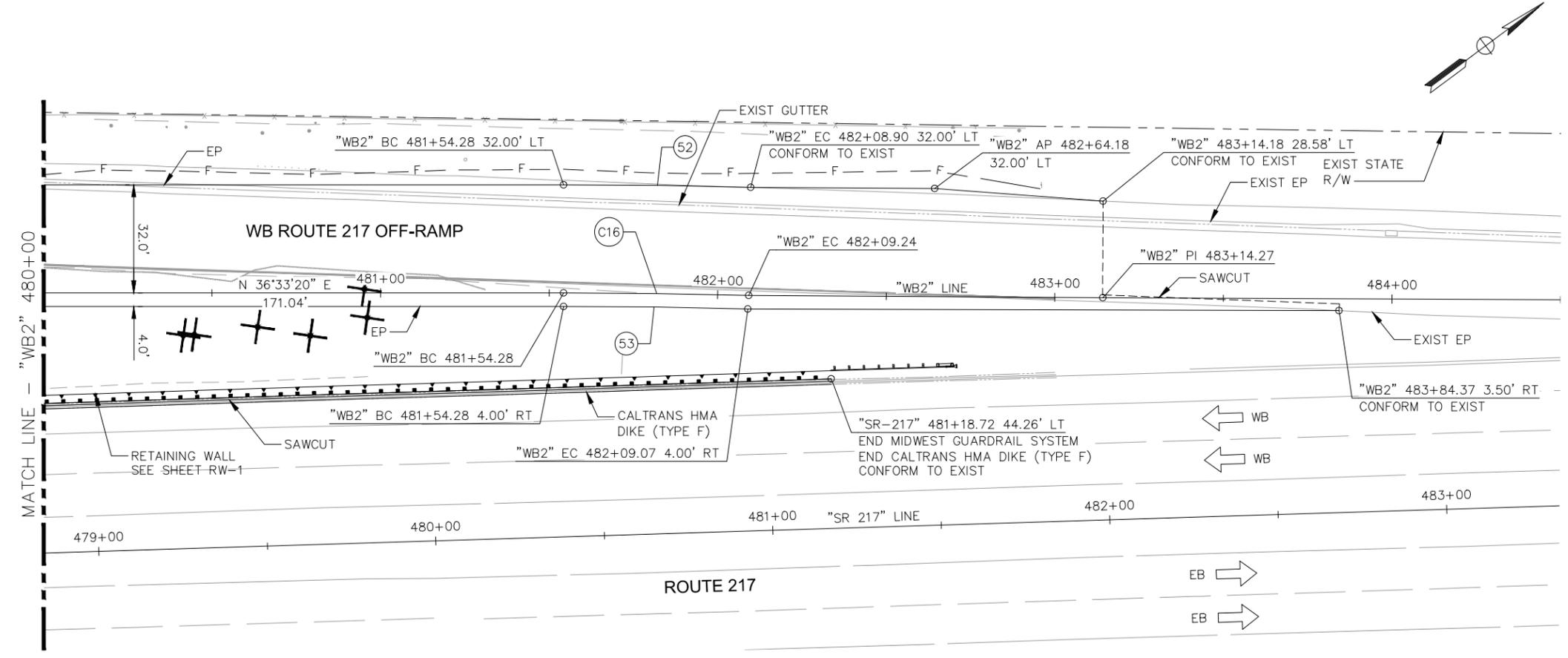
4611U0

DATE PLOTTED => Oct 24, 2013
TIME PLOTTED => 10:15 am

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			18	143
REGISTERED CIVIL ENGINEER			DATE		
PLANS APPROVAL DATE					
<small>THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.</small>					
CITY OF GOLETA DEPARTMENT OF PUBLIC WORKS 130 CREMONA DRIVE, SUITE B GOLETA, CA 93117			DRAKE HAGLAN AND ASSOCIATES 11060 White Rock Road, Suite 200 Rancho Cordova, CA 95670		



NOTE:
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CONTACT RIGHT OF WAY ENGINEERING AT
CALTRANS, CITY, AND COUNTY OFFICES.



ALIGNMENT CURVE DATA				
NO.	R	Δ	T	L
(C16)	1976.00	1°35'37"	27.48	54.96

EDGE OF PAVEMENT CURVE DATA				
NO.	R	Δ	T	L
(52)	2008.00	1°35'02"	27.76	55.51
(53)	1972.00	1°35'20"	27.34	54.69

PLAN
HORIZ: 1"=20'

65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
H-L-11

CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS
 CONSULTANT FUNCTIONAL SUPERVISOR
 DAVE MELIS
 CALCULATED/DESIGNED BY
 CHECKED BY
 REVISED BY
 DATE REVISED

BORDER LAST REVISED 7/2/2010

USERNAME => acastlekeane
DGN FILE => HOLLISTER_EF-L-9-WB RAMPS.dwg

RELATIVE BORDER SCALE IS IN INCHES

UNIT 0000

PROJECT NUMBER & PHASE

4611U0

LAST REVISION
 00-00-00
 DATE PLOTTED => Oct 24, 2013
 TIME PLOTTED => 10:15 am

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			19	143

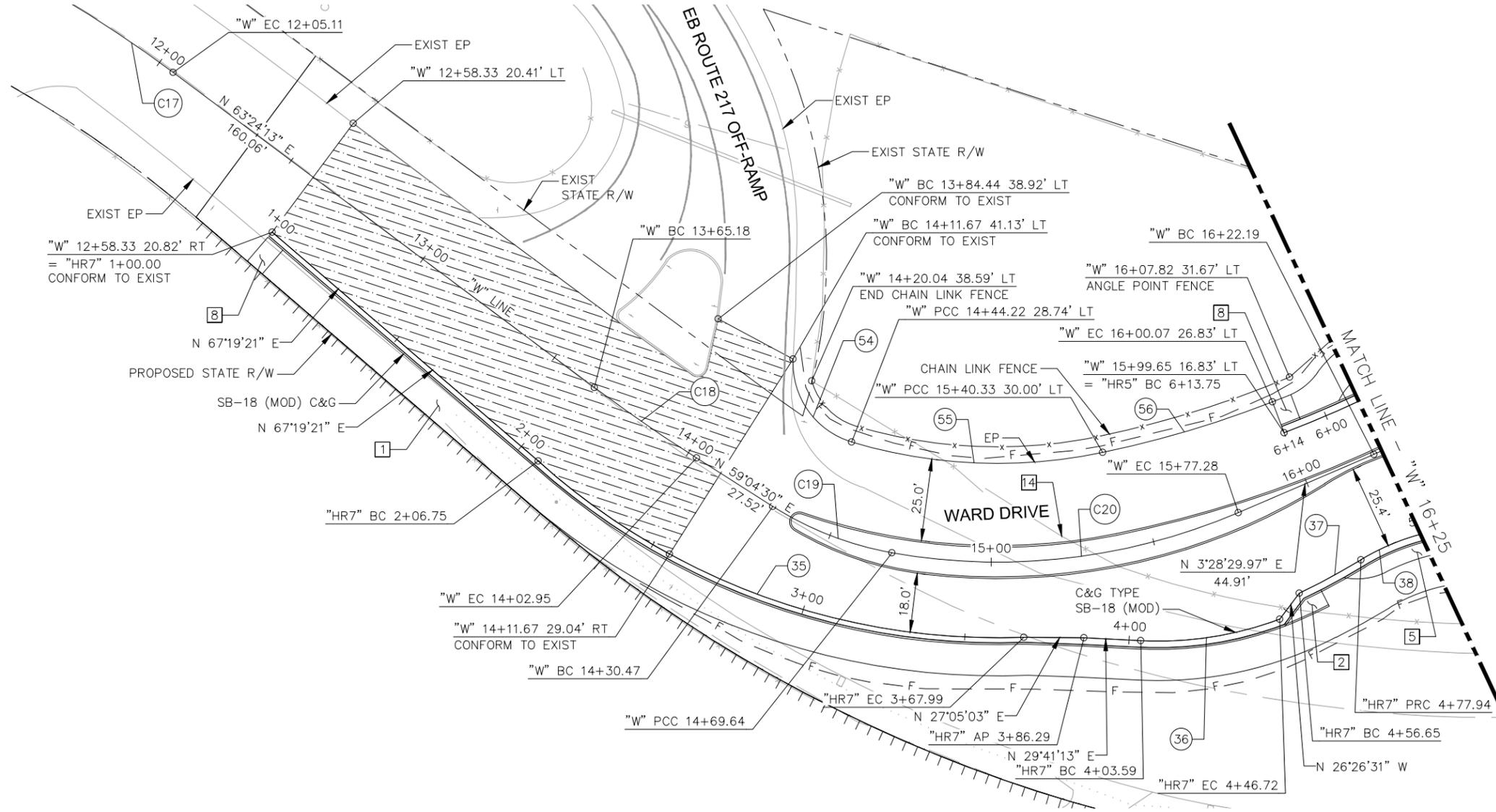
REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	



THE CITY OF GOLETA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

CITY OF GOLETA DEPARTMENT OF PUBLIC WORKS 130 CREMONA DRIVE, SUITE B GOLETA, CA 93117	DRAKE HAGLAN AND ASSOCIATES 11060 White Rock Road, Suite 200 Rancho Cordova, CA 95670
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NOTE:
FOR ACCURATE RIGHT OF WAY DATA,
CONTACT RIGHT OF WAY ENGINEERING AT
CALTRANS, CITY, AND COUNTY OFFICES.



PLAN

HORIZ: 1"=20'

65% SUBMITTAL NOT FOR CONSTRUCTION

CENTERLINE CURVE DATA

NO.	R	Δ	T	L
(C17)	390.00	30.13	104.99	205.11
(C18)	500.00	4.33	18.90	37.78
(C19)	100.00	22.44	19.84	39.17
(C20)	186.00	33.16	55.37	107.64

LIP OF GUTTER CURVE DATA

NO.	R	Δ	T	L
(35)	209.00	44.20	84.87	161.24
(36)	105.00	23.53	21.87	43.13
(37)	209.00	5.84	10.66	21.30
(38)	69.50	42.50	27.02	51.55

EDGE OF PAVEMENT CURVE DATA

NO.	R	Δ	T	L
(54)	25.00	76.28	19.63	33.28
(55)	156.00	28.37	39.43	77.25
(56)	326.00	9.49	27.06	54.00

REVISOR: []
 DATE: []
 DESIGNED BY: []
 CHECKED BY: []
 CONSULTANT FUNCTIONAL SUPERVISOR: DAVE MELIS
 CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

USERNAME => acastlekeane
DGN FILE => HOLLISTER_EF-L-12-EB RAMPS.dwg



UNIT 0000

PROJECT NUMBER & PHASE

4611U0

LAYOUT
H-L-12

LAST REVISION: []
 DATE PLOTTED => Oct 24, 2013
 TIME PLOTTED => 10:16 am

Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	SB			20	143

REGISTERED CIVIL ENGINEER	DATE
PLANS APPROVAL DATE	

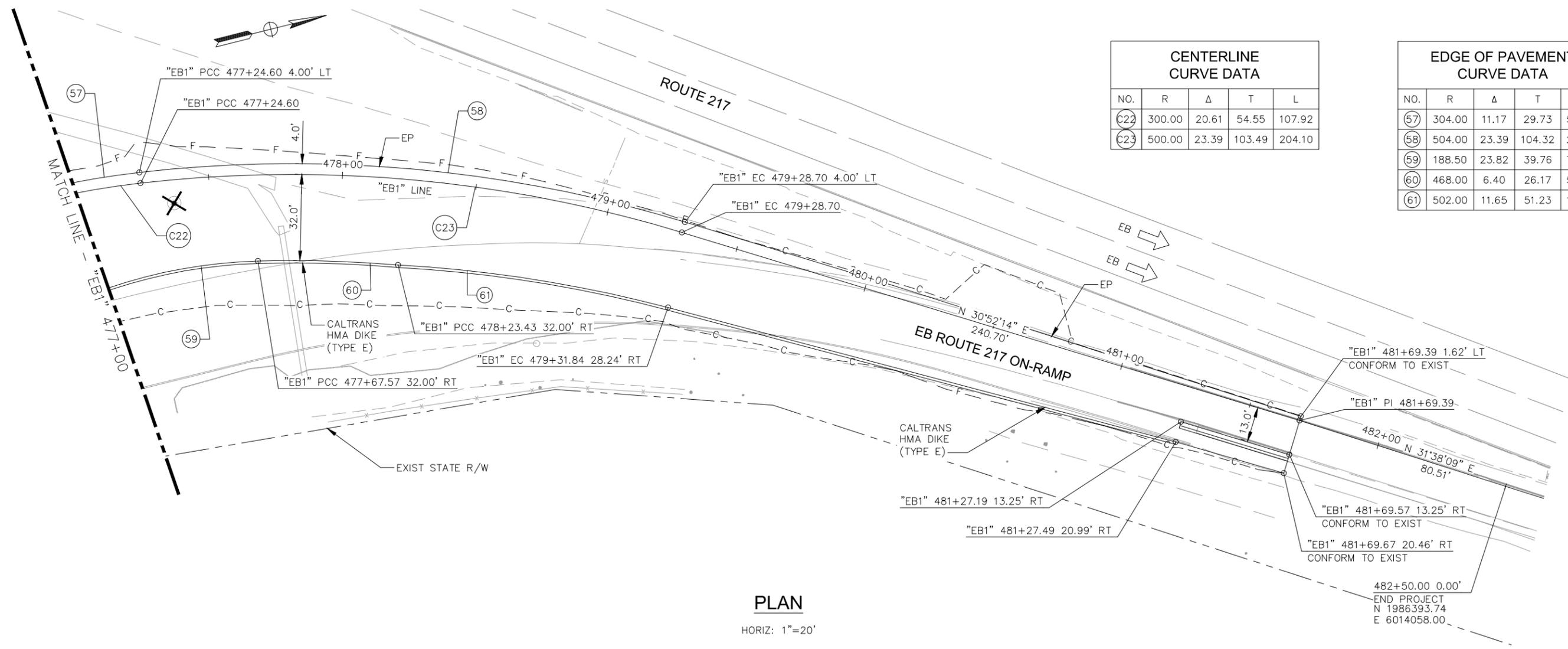


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CITY OF GOLETA DEPARTMENT OF PUBLIC WORKS 130 CREMONA DRIVE, SUITE B GOLETA, CA 93117	DRAKE HAGLAN AND ASSOCIATES 11060 White Rock Road, Suite 200 Rancho Cordova, CA 95670
--	---

NOTE:
FOR ACCURATE RIGHT OF WAY DATA,
CONTACT RIGHT OF WAY ENGINEERING AT
CALTRANS, CITY, AND COUNTY OFFICES.

REVISOR: [blank] DATE: [blank]
 CALCULATED/DESIGNED BY: [blank] CHECKED BY: [blank]
 CONSULTANT FUNCTIONAL SUPERVISOR: DAVE MELIS
 CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS



CENTERLINE CURVE DATA				
NO.	R	Δ	T	L
22	300.00	20.61	54.55	107.92
23	500.00	23.39	103.49	204.10

EDGE OF PAVEMENT CURVE DATA				
NO.	R	Δ	T	L
57	304.00	11.17	29.73	59.27
58	504.00	23.39	104.32	205.73
59	188.50	23.82	39.76	78.37
60	468.00	6.40	26.17	52.28
61	502.00	11.65	51.23	102.10

65% SUBMITTAL NOT FOR CONSTRUCTION

LAYOUT
H-L-13

CONCEPTUAL PLANT LEGEND



Specimen Trees for Roundabout
 Jacaranda mimosifolia / Jacaranda
 Platanus racemosa / California Sycamore
 Quercus tomentella / Island Live Oak



Large Canopy Street Trees
 Jacaranda mimosifolia / Jacaranda
 Koelreuteria bipinnata / Chinese Flame Tree
 Platanus x acerifolia / London Plane Tree
 Quercus virginiana / Southern Live Oak
 Ulmus parvifolia / Chinese Elm



Perennials / Grasses 18" height
 Arctostaphylos 'Pacific Mist' / Manzanita 'Pacific Mist'
 Carex divulsa / Berkeley Sedge
 Ceanothus griseus horizontalis 'Yankee Point' / California Lilac
 Helictotrichon sempervirens / Blue Oat Grass
 Lomandra confertifolia 'Little Con' / Small Mat Rush
 Lomandra longifolia 'Breeze' / Lomandra



Perennials (30 - 18 inches in height)
 Ceanothus griseus horizontalis 'Yankee Point' / California Lilac
 Festuca californica / California Fescue
 Leymus triticoides 'Canyon Prince' / Canyon Prince Wild Rye
 Muhlenbergia capillans / Hairy Awn Muhly
 Muhlenbergia rigens / Deer Grass



Shrubs (30 inches - 3 ft in height)
 Agave attenuata / Agave
 Arctostaphylos 'John Dourley' / Manzanita
 Lomandra 'Gary's Green' / Mat Rush
 Salvia apiana / White Sage
 Salvia clevelandii 'Wimfred Gillman' / Cleveland Sage
 Salvia greggii 'Alba' / Autumn Sage
 Salvia greggii 'Furmans Red' / Autumn Sage



Shrubs (3-6 ft in height)
 Arctostaphylos densiflora 'Howard McMinn' / Howard McMinn Manzanita
 Arctostaphylos 'Sunset' / Manzanita
 Heteromeles arbutifolia / Toyon
 Rhamnus californica / California Coffee Berry
 Rhus integrifolia / Lemonade Berry

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
5	Santa Barbara				

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

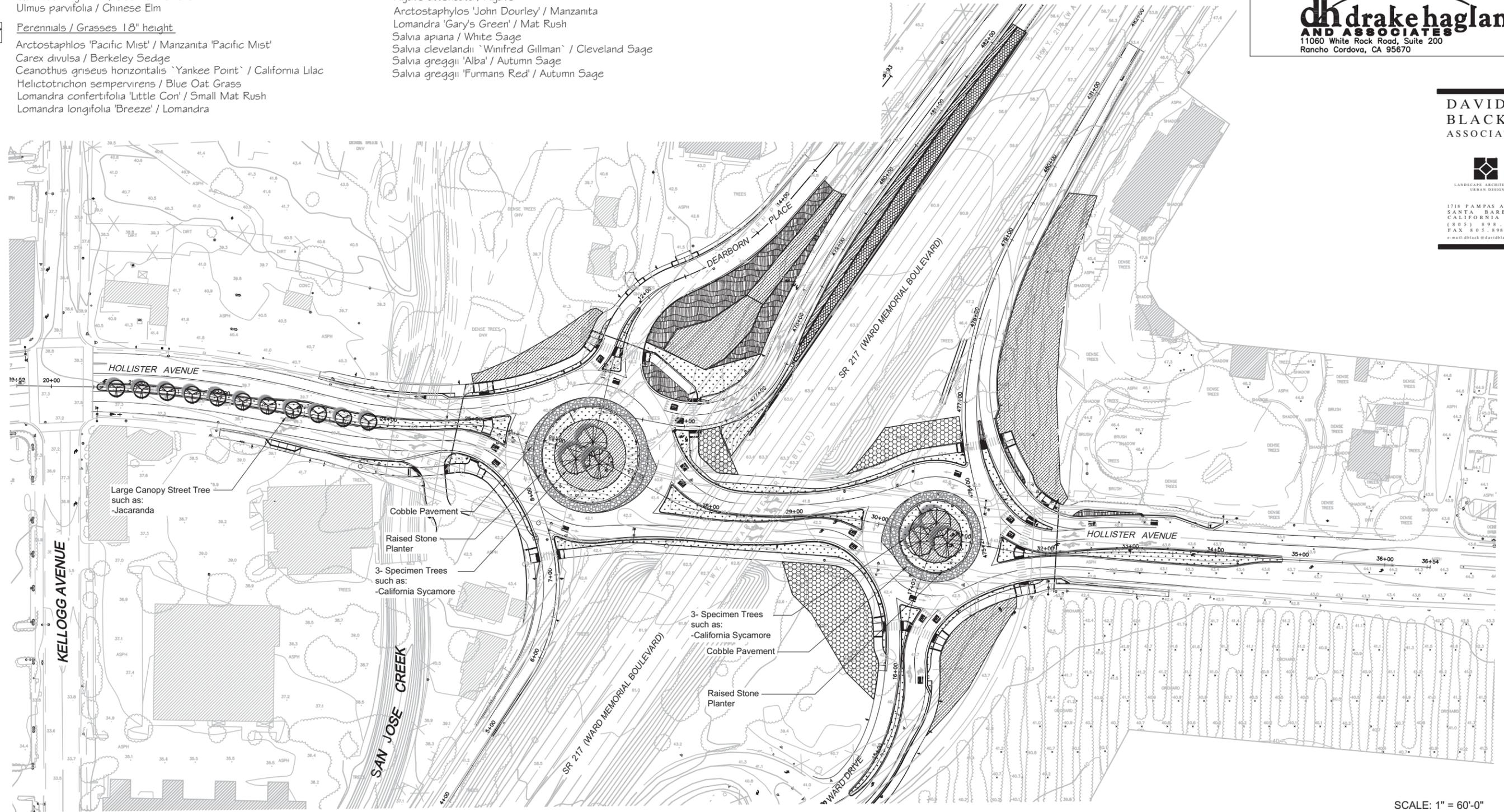
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DAVID R. BLACK & ASSOCIATES



1718 PAMPAS AVENUE
 SANTA BARBARA, CALIFORNIA 93101
 (805) 898-8717
 FAX 805-898-0373
 e-mail: dblack@davidblackla.com



SCALE: 1" = 60'-0"

Preliminary Landscape Plan - Hollister Road Extension
 Ekwil Street and Fowler Road Extensions Project

8-28-13 LP-1

REVISOR: [] DATE: []
 CALCULATED-DRAWN BY: [] CHECKED BY: []
 PROJECT ENGINEER: []
 CITY OF GOLETA - DEPARTMENT OF PUBLIC WORKS

BORDER LAST REVISED 8/7/13



USERNAME = work station
 DWG FILE = HOLLISTER Concept_8-27-13.dwg

CU 00000 EA 000000

LAST REVISION: 00-00-00
 AUG 28, 2013
 TIME PLOTTED = 4:23 pm

ATTACHMENT D

PRELIMINARY PLAN ESTIMATE

CITY OF GOLETA

DISTRICT 5

CO SBA

PROJ. NAME Ekwill Street Extension
 LIMITS Ekwill Street
 DESIGNER MNS Engineers, Inc.

QUANTITIES BY TJW
 PRICED BY SMK
 COST INDEX Caltrans

DATE 8/14/2013
 DATE 1/25/2013
 DATE 2007-2012

BEES NO.	CONTRACT ITEMS	UNIT	QUANTITY	PRICE	AMOUNT
1	120090 Construction Area Signs	LS	1	\$15,000	\$15,000
2	120120 Type III Barricade	EA	30	\$75	\$2,250
3	120149 Temporary Pavement Marking (Paint)	LF	400	\$3	\$1,100
4	120159 Temporary Traffic Stripe (Paint)	LF	1,200	\$0.3	\$360
5	141000 Temporary Fence (Type ESA)	LF	400	\$10	\$4,000
6	150605 Remove Fence	LF	200	\$2	\$400
7	150711 Remove Painted Traffic Stripe	LF	400	\$0.35	\$140
8	150712 Remove Painted Pavement Marking	SF	200	\$3	\$500
9	150742 Remove Roadside Sign	EA	6	\$100	\$600
10	150860 Remove Base & Surfacing	CY	4,890	\$20	\$97,800
11	152320 Reset Roadside Sign	EA	6	\$200	\$1,200
12	152351 Relocate Hydrant	EA	6	\$4,500	\$27,000
13	152390 Relocate Roadside Sign	EA	11	\$225	\$2,475
14	153130 Remove Concrete Curb	LF	150	\$4	\$525
15	153211 Remove Concrete Sidewalk & Driveway	CY	4,000	\$5	\$20,000
16	160102 Clearing & Grubbing	LS	1	\$75,000	\$75,000
17	190101 Roadway Excavation	CY	4,440	\$25	\$111,000
18	190140 Trench Excavation	CY	300	\$15	\$4,500
19	194001 Ditch Excavation	CY	1,500	\$35	\$52,500
20	260202 Class 1 Aggregate Base	CY	93	\$45	\$4,185
21	260203 Class 2 Aggregate Base	CY	1,990	\$40	\$79,600
22	390133 Hot Mix Asphalt (Type A)	TON	3,860	\$120	\$463,200
23	394090 Place Hot Mix Asphalt (Miscellaneous Areas)	SF	2,000	\$15	\$30,000
24	510090-M Structural Concrete, Open Bottom Culvert	EA	1	\$100,000	\$100,000
25	510092 Structural Concrete, Headwall	CY	50	\$1,750	\$87,500
26	510502 Minor Concrete (Minor Structure)	CY	120	\$1,500	\$180,000
27	566011 Roadside Sign - One Post	EA	85	\$200	\$17,000
28	568017 Roadside Sign - Other	EA	12	\$75	\$900
29	66572 Roadside Sign Panel	SF	660	\$40	\$26,400
30	15" x 21" Elliptical Reinforced Concrete Pipe	LF	93	\$130	\$12,090
31	650014 18" Reinforced Concrete Pipe	LF	1,827	\$75	\$137,025

PRELIMINARY PLAN ESTIMATE

CITY OF GOLETA

DISTRICT 5

CO SBA

PROJ. NAME Ekwill Street Extension
LIMITS Ekwill Street
DESIGNER MNS Engineers, Inc.

QUANTITIES BY TJW
PRICED BY SMK
COST INDEX Caltrans

DATE 8/14/2013
DATE 1/25/2013
DATE 2007-2012

BEES NO.	CONTRACT ITEMS	UNIT	QUANTITY	PRICE	AMOUNT
32	650036 51" Reinforced Concrete Pipe	LF	58	\$100	\$5,800
33	750007 Frame & Grate	EA	5	\$425	\$2,125
34	750010 Manhole Frame & Cover	EA	7	\$950	\$6,650
35	800360 Chain Link Fence (Type CL-6)	LF	400	\$20	\$8,000
36	839521 Cable Railing (Modified)	LF	200	\$20	\$4,000
37	840656 Paint Traffic Stripe (2-Coat)	LF	12568	\$1	\$12,568
38	840515 Paint Pavement Markings (Thermoplastic)	SF	1522	\$3	\$3,805
39	850111 Pavement Marker (Retroreflective)	EA	13	\$4	\$52
40	869001 Miscellaneous Electrical	LS	1	\$67,050	\$135,650
41	66882 Light Standard and Foundation	LS	1	\$376,605	\$462,135

SUBTOTAL	\$2,195,035
MOBILIZATION (999990) (@ 5%)	\$109,752
SUBTOTAL	\$2,304,787
CONTINGENCIES (@ 15%)	\$345,718
TOTAL COST	\$2,650,505
<u>WORK BY RAILROAD OR UTILITY FORCES</u>	<u>\$0</u>
GRAND TOTAL	\$2,650,505
FOR BUDGET PURPOSES - SAY	\$2,660,000

COMMENTS: Cost is based upon historical data (typically 2012-2007 Caltrans Contract Cost Data). Future estimates may vary. Utility work is to be determined.



FOWLER ROAD EXTENSION PROJECT
CITY OF GOLETA
 65% SUBMITTAL CONSTRUCTION COST ESTIMATE

Item No.	BID ITEM No.	BID ITEM DESCRIPTION	UNIT OF MEASURE	ESTIMATED QUANTITY	UNIT PRICE	AMOUNT
1	074017	PREPARE WATER POLLUTION CONTROL PLAN	LS	1	\$ 77,000.00	\$ 77,000.00
2	074019	PREPARE STORM WATER POLLUTION PREVENTION PLAN	LS	1	\$ 10,000.00	\$ 10,000.00
3	074028	TEMPORARY FIBER ROLL	LF	1,458	\$ 3.00	\$ 4,374.00
4	074029	TEMPORARY SILT FENCE	LF	2,167	\$ 4.00	\$ 8,668.00
5	074032	TEMPORARY CONCRETE WASHOUT	EA	1	\$ 2,000.00	\$ 2,000.00
6	074033	TEMPORARY CONSTRUCTION ENTRANCE	EA	1	\$ 2,500.00	\$ 2,500.00
7	074038	TEMPORARY DRAINAGE INLET PROTECTION	EA	13	\$ 200.00	\$ 2,600.00
8	120090	CONSTRUCTION AREA SIGNS	LS	1	\$ 10,000.00	\$ 10,000.00
9	120100	TRAFFIC CONTROL SYSTEM	LS	1	\$ 50,000.00	\$ 50,000.00
10	120120	TYPE III BARRICADE	EA	11	\$ 120.00	\$ 1,320.00
11	120150	TEMPORARY PAVEMENT MARKING	SQFT	200	\$ 3.00	\$ 600.00
12	120161	TEMPORARY TRAFFIC STRIPE	LF	9,820	\$ 1.00	\$ 9,820.00
13	120165	CHANNELIZER (SURFACE MOUNTED)	EA	150	\$ 40.00	\$ 6,000.00
14	129110	TEMPORARY CRASH CUSHION	EA	1	\$ 2,500.00	\$ 2,500.00
15	150608	REMOVE CHAIN LINK FENCE	LF	1,524	\$ 4.00	\$ 6,096.00
16	150620	REMOVE GATE	EA	4	\$ 300.00	\$ 1,200.00
17	150742	REMOVE ROADSIDE SIGN	EA	16	\$ 125.00	\$ 2,000.00
18	150768	REMOVE ASPHALT CONCRETE PAVEMENT	CY	341	\$ 30.00	\$ 10,230.00
19	150860	REMOVE BASE AND SURFACING	CY	2,410	\$ 15.00	\$ 36,150.00
20	152351	RELOCATE HYDRANT	EA	1	\$ 4,500.00	\$ 4,500.00
21	152386	RELOCATE ROADSIDE SIGN-ONE POST	EA	5	\$ 200.00	\$ 1,000.00
22	152401	ADJUST WATER METER TO GRADE	EA	5	\$ 300.00	\$ 1,500.00
23	152402	ADJUST WATER VALVE TO GRADE	EA	3	\$ 500.00	\$ 1,500.00
24	152440	ADJUST MANHOLE TO GRADE	EA	11	\$ 1,000.00	\$ 11,000.00
25	153121	REMOVE CONCRETE	CY	68	\$ 110.00	\$ 7,480.00
26	153240	REMOVE CONCRETE (CURB, GUTTER, AND SIDEWALK)	CY	130	\$ 110.00	\$ 14,300.00
27	160101	CLEARING AND GRUBBING	LS	1	\$ 50,000.00	\$ 50,000.00
28	160120	REMOVE TREE	EA	28	\$ 1,000.00	\$ 28,000.00
29	190101	ROADWAY EXCAVATION	CY	3,600	\$ 40.00	\$ 144,000.00
30	198010	IMPORTED BORROW	CY	6,100	\$ 50.00	\$ 305,000.00
31	203XXX	EROSION CONTROL	LS	1	\$ 10,000.00	\$ 10,000.00
32	204XXX	LANDSCAPING	LS	1	\$ 140,000.00	\$ 140,000.00
33	208000	IRRIGATION SYSTEM	LS	1	\$ 80,000.00	\$ 80,000.00
34	260203	CLASS 2 AGGREGATE BASE	CY	4,554	\$ 40.00	\$ 182,160.00
35	390132	HOT MIX ASPHALT (TYPE A)	TON	4,300	\$ 100.00	\$ 430,000.00
36	510050	STRUCTURAL CONCRETE (ARCH CULVERT)	LS	1	\$ 300,000.00	\$ 300,000.00
37	566011	ROADSIDE SIGN- ONE POST	EA	36	\$ 275.00	\$ 9,900.00
38	650014	18" REINFORCED CONCRETE PIPE	LF	273	\$ 100.00	\$ 27,300.00
39	650026	36" REINFORCED CONCRETE PIPE	LF	573	\$ 200.00	\$ 114,600.00
40	650XXX	DRAINAGE INLET	EA	12	\$ 4,000.00	\$ 48,000.00
41	650XXX	SD MANHOLE	EA	3	\$ 4,000.00	\$ 12,000.00
42	730010	MINOR CONCRETE (CURB)	LF	960	\$ 15.00	\$ 14,400.00
43	730010	MINOR CONCRETE (MOUNTABLE CURB)	LF	276	\$ 25.00	\$ 6,900.00
44	730070	DETECTABLE WARNING SURFACE	EA	20	\$ 600.00	\$ 12,000.00
45	731504	MINOR CONCRETE (CURB AND GUTTER)	LF	3,853	\$ 25.00	\$ 96,325.00
46	731509	MINOR CONCRETE (CONCRETE MOW STRIP)	CY	2	\$ 150.00	\$ 300.00
47	731511	MINOR CONCRETE (TRUCK APRON)	CY	49	\$ 800.00	\$ 39,200.00
48	731516	MINOR CONCRETE (DRIVEWAY)	CY	77	\$ 500.00	\$ 38,500.00
49	731521	MINOR CONCRETE (SIDEWALK)	CY	236	\$ 400.00	\$ 94,400.00

Item No.	BID ITEM No.	BID ITEM DESCRIPTION	UNIT OF MEASURE	ESTIMATED QUANTITY	UNIT PRICE	AMOUNT
50	731623	MINOR CONCRETE (CURB RAMP)	CY	31	\$ 600.00	\$ 18,600.00
51	800360	CHAIN LINK FENCE (TYPE CL-6)	LF	1,520	\$ 20.00	\$ 30,400.00
52	802670	24' CHAIN LINK GATE (TYPE CL-6)	EA	3	\$ 2,500.00	\$ 7,500.00
53	840501	THERMOPLASTIC TRAFFIC STRIPE	LF	7,553	\$ 1.50	\$ 11,329.50
54	840515	THERMOPLASTIC PAVEMENT MARKING	SQFT	865	\$ 3.50	\$ 3,027.50
55	860402	LIGHTING (CITY STREET)	LS	1	\$ 75,000.00	\$ 75,000.00
						\$ 2,613,180
						\$ 261,318
						\$ 2,874,498
						\$ 574,900
						\$ 3,449,398
FOR ESTIMATING PURPOSES						\$ 3,450,000



**HOLLISTER AVENUE / SR-217 INTERCHANGE MODIFICATIONS
(INCLUDING KELLOGG AVENUE WIDENING)
CITY OF GOLETA
65% SUBMITTAL CONSTRUCTION COST ESTIMATE**

Item No.	BID ITEM No.	BID ITEM DESCRIPTION	UNIT OF MEASURE	ESTIMATED QUANTITY	UNIT PRICE	AMOUNT
1	074017	PREPARE WATER POLLUTION CONTROL PLAN	LS	1	\$ 135,000.00	\$ 135,000.00
2	074019	PREPARE STORM WATER POLLUTION PREVENTION PLAN	LS	1	\$ 10,000.00	\$ 10,000.00
3	120090	CONSTRUCTION AREA SIGNS	LS	1	\$ 50,000.00	\$ 50,000.00
4	120100	TRAFFIC CONTROL SYSTEM	LS	1	\$ 150,000.00	\$ 150,000.00
5	120120	TYPE III BARRICADE	EA	2	\$ 120.00	\$ 240.00
6	120150	TEMPORARY PAVEMENT MARKING	SQFT	1800	\$ 3.00	\$ 5,400.00
7	120161	TEMPORARY TRAFFIC STRIPE	LF	21700	\$ 1.00	\$ 21,700.00
8	120165	CHANNELIZER (SURFACE MOUNTED)	EA	300	\$ 40.00	\$ 12,000.00
9	129000	TEMPORARY RAILING (TYPE K)	LF	2100	\$ 30.00	\$ 63,000.00
10	129110	TEMPORARY CRASH CUSHION	EA	6	\$ 2,500.00	\$ 15,000.00
11	150XXX	REMOVE LIGHTING	EA	7	\$ 1,000.00	\$ 7,000.00
12	150XXX	REMOVE SIGNAL EQUIPMENT (LOCATION 1)	LS	1	\$ 50,000.00	\$ 50,000.00
13	150XXX	REMOVE SIGNAL EQUIPMENT (LOCATION 2)	LS	1	\$ 70,000.00	\$ 70,000.00
14	150604	REMOVE WOOD FENCE	LF	60	\$ 15.00	\$ 900.00
15	150608	REMOVE CHAIN LINK FENCE	LF	800	\$ 4.00	\$ 3,200.00
16	150620	REMOVE GATE	EA	1	\$ 125.00	\$ 125.00
17	150742	REMOVE ROADSIDE SIGN	EA	43	\$ 200.00	\$ 8,600.00
18	150771	REMOVE ASPHALT CONCRETE DIKE	LF	1450	\$ 10.00	\$ 14,500.00
19	150809	REMOVE CULVERT	LF	770	\$ 30.00	\$ 23,100.00
20	150820	REMOVE INLET	EA	9	\$ 1,000.00	\$ 9,000.00
21	150826	REMOVE MANHOLE	EA	2	\$ 850.00	\$ 1,700.00
22	150860	REMOVE BASE AND SURFACING	CY	1800	\$ 15.00	\$ 27,000.00
23	152XXX	ADJUST GAS VALVE TO GRADE	EA	1	\$ 500.00	\$ 500.00
24	152XXX	RELOCATE BENCH	EA	1	\$ 600.00	\$ 600.00
25	152XXX	RELOCATE NEWS- STAND	EA	3	\$ 500.00	\$ 1,500.00
26	152XXX	RELOCATE FLAGPOLE	LS	1	\$ 500.00	\$ 500.00
27	152XXX	RELOCATE PULL BOX	EA	10	\$ 300.00	\$ 3,000.00
28	152XXX	RELOCATE UTILITY VAULT	EA	8	\$ 800.00	\$ 6,400.00
29	152370	RELOCATE MAILBOX	EA	1	\$ 300.00	\$ 300.00
30	152390	RELOCATE ROADSIDE SIGN	EA	30	\$ 200.00	\$ 6,000.00
31	152401	ADJUST WATER METER TO GRADE	EA	4	\$ 300.00	\$ 1,200.00
32	152402	ADJUST WATER VALVE TO GRADE	EA	7	\$ 500.00	\$ 3,500.00
33	152410	RELOCATE WATER METER	EA	4	\$ 3,000.00	\$ 12,000.00
34	152440	ADJUST MANHOLE TO GRADE	EA	7	\$ 1,000.00	\$ 7,000.00
35	152469	ADJUST UTILITY COVER TO GRADE	EA	1	\$ 700.00	\$ 700.00
36	152604	MODIFY INLET	EA	1	\$ 2,000.00	\$ 2,000.00
37	152610	MODIFY MANHOLE	EA	1	\$ 1,000.00	\$ 1,000.00
38	153103	COLD PLANE ASPHALT CONCRETE PAVEMENT	SQYD	10300	\$ 5.00	\$ 51,500.00
39	153130	REMOVE CONCRETE CURB	LF	2300	\$ 15.00	\$ 34,500.00
40	153142	REMOVE CONCRETE ISLAND	CY	180	\$ 200.00	\$ 36,000.00
41	153220	REMOVE CONCRETE (CHANNEL)	CY	110	\$ 90.00	\$ 9,900.00
42	153240	REMOVE CONCRETE (CURB, GUTTER, AND SIDEWALK)	CY	1000	\$ 110.00	\$ 110,000.00
43	153247	REMOVE CONCRETE (MISCELLANEOUS)	CY	3	\$ 110.00	\$ 330.00
44	160101	CLEARING AND GRUBBING	LS	1	\$ 100,000.00	\$ 100,000.00
45	160120	REMOVE TREE	EA	56	\$ 1,200.00	\$ 67,200.00
46	190101	ROADWAY EXCAVATION	CY	18300	\$ 40.00	\$ 732,000.00
47	203XXX	EROSION CONTROL	LS	1	\$ 20,000.00	\$ 20,000.00
48	204XXX	LANDSCAPING	LS	1	\$ 285,000.00	\$ 285,000.00

Item No.	BID ITEM No.	BID ITEM DESCRIPTION	UNIT OF MEASURE	ESTIMATED QUANTITY	UNIT PRICE	AMOUNT
49	208000	IRRIGATION SYSTEM	LS	1	\$ 85,000.00	\$ 85,000.00
50	260203	CLASS 2 AGGREGATE BASE	CY	7000	\$ 40.00	\$ 280,000.00
51	377501	SLURRY SEAL	TON	55	\$ 300.00	\$ 16,500.00
52	390132	HOT MIX ASPHALT (TYPE A)	TON	6100	\$ 100.00	\$ 610,000.00
53	393004	GEOSYNTHETIC PAVEMENT INTERLAYER (PAVING FABRIC)	SQYD	1500	\$ 2.00	\$ 3,000.00
54	394073	PLACE HOT MIX ASPHALT DIKE (TYPE E)	LF	700	\$ 5.00	\$ 3,500.00
55	510060	STRUCTURAL CONCRETE (RETAINING WALL)	LS	1	\$ 1,000,000.00	\$ 1,000,000.00
56	566011	ROADSIDE SIGN-ONE POST	EA	60	\$ 275.00	\$ 16,500.00
57	650014	18" REINFORCED CONCRETE PIPE	LF	660	\$ 100.00	\$ 66,000.00
58	650022	30" REINFORCED CONCRETE PIPE	LF	335	\$ 150.00	\$ 50,250.00
59	650034	48" REINFORCED CONCRETE PIPE	LF	130	\$ 200.00	\$ 26,000.00
60	650XXX	DROP INLET	EA	11	\$ 4,000.00	\$ 44,000.00
61	650XXX	SD MANHOLE	EA	9	\$ 4,000.00	\$ 36,000.00
62	730010	MINOR CONCRETE (CURB)	LF	4780	\$ 15.00	\$ 71,700.00
63	730010	MINOR CONCRETE (MOUNTABLE CURB)	LF	800	\$ 25.00	\$ 20,000.00
64	730045	MINOR CONCRETE (GUTTER)	CY	7	\$ 750.00	\$ 5,250.00
65	730070	DETECTABLE WARNING SURFACE	EA	28	\$ 600.00	\$ 16,800.00
66	731504	MINOR CONCRETE (CURB AND GUTTER)	LF	4500	\$ 25.00	\$ 112,500.00
67	731511	MINOR CONCRETE (TRUCK APRON)	CY	140	\$ 800.00	\$ 112,000.00
68	731516	MINOR CONCRETE (DRIVEWAY)	CY	55	\$ 500.00	\$ 27,500.00
69	731521	MINOR CONCRETE (SIDEWALK)	CY	330	\$ 400.00	\$ 132,000.00
70	731623	MINOR CONCRETE (CURB RAMP)	CY	55	\$ 600.00	\$ 33,000.00
71	840501	THERMOPLASTIC TRAFFIC STRIPE	LF	18400	\$ 1.50	\$ 27,600.00
72	840515	THERMOPLASTIC PAVEMENT MARKING	SQFT	2650	\$ 3.50	\$ 9,275.00
73	860402	LIGHTING (CITY STREET)	LS	1	\$ 75,000.00	\$ 75,000.00
74	861XXX	REMOVE AND SALVAGE TRAFFIC SIGNAL AND LIGHTING	LS	2	\$ 7,500.00	\$ 15,000.00
						\$ 5,064,470
						\$ 506,447
						\$ 5,570,917
						\$ 1,114,183
						\$ 6,685,100
FOR ESTIMATING PURPOSES						\$ 6,700,000

5/13/2014

	Hollister/217	Ekwill Street	Fowler Road	Subtotals
Overall Project Area (SF)	333,442	305,997	164,299	
Area of Ped/Bike Facilities (SF)	37,219	68,317	44,213	
	11.2%	22.3%	26.9%	60.4%

	Subtotals (Bold by Phase)			ATP Funds	City Other Funds	Funds	
Total Project Construction Cost Estimate	\$6,700,000	\$2,660,000	\$3,450,000	\$12,810,000			
Construction for Roadway	\$5,952,140	\$2,066,120	\$2,521,600	\$10,539,860		\$10,539,860	Subtotal STIP
Construction Cost for Ped/Bike Facilities	\$747,860	\$593,880	\$928,400	\$2,270,140			Subtotal Bike/ Ped
			11.47%	\$260,385	\$260,385		City (GTIP) match
			88.53%	\$2,009,755	\$2,009,755		ATP
			Check	\$2,270,140.00			check
Construction Management				\$1,577,200		\$1,577,200	City
Total Construction + CM				\$14,387,200			
E&P (PA&ED)				\$2,022,000		\$2,022,000	City (RIP-State Cash)
PS&E Phase				\$2,053,053		\$2,053,053	City (RIP- NH, GTIP)
Right of Way - Costs	\$659,000	\$3,297,900	\$1,092,000	\$5,048,900			City (RIP- NH)
Right of Way - Support				\$360,107			City (RIP- NH)
Total Project Right of Way Costs				\$5,409,007		\$5,409,007	City (RIP- NH)
Other Project Funds - Subtotal							construction-Bike/ Ped+ROW
Total Project Funds				\$23,871,260	\$2,270,140	\$21,601,120	
						\$23,871,260	check

ATTACHMENT E

ATTACHMENT F



2040

REGIONAL TRANSPORTATION PLAN and Sustainable Communities Strategy



FINAL

2040 Regional Transportation Plan & Sustainable Communities Strategy

Adopted August 15, 2013

Prepared by:

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E.2 PROGRAMMED PROJECTS LIST - OTHER PROJECTS

Cost figures are in thousands of dollars.

												YOE \$
Project Type	Subtype	2040 RTP ID#	2013 FTIP #	Project Status (12/2011)	Route #	Project Title	Project Description	Project Purpose (Strategy)	Regional Benefits	Planning Doc / Funding Source	Year Operational	Total (\$000)
ITS		CT-500		Design	101	Flashing Beacons and Bicycle Detection	Project located along Route 101 in SLO County at Wellsona Rd. as well as in Santa Barbara County at Gaviota Tunnel and Arroyo Quemado Bridge.	Bicycle Safety	Santa Barbara/San Luis Obispo Counties	SHOPP, Minor B	2012	\$240
RAIL		CT-700	CT21	Environmental		Santa Barbara County Rail Siding Project (Ortega) (LOSSAN #SB-11)	Create new sections of double track in Santa Barbara County based on modeling results, engineering feasibility and environmental constraints. From MP 380.7 to MP 273.2, Pacific Surfliner route.	Inadequate passing opportunities, Improve on-time performance.	South Coast	2010 STIP	2017	\$14,450
RAIL		CT-701	CT22	Environmental		VEN - SB Rail Siding Project (Seacliff)	From MP 423.0 to MP 368.6, Pacific Surfliner route, complete environmental clearance, design, engineering and permitting for siding projects in Santa Barbara and Ventura Counties.	Inadequate passing opportunities, Improve on-time performance.	South Coast	2010 STIP	2017	\$9,870
Local Projects-Variou												
HWY	Maintenance & Rehabilitation	Var-101	LOCALHSIP	Ongoing		Highway Safety Improvement Program	Highway Safety Improvement Program (Note that HSIP replaces HES)	Reduce the frequency and severity of collisions on all public roads.	Countywide	HSIP, Local	Ongoing	\$5,775
ST/RDS	Maintenance & Rehabilitation	Var-200		Ongoing		System-Level Maintenance & Rehabilitation	Maintenance of city streets and county roads including patching, overlay, and sealing; maintenance of street lights and traffic signals; and snow removal and storm damage repair		Countywide		Ongoing	\$143,589
ST/RDS	Maintenance & Rehabilitation	Var-201	CT14	Ongoing		Lump Sum - Local Agency - HBP and Seismic Project	Local HBP Lump Sum	HBP and Seismic (Non-capacity increasing projects only (includes seismic retrofit). Projects are consistent with 40 CFR part 93.126, 127, 128, exempt tables 2 & 3)	Countywide	HBP, Local Bridge Seismic Retrofit Account, Local	Ongoing	\$113,073
ST/RDS	Maintenance & Rehabilitation	Var-202	LOCAL HR3	Ongoing		High Risk Rural Roads Program	Improvements of site distance, roadway width, super elevation change to alignment and traffic information signs. Projects are consistent with the applicable classifications under 23 CFR 771.117(c) and (d).	Reduce the number of vehicular traffic incidents	Countywide	HR3, Local	Ongoing	\$4,126
BIKE/PED		Var-300	SRTS	Ongoing		Safe Routes To School	Infrastructure-related projects that are consistent with the applicable classifications under 23 CFR 771.117(c) and (d).	Improve the ability of students to walk and bicycle to school	Countywide	SRTS	Ongoing	\$3,742
TRANSIT	Operations	Var-400	VAR01			Volunteer Driver Program	Community Partners in Caring driver program for seniors who are unable to access public transportation		North County	FTA 5317 (New Freedom), Loc Funds - Private Funds	Ongoing	\$605
TDM		Var-600	VAR14			CalVans Voucher Program	Operating Assistance to continue marketing of the CalVans Voucher Program to those not aware of the transportation options provided by the CalVans. The vouchers provide a 50% reduction in the actual cost for the rider.		Countywide	FTA 5316 (JARC)	2013	\$50
City of Carpinteria												
ST/RDS	Construction	C-200				Carpinteria Avenue Bridge Replacement Project	Replace the existing bridge. Does not increase transportation related capacity	Replaces a structurally deficient hydraulically obsolete roadway bridge	Carpinteria	Measure A and HBP	2014	\$10,000
City of Goleta												
HWY	Construction	Go-100		ROW and Final design	101	Los Carneros Overhead Replacement Project	Location: Hwy 101 and Los Carneros Rd Interchange. Widen approach to SB ramp and replace RR bridge.	Excessive delays, forecasted low LOS (E).	Goleta	General Plan, GTIP/ HBP, Developer Impact Fees	2013	\$9,020
HWY	Construction	Go-101				Storke Road Widening	Location: Storke Rd from Whittier to City limits. Widen roadway.	Forecasted low LOS, ADT projected to exceed capacity in 2007.	Goleta	General Plan, GTIP/Measure A, Local, Developer Impact Fees	2015	\$2,950

E.2 PROGRAMMED PROJECTS LIST - OTHER PROJECTS

Cost figures are in thousands of dollars.

Project Type	Subtype	2040 RTP ID#	2013 FTIP #	Project Status (12/2011)	Route #	Project Title	Project Description	Project Purpose (Strategy)	Regional Benefits	Planning Doc / Funding Source	Year Operational	YOE \$
												Total (\$000)
HWY	Construction	Go-103	GOLETA18		101	Highway 101/Cathedral Oaks Landscaping Enhancement	Design and install landscaping at the Highway 101/Cathedral Oaks Interchange			RSTP, STIP	2012	\$351
ST/RDS	Construction	Go-200	GOLETA08	ROW and Final Design		Fowler & Ekwil / Fairview / Kellogg / Rt217	Local road improvements & interchange modifications at Ekwil and Fowler Rds. Construct new east-west roadways & extend Fowler Rd and Ekwil St from Fairview Ave on the west to Kellogg Ave & Route 217 on the east.	Provides alternate east/west route to improve operation of Hollister and reduce congestion.	Goleta	General Plan, GTIP, Old Town Revitalization Plan/STIP-RIP	2015	\$17,766
ST/RDS	Construction	Go-201		Design		Hollister Redesign	Modify Hollister Ave left turn channelization and raise medians in Old Town area.	Low LOS. Improve pedestrian access.	Goleta	General Plan, GTIP/STIP, Local Measure D, Developer Impact Fees, RDA	2017	\$14,084
ST/RDS	Construction	Go-202		Preliminary Engineering		Hollister Avenue Bridge Replacement	Remove existing bridge over San Jose Creek and replace with a new, wider bridge with greater hydraulic capacity. Additional width for sidewalks and bike lanes.	The existing bridge was constructed with reactive aggregate and has a limited life span. The bridge needs to be replaced before load restrictions are imposed which would greatly impact commuters and businesses from both Goleta and Santa Barbara. Hollister is the only parallel route to Hwy 101 connecting Goleta and Santa Barbara without interruption.	Goleta Valley	General Plan (San Jose Creek)/ HBP, Measure A, local funds: (SR68.6 and FO)	2015	\$4,875
ST/RDS	Construction	Go-203		Design		Los Carneros at Calle Real Roundabout	Install a roundabout at the intersection of Los Carneros and Calle Real to replace the three way STOP controls.	The intersection is currently STOP controlled in all directions. The minor leg (Calle Real) used to be the only leg with a STOP control, until a spike in broadside collisions was noted. The three-way STOP is an interim measure. The proximity to the Los Carneros Interchange with its signalized ramps makes installing traffic signals at the intersection of Los Carneros and Calle Real infeasible. There is ample right of way to construct a roundabout at this location and it is the optimal operational solution.	Goleta	General Plan, GTIP/SLPP, Developer Impact Fees, Measure A	2012	\$995
ST/RDS	Construction	Go-204				Hollister Avenue Capacity Improvement Project	Project will construct an additional lane on the Hollister Avenue WB from Storke Road to Marketplace.	Installation of this additional lane is necessary to accommodate planned development.	Goleta Valley	General Plan, GTIP/ Developer Impacts fees, local funds	2013	\$675
ST/RDS	Construction	Go-205				Patterson Avenue/Hwy 101 Interchange NB and SB Ramp Modifications	Widen or replace existing overcrossing and overhead to accommodate additional turn lanes and Class II bike lanes. Additional left turn onto SB ramp, additional right turn onto SB ramp, additional right turn onto the NB ramp. Signal modifications as necessary to accommodate peds and bikes.	Overall traffic volumes and turning movement volumes are expected to increase and these extensive modifications are necessary to accommodate planned development.	Goleta Valley	General Plan, GTIP/ Developer impact fees, local funds	2018	\$6,800
ST/RDS	Construction	Go-206				Hollister Avenue/Patterson Avenue Operational Improvements	Add a WB Hollister Avenue free right turn lane onto NB Patterson Avenue; Widen Hollister on the south side as necessary; install a new traffic signal at the Hollister Avenue/Patterson Avenue intersection.	Overall traffic volumes and turning movement volumes are expected to increase and these extensive modifications are necessary to accommodate planned development.	Goleta Valley	General Plan, GTIP/ Developer impact fees	2016	\$1,277
ST/RDS	Construction	Go-207				Los Carneros Road Capacity Improvements	Widen Los Carneros Road to Four Lanes from Hollister Avenue to the City Limits	Additional lanes are necessary to accommodate planned development	Goleta Valley	General Plan, GTIP/ Developer impact fees	2014	\$3,580

E.2 PROGRAMMED PROJECTS LIST - OTHER PROJECTS

Cost figures are in thousands of dollars.

Project Type	Subtype	2040 RTP ID#	2013 FTIP #	Project Status (12/2011)	Route #	Project Title	Project Description	Project Purpose (Strategy)	Regional Benefits	Planning Doc / Funding Source	Year Operational	YOE \$
												Total (\$000)
ST/RDS	Construction	Go-208				Los Carneros & Hollister Operational Improvements	Add a double left turn from Hollister to SB Los Carneros; median modifications on Hollister east of Los Carneros; minor traffic signal modification at Hollister & Los Carneros	Intersection modifications necessary to support ongoing development	Goleta Valley	General Plan, GTIP/ Developer impact fees	2014	\$765
ST/RDS	Construction	Go-209				Los Carneros/Calle Koral Operational Improvements	Additional NB through lane on Los Carneros and signal modifications to allow for construction of the western leg of the intersection.	Additional lanes are necessary to accommodate planned development	Goleta Valley	General Plan, GTIP/ Developer impact fees	2015	\$345
ST/RDS	Construction	Go-210				Storke Road Capacity Improvements Hollister to 101	Install additional NB lane on Storke Road from Hollister to the SB on-ramp. Ramp intersection modifications as necessary.	Lane addition and interchange modifications necessary to support planned development	Goleta Valley	General Plan, GTIP/ Developer impact fees	2014	\$760
ST/RDS	Construction	Go-211				Los Carneros Way Realignment Project	Realign Los Carneros Way to meet Calle Koral approximately 400 ft. southeast of its current location. This will allow for improved operations and safety while handling increased traffic volumes.	Improved operations and safety while proving increased roadway LOS though buildout.	Goleta	General Plan, GTIP/ Developer impact fees	2015	\$400
ST/RDS	Construction	Go-212				Fairview Avenue and Stow Canyon Road Traffic Signal Installation	Install traffic signals at the intersection of Fairview Avenue and Stow Canyon Road; install frontage improvements on east side of Fairview to accommodate bikes/peds	Traffic signals will be necessary to accommodate planned growth.	Goleta	General Plan, GTIP/ Developer impact fees	2016	\$275
ST/RDS	Construction	Go-213				Fairview Avenue and Calle Real Intersection Operational Improvement Project	Add additional eastbound through lane on Calle Real in western leg of intersection by adding separate turn pocket. Add additional northbound left turn lane on Fairview Avenue in the southern leg of the intersection to accommodate double left turns. Add Class II bike lanes in both directions on Fairview Avenue in southern leg.	Modifications are necessary to maintain acceptable LOS at this intersection.	Goleta Valley	General Plan, GTIP/ Developer impact fees	2017	\$1,940
ST/RDS	Construction	Go-214				Fairview Avenue/Hwy 101 Interchange SB Ramp Modifications	Ramp and intersection improvements to allow for right turn lane; install bike activated signal at the SB on-ramp.	Modifications are necessary to maintain acceptable LOS at this intersection, and to allow for a better walking and biking experience. Modifications will encourage walking and cycling.	Goleta Valley	General Plan, GTIP/ Developer impact fees	2019	\$4,900
ST/RDS	Construction	Go-215				Fairview Avenue/Hwy 101 Interchange NB Ramp Modifications	Add WB though lane on the NB off-ramp to SB Fairview Avenue. Replace traffic signals at intersection and relocate standards. Add additional right turn lane from NB Fairview Avenue at NB 101 on-ramp.	Modifications are necessary to maintain acceptable LOS at this intersection, and to allow for a better walking and biking experience. Modifications will encourage walking and cycling.	Goleta Valley	General Plan, GTIP/ Developer impact fees	2019	\$3,980
ST/RDS	Construction	Go-216				Phelps Road Extension	Phelps Rd, Los Carneros to Storke. Circulation improvements. LOS improvements for Storke/Hollister, El Colegio/Los Carneros and Los Carneros/Hollister Intersections. (County/UCSB/City of Goleta joint project)	Improve circulation, LOS.	Goleta, Isla Vista, UCSB	General Plan, GTIP/ Measure A, Local, Developer Impact Fees, UCSB	2025	\$544
ST/RDS	Construction	Go-217				South Fairview Improvements	Location: Fairview Ave, Hollister Ave to Fowler Rd. Construct class II bike lanes, landscaped raised medians, and vehicle capacity modifications.	Forecasted low LOS. Improve circulation around Hollister corridor.	Goleta	General Plan, BTP/ Measure A, Local	2016	\$1,480
BIKE/PED		Go-300		Preliminary Engineering		San Jose Creek Bikeway-Middle Segment	Construct class I bike path from north of Calle Real to Hollister.	Bicycle access (may be) under highway, regional bikeway segment. Improves cross-town bicycle circulation. Links access north of Highway 101 to UCSB and the beach.	Goleta Valley	General Plan, Old Town Revitalization Plan, BTP/RSTP	2015	\$300
BIKE/PED		Go-302				Goleta Sidewalk Infill Project	Install 5,000 linear feet of curb, gutter and sidewalk at various locations in the City.	Project will encourage walking, improve safety and drainage.	Goleta	General Plan - Maintenance/ Measure A, local funds	ongoing	\$280

E.2 PROGRAMMED PROJECTS LIST - OTHER PROJECTS

Cost figures are in thousands of dollars.

Project Type	Subtype	2040 RTP ID#	2013 FTIP #	Project Status (12/2011)	Route #	Project Title	Project Description	Project Purpose (Strategy)	Regional Benefits	Planning Doc / Funding Source	Year Operational	YOE \$
												Total (\$000)
BIKE/PED		Go-303				Cathedral Oaks Class I Bike Path	Construct a Class I bike path on Cathedral Oaks from Glen Annie to La Patera, 1.63 miles	This Class I bike path will encourage bicycling and improve safety for cyclists of all skill levels.	Goleta Valley	General Plan, BTP, GTIP/ Developer Impact fees, Measure A, local funds	2020	\$1,290
BIKE/PED		Go-304				Patterson Avenue Class II Bike Lanes	Construct Class II bike lanes in both directions on Patterson Avenue between Hollister Avenue and the City Limits.	While this project is less than a mile in length, it will increase the connectivity and improve safety for cyclists, thereby encouraging cycling by persons of less than top skill levels.	Goleta Valley	General Plan, BTP, GTIP/ Developer Impact fees, Measure A, local funds	2018	\$129
BIKE/PED		Go-305				Hollister Class I Bikeway	Construct class 1 bike path, Pacific Oaks-Ellwood Elementary.	Safety	Goleta Valley	General Plan, BTP/Measure A, Local	2015	\$606
BIKE/PED		Go-306				La Patera Overcrossing	Location: Goleta Old Town Calle Real. Construct new pedestrian overcrossing.	Bike/pedestrian access over Hwy 101.	Goleta	General Plan, BTP, GTIP/ RTIP/STIP, Regional Measure A, Local funds	2025	\$36,000
BIKE/PED		Go-307				Bikeway infill project	Location: A. La Patera, railroad to Hollister Ave, B. Hollister Ave, Kellogg to Maria Ignacio, C. Patterson Ave, Hollister to Atascadero Creek, D. Patterson Ave, Pacific Oaks. Construct A. Class II, B. Class I, C. Class I, D. Class II bike path.	Improve bicycle circulation, connectivity filling missing segment of bikeway.	Goleta Valley	General Plan, BTP/Measure A, Local	2016	\$850
City of Guadalupe												
TRANSIT	Operations	Gu-400	GUAD4			Operating Assistance for Guadalupe Transit	Transit Operating Assistance for Guadalupe Transit	Provide operating assistance for Guadalupe Transit	Guadalupe/ Santa Maria Valley	FTA 5311 (Rural & Small Transit), TDA	Ongoing	\$656
TRANSIT	Capital	Gu-401	GUAD11			City of Guadalupe Hybrid Vehicle	Project will purchase a hybrid vehicle for City of Guadalupe.		Guadalupe/ Santa Maria Valley	Demo-LU, Local	2012	\$38
City of Lompoc												
TRANSIT	Operations	COLT-400	LOMPOC9			Operating Assistance for COLT	Transit Operating Assistance for COLT	Provide operating assistance for COLT	Lompoc Valley	FTA 5307 (Urban Area Formula), TDA	Ongoing	\$12,022
TRANSIT	Operations	COLT-401	LOMPOC13			Operating Assistance for Wine Country Express	Transit Operating Assistance for Wine Country Express	Provide operating assistance for Wine Country Express	Lompoc Valley, Santa Ynez Valle	FTA 5316 (JARC), TDA	Ongoing	\$469
TRANSIT	Capital	COLT-402	LOMPOC8			Bus Stops and Shelters	Bus stops and shelters - City of Lompoc Transit.		Lompoc Valley	FTA 5307, TDA	2012	\$240
TRANSIT	Capital	L-403	LOMPOC19			City of Lompoc Electric Utility Vehicle and Charging Station Purchase	Project to purchase electric vehicles and charging stations.		Lompoc Valley	Demo-LU, Local	2012	\$70
City of Santa Barbara												
ST/RDS	Construction	SB-200	SBCITY3	PA&ED		Intersection Improvements – Las Positas and Cliff	Construct roundabout	Improve LOS during peak hours	Santa Barbara/ Goleta	STIP-RIP	2016	\$750
ITS		SB-500	SBCITY15			State Street Smart Corridor	Advanced Traffic Corridor System. Project #102.		Santa Barbara	TCRP - Traffic Congestion Relief Fund	2014	\$922
City of Santa Maria												

ATTACHMENT G



TECHNICAL MEMORANDUM

City of Goleta On-Call Modeling Services

ATP Grant Application Technical Support

Hollister Avenue/State Route 217 Interchange Improvements and Ekwill Street and Fowler Road Extension Project Benefits

Date: May 19, 2014 Project #: 12904
To: Teresa Lopez, City of Goleta
From: Matt Braughton, Jim Damkowitz, Kittelson & Associates
cc: Rosemarie Gaglione, City of Goleta

INFRASTRUCTURE

Bicycle Demand

After construction of the Class II bicycle paths on the Ekwill Street and Fowler Road Extension Project, expected bicycle demand was calculated by adapting the Benefit-Cost Analysis Tool (developed as part of *NCHRP Report 522: Guidelines for Analysis of Investments in Bicycle Facilities*) to the local conditions of the City of Goleta, where possible. This adapted methodology for estimating the use of a new facility is based on two assumptions:

1. Existing nearby commuter bicyclists will use the new facilities; and,
2. The new facility will induce new bicyclists based on the local bicycling commute rate (based on the 2012 ACS 5-year estimates), children's bicycling rate (5%, based on the 2011 NHTS), and nearby residential population.

Population near Improvement

In order to develop estimates of bicycle demand, the population within near the improvement was calculated using 2012 ACS 5-year population estimates at the block group level and distance buffers of 2400 meters, 1600 meters, and 800 meters based on the *NCHRP Report 552* methodology. The total population within each distance range was estimated by factoring the population of all block groups intersected by each distance buffer by the percentage of the block group within the buffer.

Existing Daily Bicycle Demand

The *NCHRP Report 552* estimation technique was used to estimate the nearby existing bicycle commuter population using available local data. Potential existing commuters were calculated by estimating each distance buffer's commuter population (based on the City of Goleta's commuter

percentage from the 2012 ACS data, 51.58%) and multiplying the resulting commuter population by the bicycle commute percentage for the City (5.44%). The number of commuters estimated to be near the Ekwill Street Extension and and Fowler Road Extension using this methodology are 75 and 68 commuters, respectively.

However, in addition to commuters, there are also adult bicyclists who do not bicycle to work. In order to estimate the total number of non-commute adult bicyclists, rates were developed following the *NCHRP Report 552* methodology. The method uses Census commute shares to extrapolate high, moderate, and low total adult non-commuter bicycling rate estimates based on the sketch planning method in Appendix A of *NCHRP Report 552*. The formulas used to estimate the total adult bicycling rates (*T*) are:

$$T_{High} = 0.6 + 3 * C$$

$$T_{Moderate} = 0.4 + 1.2 * C$$

$$T_{Low} = C$$

Where *C* is the City of Goleta’s bicycle commute share (5.44%). These bicycling rates estimates are then multiplied by the adult population within the three established distance ranges. The adult population was calculated by multiplying each distance range’s population by the percentage of the City of Goleta’s population over the age of 18, 80.61%. The three distance range estimates are then summed. The resulting estimates of adult bicycling rates total existing non-commute adult cyclists are presented in Table 1.

Table 1. Estimated Adult Bicycling Rates and Adult Bicyclist Population

Estimate	Adult Bicycling Rate	Existing Adult Bicyclists Estimate, Ekwill Street Extension	Existing Adult Bicyclists Estimate, Fowler Road Extension
High	16.91%	188	171
Moderate	6.92%	77	70
Low	5.44%	60	55

In addition to adult commuters and other adult bicyclists, existing daily child bicyclists are estimated using the National Household Travel Survey’s estimate of daily child bicycling riding of 5% and multiplying that percentage by the total child population within each distance range. The percentage of the population that are children was calculated using the percentage of the City of Goleta’s population under the age of 18, 19.39%. A total of 13 and 12 children were estimated to bicycle within the vicinity of the Ekwill Street and Fowler Road extensions, respectively.

Daily Induced Bicycling Demand

Following the induced demand estimation methodology of *NCHRP Report 552* each of the three distance range populations for each group (commuters, non-commuters, and children) are multiplied by likelihood multipliers developed as part of *NCHRP Report 552* to estimate the number of induced cyclists in each group. The likelihood factors for each distance range and the total bicycling populations within that distance range for each group are shown in Table 2 and Table 3 for the Ekwill Street and Fowler Road extensions, respectively.

Table 2. Distance-Based Likelihood Multipliers and Total Bicyclist Populations by Group, Ekwil Street

Distance Range	Likelihood Multiplier	Commuter Bicyclists	Adult Non-Commute Bicyclists, High	Adult Non-Commute Bicyclists, Moderate	Adult Non-Commute Bicyclists, Low	Child Bicyclists
2400 meters	0.15	48	121	50	39	9
1600 meters	0.44	22	54	22	17	4
800 meters	0.51	5	12	5	3	1

Table 3. Distance-Based Likelihood Multipliers and Total Bicyclist Populations by Group, Fowler Road

Distance Range	Likelihood Multiplier	Commuter Bicyclists	Adult Non-Commute Bicyclists, High	Adult Non-Commute Bicyclists, Moderate	Adult Non-Commute Bicyclists, Low	Child Bicyclists
2400 meters	0.15	48	122	50	39	9
1600 meters	0.44	17	42	17	13	3
800 meters	0.51	3	8	3	3	1

The induced bicycling estimates for each improvement by group are calculated by multiplying each of the population groups by the likelihood multiplier, and are shown in Table 4 and Table 5 for the Ekwil Street and Fowler Road extensions, respectively.

Table 4. Induced Bicycling Demand by Distance and Population Group, Ekwil Street

Distance Range	Commuter Bicyclists	Adult Non-Commute Bicyclists, High	Adult Non-Commute Bicyclists, Moderate	Adult Non-Commute Bicyclists, Low	Child Bicyclists
2400 meters	7	18	7	6	1
1600 meters	10	24	10	8	2
800 meters	2	6	3	2	0

Table 5. Induced Bicycling Demand by Distance and Population Group, Fowler Road

Distance Range	Commuter Bicyclists	Adult Non-Commute Bicyclists, High	Adult Non-Commute Bicyclists, Moderate	Adult Non-Commute Bicyclists, Low	Child Bicyclists
2400 meters	7	18	7	6	1
1600 meters	7	18	8	6	1
800 meters	2	4	2	1	0

Total new (induced) bicyclists are calculated by summing each distance and population group for each of the high, moderate, and low adult non-commute bicyclist estimates, shown in Table 6.

Table 6. Total New Bicyclists by Non-Commute Bicyclist Estimate

Estimate	Total New Bicyclists, Ekwil Street	Total New Bicyclists, Fowler Road
High	71	60
Moderate	42	36
Low	38	32

COST EFFECTIVENESS

Bicycle Lane Benefits

Benefits for the proposed bicycle improvement were calculated using *NCHRP Report 552's* Benefit-Cost Analysis Tool methodology. Benefits were calculated for four types: mobility benefit, health benefit, recreation benefit, and decreased auto use benefit. Each benefit and methodology is summarized below. All benefit calculations used the future and existing bicycle population calculations described in the previous section.

Mobility Benefit

The mobility benefit estimates the value of the new bicycle facility for bicycle commuters relative to riding on a street with parked cars. Based on stated-preference research, *NCHRP Report 552* found that commuters are willing to spend an additional 18.02 extra minutes per trip to travel on a Class II bicycle lane without parking. Additionally, Caltrans Life-Cycle Benefit-Cost Analysis Economic Parameters 2012 value an automobile driver's value of time as \$12.50 per hour. This valuation was assumed to hold for bicyclists. Using this valuation of time, the per-trip benefit of the new facility is \$3.75 for the Class II bicycle lanes on Ekwil Street and Fowler Road. Multiplying the per-trip benefit by the number of daily existing and induced commuters (94 for the Ekwil Street extension and 84 for the Fowler Road extension) and doubling these estimates to include both to and from commutes results in the daily mobility benefit for each improvement. These daily benefits is then annualized by multiplying the daily benefit by 47 weeks per year, and 5 days per week.

Additionally, because the average peak home-based work trip length for the Goleta subarea of the SBCAG travel demand model is 7.34 miles and the Ekwil Street and Fowler Road extension projects are only 0.26 and 0.24, the benefits were factored by 0.03 (the length of the two projects as the percentage of the average trip length) to better capture the overall benefit of the improvement to commuters.

The total annual mobility benefit for the Ekwil Street Class II Bicycle Lanes is \$5,778. The total annual mobility benefit for the Fowler Road Class II Bicycle Lanes is \$4,780.

Health Benefit

The health benefit estimates the per-capita cost savings from physical activity at \$128 based on the findings of Appendix E of *NCHRP Report 552*. To obtain the annual health benefit the total new cyclists (not including existing) are multiplied by \$128.

The annual health benefit for the Ekwill Street Class II Bicycle Lanes is \$4,864 using the low estimate of new bicyclists, \$5,376 using the moderate estimate of new bicyclists, and \$9,088 using the high estimate of new bicyclists. The annual health benefit for the Fowler Road Class II Bicycle Lanes is \$4,096 using the low estimate of new bicyclists, \$4,608 using the moderate estimate of new bicyclists, and \$7,680 using the high estimate of new bicyclists.

Recreation Benefit

Following the research of *NCHRP Report 552*, outdoor recreational activities are estimated at \$10 per hour, beyond the value of the time taken by the activity itself. Assuming that a typical day of bicycling involves an hour of bicycling activity the number of new bicyclists, excluding new commuters is multiplied by \$10. This daily recreational benefit is then annualized by multiplying by 365 days a year.

The annual recreation benefit for the Ekwill Street Class II Bicycle Lanes is \$69,350 using the low estimate of new bicyclists, \$83,950 using the moderate estimate of new bicyclists, and \$189,800 using the high estimate of new bicyclists. The annual recreation benefit for the Fowler Road Class II Bicycle Lanes is \$58,400 using the low estimate of new bicyclists, \$73,000 using the moderate estimate of new bicyclists, and \$160,600 using the high estimate of new bicyclists.

Decreased Auto Use Benefit

The decreased auto use benefit estimates the benefits from reduced congestion, reduced air pollution, and user cost savings as a result of new bicycle commuters. Using the total benefit per mile from *NCHRP Report 552* for a suburban/small city area of 8 cents per mile during the peak period, the number of new commuters (60) is multiplied by double the average home based work trip length to generate a daily commute benefit. This daily commute benefit is then multiplied by 47 weeks per year and 5 days per week to annualize the decreased auto use benefit.

The annual decreased auto use benefit for the Ekwill Street Class II Bicycle Lanes is \$5,244. The annual decreased auto use benefit for the Fowler Road Class II Bicycle Lanes is \$4,416.

Total Benefit

A summary of the individual benefits and the total benefit of the Ekwill Street and Fowler Road Class II Bicycle Lanes projects are summarized below in Table 7 by the high, moderate, and low estimates of adult non-commute bicyclists for each project.

Table 7. Summary and Total of Benefits of the Ekwill Street and Fowler Road Class II Bicycle Lanes

Bicycle Facility Benefits	Ekwill Street	Folwer Road	Total
Annual Mobility Benefit	\$5,778	\$4,780	\$10,558
Annual Health Benefit			
<i>High Estimate</i>	\$9,088	\$7,680	\$16,768
<i>Moderate Estimate</i>	\$5,376	\$4,608	\$9,984
<i>Low Estimate</i>	\$4,864	\$4,096	\$8,960
Annual Recreation Benefit			
<i>High Estimate</i>	\$189,800	\$160,600	\$350,400
<i>Moderate Estimate</i>	\$83,950	\$73,000	\$156,950
<i>Low Estimate</i>	\$69,350	\$58,400	\$127,750
Annual Decreased Auto Use Benefit	\$5,244	\$4,416	\$9,660
Total Annual Benefit, High	\$209,910	\$177,476	\$387,386
Total Annual Benefit, Moderate	\$100,348	\$86,804	\$187,152
Total Annual Benefit, Low	\$85,236	\$71,692	\$156,928

SAFETY

Bicycle Lane Crash Reduction

The extensions of Ekwill Street and Fowler Road with Class II bicycle lanes versus extensions without bicycle lanes will increase the safety of bicycle along the project corridors. In order to quantify the potential safety benefits of installing Class II bicycle lanes, the crash modification factors that best fit the proposed improvements was researched and selected from the FHWA Crash Modification Clearinghouse. Crash modification factors are multiplicative factors to compute the expected number of crashes after implementing a given countermeasure at a specific site. A crash modification factor for the provision of bicycle lanes is expected to reduce the number of collisions per year by 35%.

State Route 217 and Hollister Avenue Ramp Roundabouts

In order to quantify the potential safety benefits of converting the two intersections of Hollister Avenue and State Route 217 ramps, the crash modification factor that best fit the proposed conversion was used from the AASHTO Highway Safety Manual. While research is currently in-progress to address the different conditions at highway ramp intersections, crash modification factors (CMFs) are currently available in the Highway Safety Manual for conversions of signalized intersections to modern roundabouts. Crash modification factors are multiplicative factors to compute the expected number of crashes after implementing a given countermeasure at a specific site. The most appropriate crash modification factor for the two ramp intersections from the Highway Safety Manual is the conversion of an urban signalized intersection to a modern roundabout which is expected to reduce all collisions by 1% and injury collisions by 60% due to the reduction of traffic speeds and change conflict points from crossing conflicts to merging conflicts. Given that there were 5 injury collisions (and zero non-injury collisions) near the two ramp intersections (including one

severe bicyclist injury) and Caltrans' Cal-B/C values an injury accident cost at \$67,400 per collision, the expected annual safety benefit of the conversion of the two ramp intersections to modern roundabouts is estimated to be \$202,200 per year given a reduction of three injury collisions per year.

ATTACHMENT H

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	CANNON GREEN DR
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (CANNON GREEN DR), the query results include all collisions with the specified Primary Road of CANNON GREEN DR and all collisions with a Secondary Road of CANNON GREEN DR with a distance of 0 feet.

**City of Goleta
Traffic Engineering Department**

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 2

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
1881800	1/26/05	08:44	Cannon Green Dr & Phelps Rd	0'	In Int.	Head-On	Bicycle	South	Traveling Wrong Way	North	Making Right Turn	Wrong Side of Road	1	0	
3082517	2/28/07	17:34	Hollister Ave & Cannon Green Dr	0'	In Int.	Broadside	Bicycle	West	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	FAIRVIEW AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (FAIRVIEW AVE), the query results include all collisions with the specified Primary Road of FAIRVIEW AVE and all collisions with a Secondary Road of FAIRVIEW AVE with a distance of 0 feet.

City of Goleta
Traffic Engineering Department

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 23

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
0800500	3/22/03	22:49	Fairview Ave & Calle Real	25'	South	Rear-End	Bicycle	North	Stopped in Road	North	Proceeding Straight	Unsafe Speed	0	0	
1015337	8/25/03	07:37	Fairview Ave & Shirrell Wy	0'	In Int.	Head-On	Bicycle	East	Making Right Turn	North	Proceeding Straight	Improper Turning	1	0	
1778467	11/23/04	19:28	Fairview Ave & Stow Canyon Rd	550'	South	Broadside	Bicycle	South	Traveling Wrong Way	West	Making Right Turn	Wrong Side of Road	1	0	
1796340	12/9/04	12:00	Fairview Ave & Cathedral Oaks Rd	0'	In Int.	Broadside	Bicycle	North	Making Left Turn	South	Proceeding Straight	Auto R/W Violation	1	0	
1929112	2/18/05	11:03	Fairview Ave & Rt 101 Sboff/R	0'	In Int.	Broadside	Bicycle	North	Traveling Wrong Way	East	Making Right Turn	Wrong Side of Road	1	0	
2223055	8/30/05	14:09	Fairview Ave & Hollister Ave	108'	South	Head-On	Bicycle	North	Traveling Wrong Way	South	Making Right Turn	Wrong Side of Road	0	0	
9011913	8/11/06	10:34	Fairview Ave & Cathedral Oaks Rd	0'	In Int.	Broadside	Bicycle	East	Proceeding Straight	South	Proceeding Straight	Traffic Signals and Signs	0	1	
3038461	2/2/07	07:50	Fairview Ave & Rt 101	10'	South	Broadside	Bicycle	South	Making Right Turn	North	Proceeding Straight	Wrong Side of Road	1	0	
3129061	3/30/07	14:22	Fairview Ave & Olney St	29'	North	Broadside	Bicycle	North	Making Right Turn	North	Proceeding Straight	Improper Turning	1	0	
07-9023	6/4/07	14:54	Fairview Ave & Berkeley Rd	0'	In Int.	Head-On	Bicycle	South	Proceeding Straight	East	Traveling Wrong Way	Unsafe Speed	1	0	
07-10780	7/4/07	18:51	Fairview Ave & Olney St	0'	In Int.	Broadside	Bicycle	South	Proceeding Straight	West	Stopped In Road	Wrong Side of Road	1	0	
08-546	1/10/08	17:40	Fairview Ave & Calle Real	50'	North	Other	Bicycle	South	Making Left Turn	North	Proceeding Straight	Auto R/W Violation	1	0	
09-1459	1/27/09	19:15	Fairview Ave & Calle Real	159'	South	Sideswipe	Bicycle	North	Proceeding Straight	North	Proceeding Straight	Other Hazardous Movement	1	0	

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
09-12995	8/12/09	17:15	Fairview Ave & Carson St	0'	In Int.	Broadside	Bicycle	South	Proceeding Straight	West	Making Right Turn	Wrong Side of Road	1	0	
09-19160	11/18/09	18:51	Fairview Ave & Encina Rd	0'	In Int.	Broadside	Bicycle	South	Proceeding Straight	North	Making Right Turn	Wrong Side of Road	1	0	
10-672	1/13/10	16:20	Fairview Ave & Berkeley Rd	0'	In Int.	Broadside	Bicycle	East	Proceeding Straight	North	Proceeding Straight	Wrong Side of Road	1	0	
10-16793	11/3/10	14:33	Fairview Ave & Stow Canyon Rd	184'	South	Other	Bicycle	North	Changing Lanes	North	Proceeding Straight	Unsafe Lane Change	1	0	
10-17151	11/10/10	17:55	Fairview Ave & Rt 101 Sbon/R	170'	South	Broadside	Bicycle	North	Changing Lanes	North	Proceeding Straight	Unsafe Lane Change	1	0	
12-8440	6/21/12	16:19	Fairview Ave & Rt 101 Nbon/R	0'	In Int.	Other	Bicycle	South	Proceeding Straight	East	Proceeding Straight	Traffic Signals and Signs	1	0	
12-16694	12/12/12	12:51	Fairview Ave & Rt 101 Sboff/R	0'	In Int.	Other	Bicycle	North	Proceeding Straight	East	Making Right Turn	Wrong Side of Road	0	0	
13-7096	5/20/13	12:22	Fairview Ave & Hollister Ave	303'	South	Other	Bicycle	South	Traveling Wrong Way	West	Entering Traffic	Wrong Side of Road	1	0	
13-7669	6/1/13	10:38	Fairview Ave & Hollister Ave	0'	In Int.	Broadside	Bicycle	North	Proceeding Straight	North	Proceeding Straight	Unknown	0	0	
13-15514	11/7/13	08:19	Fairview Ave & Daley St	0'	In Int.	Broadside	Bicycle	South	Proceeding Straight	West	Entering Traffic	Wrong Side of Road	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	FAIRVIEW AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Pedestrian'

When only a Primary Road is specified (FAIRVIEW AVE), the query results include all collisions with the specified Primary Road of FAIRVIEW AVE and all collisions with a Secondary Road of FAIRVIEW AVE with a distance of 0 feet.

City of Goleta
Traffic Engineering Department

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 8

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
0702799	2/18/03	15:33	Fairview Ave & Encina Rd	160'	South	Vehicle - Pedestrian	Pedestrian	West	Other	South	Proceeding Straight	Pedestrian Violation	1	0	
1015452	8/15/03	15:21	Fairview Ave & Rt 101 Sboff/R	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Stopped in Road	North	Other	Ped R/W Violation	1	0	
1462331	5/20/04	08:13	Shirrell Wy & Fairview Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	East	Making Right Turn	South	Other	Ped R/W Violation	1	0	
2930592	11/28/06	15:47	Fairview Ave & Encina Rd	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Making Left Turn	Not Stat	Other	Ped R/W Violation	0	0	
07-5887	11/29/06	16:00	Fairview Ave & Encina Rd	0'	In Int.	Vehicle - Pedestrian	Pedestrian	West	Making Left Turn	North	Other	Ped R/W Violation	1	0	
07-15510	9/18/07	16:25	Fairview Ave & Rt 101 Sbon/R	50'	South	Vehicle - Pedestrian	Pedestrian	North	Other Unsafe Turning	West		Improper Turning	1	0	
11-12677	9/30/11	17:03	Fairview Ave & Encina Rd	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Making Left Turn	East	Proceeding Straight	Improper Passing	0	0	
12-593	1/14/12	15:30	Carson St & Fairview Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	West	Making Left Turn	North		Ped R/W Violation	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	HOLLISTER AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (HOLLISTER AVE), the query results include all collisions with the specified Primary Road of HOLLISTER AVE and all collisions with a Secondary Road of HOLLISTER AVE with a distance of 0 feet.

City of Goleta
Traffic Engineering Department

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 61

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
0326787	7/13/02	12:19	Hollister Ave & Tecolote Ave	0'	In Int.	Head-On	Bicycle	East	Other Unsafe Turning	South	Making Right Turn	Wrong Side of Road	1	0	
0957112	7/30/02	17:02	Hollister Ave & Nectarine Ave	181'	West	Sideswipe	Bicycle	West	Parked	West	Proceeding Straight	Other Hazardous Movement	1	0	
0345284	7/31/02	20:32	Hollister Ave & Glen Annie Rd	82'	West	Broadside	Bicycle	North	Other	West	Proceeding Straight	Wrong Side of Road	1	0	
0382435	8/16/02	10:37	Hollister Ave & Storke Rd	0'	In Int.	Broadside	Bicycle	South	Proceeding Straight	East	Proceeding Straight	Auto R/W Violation	1	0	
0432823	9/11/02	16:00	Hollister Ave & Patterson Ave	0'	In Int.	Vehicle - Pedestrian	Bicycle	Not State	Making Right Turn	East	Stopped in Road	Unknown	0	0	
0702866	2/27/03	17:18	Hollister Ave & Coronado Dr	0'	In Int.	Broadside	Bicycle	West	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	
0800817	4/25/03	18:26	Hollister Ave & Fairview Ave	48'	West	Other	Bicycle	South	Making Right Turn	North	Proceeding Straight	Auto R/W Violation	1	0	
0955062	6/3/03	07:57	Hollister Ave & Entrance Rd	0'	In Int.	Broadside	Bicycle	North	Making Right Turn	East	Proceeding Straight	Auto R/W Violation	1	0	
0957611	6/25/03	18:07	Hollister Ave & Rutherford St	336'	East	Broadside	Bicycle	East	Proceeding Straight	South	Other	Unsafe Speed	1	0	
1109183	10/28/03	14:14	Hollister Ave & Rutherford St	0'	In Int.	Broadside	Bicycle	West	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	
1245219	1/17/04	21:17	Orange Ave & Hollister Ave	0'	In Int.	Sideswipe	Bicycle	West	Making Right Turn	North	Proceeding Straight	Improper Turning	1	0	
1361182	3/12/04	14:30	Hollister Ave & Rutherford St	325'	East	Broadside	Bicycle	South	Proceeding Straight	East	Proceeding Straight	Auto R/W Violation	1	0	
1491212	6/15/04	11:24	Hollister Ave & Patterson Ave	696'	West	Broadside	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	1	0	

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
1693351	10/7/04	17:45	Hollister Ave & Edison Wooden Pole	130'	West	Broadside	Bicycle	West	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	
1731236	10/21/04	20:45	Hollister Ave & Coromar Dr (W)	655'	West	Broadside	Bicycle	South	Entering Traffic	West	Proceeding Straight	Auto R/W Violation	0	0	
2035209	4/18/05	13:44	Hollister Ave & Magnolia Ave (W)	42'	East	Sideswipe	Bicycle	West	Other	North	Proceeding Straight	Other Hazardous Movement	1	0	
2103149	6/16/05	14:07	Santa Felicia Dr & Hollister Ave	0'	In Int.	Broadside	Bicycle	West	Traveling Wrong Way	North	Stopped in Road	Wrong Side of Road	1	0	
2117851	7/10/05	15:45	Hollister Ave & Storke Rd	360'	East	Other	Bicycle	East	Making Right Turn	East	Proceeding Straight	Improper Turning	1	0	
2308796	10/11/05	19:21	Hollister Ave & La Patera Ln	0'	In Int.	Other	Bicycle	West	Making Right Turn	North	Proceeding Straight	Other	1	0	
2369631	11/11/05	10:46	Hollister Ave & Entrance Rd	170'	West	Other	Bicycle	East	Proceeding Straight	South	Making Right Turn	Wrong Side of Road	1	0	
2353103	11/16/05	07:50	Hollister Ave & Patterson Ave	411'	East	Broadside	Bicycle	East	Making Left Turn	West	Proceeding Straight	Auto R/W Violation	1	0	
2416562	12/1/05	15:05	Hollister Ave & Los Carneros Rd	0'	In Int.	Head-On	Bicycle	South	Proceeding Straight	North	Making Right Turn	Wrong Side of Road	1	0	
2629045	5/12/06	08:07	Hollister Ave & Dearborn Pl	0'	In Int.	Sideswipe	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	1	0	
2701591	6/16/06	14:06	Hollister Ave & Patterson Ave	228'	East	Broadside	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	1	0	
2814703	6/29/06	16:54	Hollister Ave & Dearborn Pl	0'	In Int.	Broadside	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	0	0	
2739162	7/5/06	20:02	Hollister Ave & Santa Barbara Shores Dr	17'	East	Head-On	Bicycle	West	Making Left Turn	East	Making Right Turn	Wrong Side of Road	1	0	
2739146	7/10/06	17:04	Hollister Ave & Magnolia Ave	63'	West	Hit Object	Bicycle	West	Parked	West	Proceeding Straight	Other Hazardous Movement	1	0	
2827546	9/15/06	19:06	Hollister Ave & Entrance Rd	0'	In Int.	Broadside	Bicycle	West	Proceeding Straight	North	Making Right Turn	Wrong Side of Road	1	0	
2913113	11/14/06	17:25	Hollister Ave & Tecolote Ave	20'	East	Sideswipe	Bicycle	West	Parked	West	Proceeding Straight	Other Hazardous Movement	1	0	

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
2953442	12/7/06	09:00	Hollister Ave & Rutherford St	40'	West	Sideswipe	Bicycle	West	Parked	West	Proceeding Straight	Other Hazardous Movement	1	0	
3067940	2/15/07	13:27	Hollister Ave & Kellogg Ave	264'	West	Other	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	1	0	
3082517	2/28/07	17:34	Hollister Ave & Cannon Green Dr	0'	In Int.	Broadside	Bicycle	West	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	
07-12873	8/8/07	08:17	Hollister Ave & Kellogg Ave	0'	In Int.	Broadside	Bicycle	West	Proceeding Straight	North	Making Right Turn	Unsafe Speed	0	1	
07-15059	9/11/07	16:48	Hollister Ave & Ward Dr	124'	East	Other	Bicycle	East	Proceeding Straight	East	Parked	Unsafe Speed	1	0	
08-7323	4/10/08	18:57	Hollister Ave & Cortona Dr	117'	East	Broadside	Bicycle	West	Proceeding Straight	North	Entering Traffic	Wrong Side of Road	1	0	
08-11739	6/19/08	08:00	Hollister Ave & Storke Rd	176'	East	Sideswipe	Bicycle	East	Making Right Turn	East	Proceeding Straight	Driving Under Influence	1	0	
08-16220	8/29/08	10:37	Hollister Ave & Pacific Oaks Rd	0'	In Int.	Broadside	Bicycle	West	Traveling Wrong Way	North	Making Right Turn	Wrong Side of Road	1	0	
08-24987	9/30/08	17:09	Hollister Ave & Pebble Beach Dr	0'	In Int.	Broadside	Bicycle	South	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	
08-28207	11/3/08	08:58	Hollister Ave & Cannon Green Dr	165'	East	Broadside	Bicycle	East	Proceeding Straight	North	Making Left Turn	Driving Under Influence	1	0	
09-5237	4/2/09	20:13	Hollister Ave & Coromar Dr	160'	West	Rear-End	Bicycle	West		West	Ran Off Road	Unknown	1	0	
09-10199	6/24/09	16:58	Hollister Ave & Kinman Ave	85'	East	Other	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	1	0	
09-18954	11/14/09	15:34	Hollister Ave & Kellogg Ave	0'	In Int.	Broadside	Bicycle	South	Proceeding Straight	West	Making Left Turn	Wrong Side of Road	1	0	
09-20408	12/16/09	12:28	Hollister Ave & Rt 101 Sbon/R	0'	In Int.	Broadside	Bicycle	South	Making Left Turn	North	Proceeding Straight	Improper Turning	1	0	
10-3894	3/15/10	20:10	Hollister Ave & Entrance Rd	20'	East	Other	Bicycle	West	Proceeding Straight	West	Stopped In Road	Unsafe Speed	0	0	
10-4808	4/1/10	17:52	Hollister Ave & Chapel St	194'	West	Broadside	Bicycle	West	Proceeding Straight	North	Making Right Turn	Wrong Side of Road	1	0	

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
10-5737	4/16/10	18:21	Hollister Ave & Rutherford St	206'	East	Other	Bicycle	West	Parked	West	Proceeding Straight	Other Hazardous Movement	1	0	
10-6634	5/3/10	16:04	Hollister Ave & Storke Rd	23'	West	Rear-End	Bicycle	East	Making Right Turn	West	Stopped In Road	Unsafe Speed	0	0	
10-7115	5/12/10	08:03	Hollister Ave & Mentor Dr	243'	East	Broadside	Bicycle	East	Slowing/Stopping	South	Proceeding Straight	Wrong Side of Road	1	0	
10-12478	8/23/10	08:06	Hollister Ave & Nectarine Ave	40'	West	Broadside	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	1	0	
10-14678	9/27/10	16:15	Hollister Ave & Storke Rd	59'	East	Broadside	Bicycle	East	Other Unsafe Turning	East	Proceeding Straight	Improper Turning	0	0	
10-18501	12/10/10	15:17	Hollister Ave & Cannon Green Dr	214'	East	Broadside	Bicycle	East	Proceeding Straight	South	Making Right Turn	Wrong Side of Road	1	0	
11-329	1/7/11	14:15	Hollister Ave & Los Carneros Rd	18'	West	Rear-End	Bicycle	East	Proceeding Straight	East	Stopped In Road	Unsafe Speed	1	0	
11-3632	3/16/11	18:18	Hollister Ave & Lowell Wy	0'	In Int.	Broadside	Bicycle	West	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	
11-4414	4/4/11	17:38	Hollister Ave & Chapel St	312'	West	Broadside	Bicycle	East	Proceeding Straight	South	Making Right Turn	Wrong Side of Road	1	0	
11-10462	8/11/11	23:20	Hollister Ave & Entrance Rd	0'	In Int.	Head-On	Bicycle	West	Proceeding Straight	North	Making Right Turn	Wrong Side of Road	1	0	
12-9781	7/18/12	18:41	Hollister Ave & Rutherford St	350'	East	Other	Bicycle	West	Making Left Turn	West	Proceeding Straight	Auto R/W Violation	1	0	
12-10422	8/1/12	09:22	Hollister Ave & Kinman Ave	348'	West	Broadside	Bicycle	West	Making Right Turn	West	Proceeding Straight	Improper Turning	1	0	
13-7669	6/1/13	10:38	Fairview Ave & Hollister Ave	0'	In Int.	Broadside	Bicycle	North	Proceeding Straight	North	Proceeding Straight	Unknown	0	0	
13-9636	7/12/13	09:37	Hollister Ave & Pine Ave	112'	East	Broadside	Bicycle	East	Changing Lanes	East	Proceeding Straight	Unsafe Lane Change	1	0	
13-12860	9/14/13	17:13	Hollister Ave & Ellwood School	10'	East	Other	Bicycle	South	Entering Traffic	West	Stopped In Road	Other Hazardous Movement	0	0	
13-12899	9/15/13	16:34	Hollister Ave & Coronado Dr	0'	In Int.	Other	Bicycle	South	Making Left Turn	East	Proceeding Straight	Auto R/W Violation	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	HOLLISTER AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Pedestrian'

When only a Primary Road is specified (HOLLISTER AVE), the query results include all collisions with the specified Primary Road of HOLLISTER AVE and all collisions with a Secondary Road of HOLLISTER AVE with a distance of 0 feet.

City of Goleta
Traffic Engineering Department

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 30

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
0437002	9/10/02	08:13	Hollister Ave & Nectarine Ave	103'	West	Head-On	Pedestrian	South	Other	West	Entering Traffic	Pedestrian Violation	1	0	
0582827	10/28/02	07:56	Hollister Ave & Rutherford St	0'	In Int.	Vehicle - Pedestrian	Pedestrian	West	Making Right Turn	North	Proceeding Straight	Ped R/W Violation	1	0	
0577126	12/23/02	07:58	Hollister Ave & Kellogg Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Making Left Turn	South	Other	Ped R/W Violation	1	0	
0702769	2/24/03	16:50	Hollister Ave & Rt 217 (W)	0'	In Int.	Rear-End	Pedestrian	West	Other	West	Proceeding Straight	Impeding Traffic	1	0	
1109174	8/29/03	07:03	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	South	Not Stated	Ped R/W Violation	0	1	
1616689	9/4/04	19:31	Hollister Ave & Pine Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Making Right Turn	North	Other	Ped R/W Violation	2	0	
1756065	10/29/04	06:40	Hollister Ave & Storke Rd	249'	East	Vehicle - Pedestrian	Pedestrian	South	Not Stated	East	Proceeding Straight	Pedestrian Violation	1	0	
2117843	6/24/05	18:11	Hollister Ave & Rutherford St	233'	East	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	North	Other	Unsafe Speed	1	0	
2158800	7/8/05	15:50	Hollister Ave & Rutherford St	320'	East	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	South	Proceeding Straight	Ped R/W Violation	1	0	
2484248	1/13/06	08:17	Hollister Ave & Entrance Rd	11'	West	Vehicle - Pedestrian	Pedestrian	West	Making Left Turn	South	Proceeding Straight	Ped R/W Violation	1	0	
2651089	4/24/06	12:08	Hollister Ave & Patterson Ave	35'	West	Head-On	Pedestrian	East	Proceeding Straight	East	Not Stated	Other Than Driver or Ped	2	0	
2930572	11/2/06	21:04	Marketplace Dr & Hollister Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Stopped in Road	West	Proceeding Straight	Ped R/W Violation	1	0	
2885210	11/6/06	18:20	Hollister Ave & Rutherford St	8'	East	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	South	Proceeding Straight	Traffic Signals and Signs	1	0	

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
07-9406	4/30/07	07:35	Hollister Ave & Glen Annie Rd	3'	South	Vehicle - Pedestrian	Pedestrian	South	Making Left Turn	East	Not Applicable -	Ped R/W Violation	0	0	
07-12853	8/8/07	19:03	Hollister Ave & Magnolia Ave	36'	West	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	East	Backing	Other Hazardous Movement	1	0	
07-15017	9/10/07	18:56	Hollister Ave & Rutherford St	0'	In Int.	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	South	Proceeding Straight	Traffic Signals and Signs	1	0	
07-15991	9/26/07	18:48	Hollister Ave & Private Property	0'	In Int.	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	South		Other Improper Driving	1	0	
07-21878	12/27/07	17:44	Hollister Ave & Storke Rd	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Proceeding Straight	West	Proceeding Straight	Unknown	1	0	
08-30144	12/8/08	19:18	Hollister Ave & Cannon Green Dr	85'	West	Vehicle - Pedestrian	Pedestrian	North		West	Proceeding Straight	Pedestrian Violation	5	0	
09-18817	11/12/09	13:45	Hollister Ave & Chapel St	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Other	East	Making Left Turn	Pedestrian Violation	1	0	
09-20941	12/28/09	14:04	Hollister Ave & Rutherford St	335'	East	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	North		Improper Passing	1	0	
10-2545	2/17/10	19:37	Hollister Ave & Rutherford St	324'	East	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	North		Ped R/W Violation	1	0	
11-886	1/17/11	19:35	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	South	Proceeding Straight	Ped R/W Violation	1	0	
12-438	1/11/12	11:21	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Proceeding Straight	East	Proceeding Straight	Pedestrian Violation	1	0	
12-16134	11/29/12	16:44	Hollister Ave & Marketplace Dr	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Making Right Turn	East		Ped R/W Violation	1	0	
12-16537	12/8/12	08:45	Hollister Ave & Kellogg Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Making Left Turn	North		Ped R/W Violation	1	0	
12-17229	12/23/12	18:05	Hollister Ave & Kellogg Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Making Left Turn	South		Ped R/W Violation	1	0	
13-1142	1/25/13	18:10	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	South		Ped R/W Violation	2	0	
13-2193	2/15/13	18:37	Hollister Ave & Kinman Ave	332'	West	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	South		Ped R/W Violation	1	0	

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
13-9905	7/17/13	16:58	Hollister Ave & Rutherford St	334'	East	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	North		Ped R/W Violation	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	KELLOGG AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (KELLOGG AVE), the query results include all collisions with the specified Primary Road of KELLOGG AVE and all collisions with a Secondary Road of KELLOGG AVE with a distance of 0 feet.

**City of Goleta
Traffic Engineering Department**

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 3

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
07-12873	8/8/07	08:17	Hollister Ave & Kellogg Ave	0'	In Int.	Broadside	Bicycle	West	Proceeding Straight	North	Making Right Turn	Unsafe Speed	0	1	
08-29659	11/28/08	16:25	Calle Real & Kellogg Ave	0'	In Int.	Sideswipe	Bicycle	East	Proceeding Straight	West	Proceeding Straight	Wrong Side of Road	1	0	
09-18954	11/14/09	15:34	Hollister Ave & Kellogg Ave	0'	In Int.	Broadside	Bicycle	South	Proceeding Straight	West	Making Left Turn	Wrong Side of Road	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	KELLOGG AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Pedestrian'

When only a Primary Road is specified (KELLOGG AVE), the query results include all collisions with the specified Primary Road of KELLOGG AVE and all collisions with a Secondary Road of KELLOGG AVE with a distance of 0 feet.

**City of Goleta
Traffic Engineering Department**

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 4

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
0577126	12/23/02	07:58	Hollister Ave & Kellogg Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Making Left Turn	South	Other	Ped R/W Violation	1	0	
10-13326	9/5/10	09:30	Kellogg Ave & Kellogg Wy	0'	In Int.	Sideswipe	Pedestrian	South	Ran Off Road	South		Improper Turning	1	0	
12-16537	12/8/12	08:45	Hollister Ave & Kellogg Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Making Left Turn	North		Ped R/W Violation	1	0	
12-17229	12/23/12	18:05	Hollister Ave & Kellogg Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Making Left Turn	South		Ped R/W Violation	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	MAGNOLIA AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (MAGNOLIA AVE), the query results include all collisions with the specified Primary Road of MAGNOLIA AVE and all collisions with a Secondary Road of MAGNOLIA AVE with a distance of 0 feet.

City of Goleta
Traffic Engineering Department

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 1

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Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
2416633	12/11/05	18:30	Magnolia Ave & Gaviota St	0'	In Int.	Rear-End	Bicycle	North	Proceeding Straight	North	Proceeding Straight	Unsafe Speed	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	MATHILDA DR
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (MATHILDA DR), the query results include all collisions with the specified Primary Road of MATHILDA DR and all collisions with a Secondary Road of MATHILDA DR with a distance of 0 feet.

**City of Goleta
Traffic Engineering Department**

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 1

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Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
10-12238	8/19/10	18:14	Mathilda Dr & Strehle Ln	759'	North	Other	Bicycle	South	Traveling Wrong Way	West	Proceeding Straight	Wrong Side of Road	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	MATHILDA DR
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Pedestrian'

When only a Primary Road is specified (MATHILDA DR), the query results include all collisions with the specified Primary Road of MATHILDA DR and all collisions with a Secondary Road of MATHILDA DR with a distance of 0 feet.

City of Goleta
Traffic Engineering Department

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 1

Page 1

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
08-13034	7/10/08	18:09	Mathilda Dr & Strehle Ln	1040'	North	Vehicle - Pedestrian	Pedestrian	West	Backing	South	Other	Other Improper Driving	2	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	ORANGE AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (ORANGE AVE), the query results include all collisions with the specified Primary Road of ORANGE AVE and all collisions with a Secondary Road of ORANGE AVE with a distance of 0 feet.

City of Goleta
Traffic Engineering Department

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 1

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
1245219	1/17/04	21:17	Orange Ave & Hollister Ave	0'	In Int.	Sideswipe	Bicycle	West	Making Right Turn	North	Proceeding Straight	Improper Turning	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	ORANGE AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Pedestrian'

When only a Primary Road is specified (ORANGE AVE), the query results include all collisions with the specified Primary Road of ORANGE AVE and all collisions with a Secondary Road of ORANGE AVE with a distance of 0 feet.

**City of Goleta
Traffic Engineering Department**

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 5

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
1109174	8/29/03	07:03	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	South	Not Stated	Ped R/W Violation	0	1	
09-769	1/15/09	00:59	Orange Ave & Gaviota St	0'	In Int.	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	North	Proceeding Straight	Pedestrian Violation	1	0	
11-886	1/17/11	19:35	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	East	Proceeding Straight	South	Proceeding Straight	Ped R/W Violation	1	0	
12-438	1/11/12	11:21	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	South	Proceeding Straight	East	Proceeding Straight	Pedestrian Violation	1	0	
13-1142	1/25/13	18:10	Hollister Ave & Orange Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	West	Proceeding Straight	South		Ped R/W Violation	2	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	PACIFIC OAKS RD
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (PACIFIC OAKS RD), the query results include all collisions with the specified Primary Road of PACIFIC OAKS RD and all collisions with a Secondary Road of PACIFIC OAKS RD with a distance of 0 feet.

**City of Goleta
Traffic Engineering Department**

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 6

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
3082513	2/28/07	19:19	Pacific Oaks Rd & Davenport Rd	0'	In Int.	Rear-End	Bicycle	West	Proceeding Straight	North	Proceeding Straight	Auto R/W Violation	1	0	
08-16220	8/29/08	10:37	Hollister Ave & Pacific Oaks Rd	0'	In Int.	Broadside	Bicycle	West	Traveling Wrong Way	North	Making Right Turn	Wrong Side of Road	1	0	
09-684	1/13/09	12:44	Pacific Oaks Rd & Phelps Rd	0'	In Int.	Broadside	Bicycle	South	Traveling Wrong Way	West	Making Right Turn	Wrong Side of Road	1	0	
10-17526	11/17/10	17:57	Pacific Oaks Rd & Hollister Ave	157'	South	Broadside	Bicycle	South	Proceeding Straight	West	Making Right Turn	Wrong Side of Road	1	0	
12-2026	2/12/12	10:45	Pacific Oaks Rd & Hollister Ave	147'	South	Broadside	Bicycle	North	Entering Traffic	North	Proceeding Straight	Auto R/W Violation	1	0	
12-2337	2/18/12	17:45	Pacific Oaks Rd & Davenport Rd	0'	In Int.	Broadside	Bicycle	North	Making Left Turn	South	Proceeding Straight	Auto R/W Violation	1	0	

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	PINE AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Bicycle'

When only a Primary Road is specified (PINE AVE), the query results include all collisions with the specified Primary Road of PINE AVE and all collisions with a Secondary Road of PINE AVE with a distance of 0 feet.

City of Goleta
Traffic Engineering Department
Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 0

Page 1

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj. Kil. Ver.
---------	------	------	----------	-------	------	-------------------	--------------------------	------------------	------------------------	------------------	------------------------	-----	----------------

Settings Used For Query

<u>Parameter</u>	<u>Setting</u>
Street Name	PINE AVE
Starting Date	1/1/2002
Ending Date	12/31/2014
Distance from Intersection	>= 0' for non rear-end collisions >= 0' for rear-end collisions
Involved With	'Pedestrian'

When only a Primary Road is specified (PINE AVE), the query results include all collisions with the specified Primary Road of PINE AVE and all collisions with a Secondary Road of PINE AVE with a distance of 0 feet.

**City of Goleta
Traffic Engineering Department**

Collision Report Summary

5/13/2014

Date Range Reported: 1/1/02 - 12/31/14

Total Number of Collisions: 2

Report#	Date	Time	Location	Dist.	Dir.	Type of Collision	Motor Veh. Involved With	Dir. of Travel 1	Movement Prec. Coll. 1	Dir. of Travel 2	Movement Prec. Coll. 2	PCF	Inj.	Kil.	Ver.
1556283	7/19/04	16:02	Pine Ave & Gaviota St	130'	North	Vehicle - Pedestrian	Pedestrian	North	Proceeding Straight	East	Entering Traffic	Pedestrian Violation	1	0	
1616689	9/4/04	19:31	Hollister Ave & Pine Ave	0'	In Int.	Vehicle - Pedestrian	Pedestrian	North	Making Right Turn	North	Other	Ped R/W Violation	2	0	

ATTACHMENT I

The Ekwil Street and Fowler Road Extensions Project has been in the planning and project development phases **for over twenty four years.**

PLANNING DOCUMENTS

The following is a chronological list of planning documents that include the proposed Ekwil Street and Fowler Road Extensions Project:

- 1. July 1993 - Goleta Community Plan (GCP). Adopted by the County of Santa Barbara.**
 - The Goleta Community Plan recognized Old Town Goleta area as a key commercial core and included objectives requiring a program to implement needed circulation system improvements in the Old Town area.
 - The plan identified Ekwil Street and Fowler Road in the Goleta Community Plan Circulation Element.
 - The GCP included as a priority bicycle and pedestrian uses in transportation corridors and called for both sidewalks and bike lanes on both Ekwil Street and Fowler Road.
 - The GCP directed the development of the Old Town Revitalization Plan and associated Programmatic EIR.

- 2. June 1998 - Goleta Old Town Revitalization Plan and EIR. Adopted by the County of Santa Barbara.**
 - Between 1993 and 1998, the County of Santa Barbara developed and approved a Revitalization Plan for Goleta Old Town and an accompanying EIR.
 - The Revitalization Plan identified fifteen development sites and necessary public street and flood control related infrastructure improvements.
 - The two most significant street improvements consisted of two new roadways (Ekwil Street and Fowler Road) that connected the east side of Goleta Old Town (State Route 217 + Kellogg Avenue) to the west side of Old Town (Fairview Avenue and Santa Barbara Airport. The Goleta Old town Revitalization Plan and accompanying EIR was adopted on June 23, 1998.

- 3. September 1997 - Project Study Reports (PSR) for the Ekwil Street Extension and Fowler Road Extension Projects. Approved by the County of Santa Barbara and Caltrans.**
 - For purposes of securing transportation funding, the County of Santa Barbara prepared two Project Study Reports, one each for the proposed Ekwil Street Extension and one for the Fowler Road.

- 4. State Transportation Plan (STIP). Approved by the California Transportation Commission (CTC).**
 - In 1998 the Santa Barbara County Association of Governments (SBCAG -MPO for Santa Barbara County) approved the Regional Transportation Improvement Plan which included the Ekwil Street and Fowler Road Extension projects. The allocation of funds to these two projects was unanimously supported by the SBCAG Board.
 - Subsequent to approval of the regional plan, the two projects were included and approved by Caltrans and the California Transportation Commission (CTC) in the State Transportation Improvement Plan (STIP).

- The Ekwil Street and Fowler Road Extensions Project has been included in every subsequent STIP since 1998.

5. Federal Transportation Plan (FTIP). Approved by the Santa Barbara County Association of Governments and the Federal Highway Administration (FHWA).

- In 1999 the Santa Barbara County Association of Governments (SBCAG -MPO for Santa Barbara County) approved the Federal Transportation Improvement Plan which included the Ekwil Street and Fowler Road Extension projects.
- Subsequent to approval by SBCAG, the FTIP was approved by the Federal Highway Administration.
- The Ekwil Street and Fowler Road Extensions Project has been included in every subsequent FTIP since 1999.

6. Goleta General Plan/Coastal Land Use Plan. Adopted by the City of Goleta.

- On October 2, 2006 the City of Goleta adopted its first General Plan/Coastal Land Use Plan. The plan included Ekwil Street and Fowler Road travel lanes, sidewalks and Class II bike lanes in the Transportation element.
- The General Plan/Coastal Land Use Plan was Amended in November 2009 and this amended plan also included Ekwil Street and Fowler Road travel lanes, sidewalks and Class II bike lanes in the Transportation element.

7. Santa Barbara Airport Layout Plan.

- On February 9, 2009 the Federal Aviation Administration (FAA) revalidated the City of Santa Barbara Airport's Airport Layout Plan which identified the proposed Fowler Road as a "Future Terminal Access Road".

8. November 2011 – Final Environmental Impact Report. Approved by City of Goleta.

- On November 28, 2011 the City of Goleta approved the Final Environmental Impact Report for the Ekwil Street and Fowler road Extensions project.

AGENCY APPROVALS AND COMMUNITY MEETINGS
--

The following is a chronological list of agency approvals, community meetings and agency approvals for the Ekwil Street and Fowler Road Extensions Project:

1. Goleta Community Plan (GCP). Adopted by the County of Santa Barbara.

- The Goleta Community Plan recognized Old Town Goleta area as a key commercial core and included objectives requiring a program to implement needed circulation system improvements in the Old Town area. The plan identified Ekwil Street and Fowler Road in the Goleta Community Plan Circulation Element.
- The GCP included as a priority bicycle and pedestrian uses in transportation corridors and called for both sidewalks and bike lanes on both Ekwil Street and Fowler Road.
- It is estimated that preparation of the Goleta Community Plan in July 1993 involved over twenty community meetings.

2. Goleta Old Town Revitalization Plan and EIR. Adopted by the County of Santa Barbara.

- In 1998, the County of Santa Barbara approved a Revitalization Plan for Goleta Old Town and an accompanying EIR.
 - It is estimated that the five year effort leading up to the approval involved over twenty community meetings.
 - The Revitalization Plan identified fifteen development sites and necessary public street and flood control related infrastructure improvements. The two most significant street improvements consisted of two new roadways (Ekwill Street and Fowler Road) that connected the east side of Goleta Old Town (State Route 217 + Kellogg Avenue) to the west side of Old Town (Fairview Avenue and Santa Barbara Airport).
- 3. August 2001 - Value Analysis Study (VAS) of the Ekwill Street and Fowler Road Extension Alternatives with participation from Caltrans, the County of Santa Barbara, The University of California at Santa Barbara, The Santa Barbara Airport, The Santa Barbara Association of Governments and the Goleta Old Town Planning Area Committee.**
- In the summer of 2001, in an effort to reach consensus on project alternatives, Caltrans hosted the VAS for the Ekwill Street and Fowler Road Extensions projects. The VAS reached consensus and recommended three alternatives.
- 4. February 2002 - Incorporation of the City of Goleta.**
- In November of 2001, the voters of Goleta approved the formation of the City of Goleta and thus the City assumed the responsibility of the Ekwill street and Fowler Road Extensions projects.
 - In April 2002, the new City Council received a full presentation on project options and directed the Project Team to host a number of community workshop meetings on the scope of alternatives to be evaluated in an environmental document projects. During the months of July, August and September 2002, the Project Team held four workshops meeting with Goleta Old Town community.
- 5. December 2002 - Notice of Preparation Hearing.**
- On December 11, 2002, the City of Goleta hosted a Notice of Preparation (NOP) public hearing under CEQA seeking input on the preparation of the Goleta Old Town Infrastructure Improvements Program (GOTIP) which included the Ekwill Street and Fowler Road Extensions projects.
- 6. April 2004 - Santa Barbara County Association of Governments (SBCAG) approves the revised project scope.**
- In 2003 the City of Goleta Team developed a new project scope for a combined Ekwill Street/Fowler Road Extensions Project that satisfied the purpose and need for the project while at the same time establishing a reduced budget and meet approval from all project stakeholders, including University of California at Santa Barbara, the City of Santa Barbara Airport, County of Santa Barbara, Caltrans and SBCAG.
 - The resulting project consisted of the Ekwill Street and Fowler Road corridors (as originally proposed in 1997) along with capacity improvements (roundabouts) at the Hollister Avenue/Route 217 Interchange.
- 7. June 2004 – Revised Notice of Preparation Hearing.**
- On June of 2004, a revised Notice of Preparation notice was released by the City of Goleta and on June 30, 2004 a hearing was hosted by the City for the environmental document for the combined Ekwill Street and Fowler Road Extensions project.

8. **October 2004 – The California Transportation Commission (CTC) approved a STIP Amendment defining the revised project scope, funding and the combining of Ekwil Street and Fowler Road into a single project.**
9. **September 2008 – Updated Notice of Preparation.**
 - On September 3, 2008 an updated Notice of Preparation for the Ekwil-Fowler Extensions project was released to recognize a shift in alignment of proposed Ekwil Street.
10. **June 2010 – Conceptual Approval of Project by Design Review Board (DRB).**
 - On June 2, 2010, the City of Goleta Design Review Board conceptually approved the Ekwil Street and Fowler Road Extensions Project at its public meeting.
11. **September 2011 – Public Hearing for the Draft EIR.**
 - On August 29, 2011 a Notice of Availability was released for the Draft Environmental Impact Report (EIR) for the Ekwil Street and Fowler Road Extensions project.
 - A public hearing was hosted by the City of Goleta on September 29, 2011.

INVOLVED PUBLIC AGENCIES

The following is a list of all public agencies involved in with the project:

The City of Goleta.

- Project sponsor
- Funding agency
- Lead Agency under CEQA.

Caltrans

- Lead Agency under NEPA.
- Proving technical oversight for Project.
- Supports Project improvements to the Hollister Avenue/State Route 217 Interchange.

The County of Santa Barbara.

- Original project sponsor (prior to formation of City of Goleta)
- Author of Goleta Community Plan
- Author of Goleta Old Town Revitalization Plan

City of Santa Barbara Airport

- Responsible Agency under CEQA (a portion of Project lies with Airport jurisdiction)
- Coastal Development Permit
- Supports Projects because of resulting improved access to Airport

Santa Barbara County Association of Governments (SBCAG – MPO for Santa Barbara County)

- Approved inclusion of Project in 1998 Regional Transportation Improvement Plan for funding through STIP.

University of California at Santa Barbara (UCSB)

- Involved in identification of Project Alternatives acceptable to the University
- Supporter of revised project scope that was approved in the Final EIR.
- Supporter of Project because of resulting enhanced access to UCSB campus.

California Transportation Commission (CTC)

- The CTC approved funding for the Project through the State Transportation Improvement Plan (STIP).

REGIONAL GREENHOUSE GAS EMISSION REDUCTION

The Ekwil Street and Fowler Road Extensions Project will reduce greenhouse gas emissions by:

- The Project alleviates congestion of Hollister Avenue by providing alternative east west access across Old Town Goleta. Hollister Avenue is a four lane major arterial through the City of Goleta and also serving as a frontage road to State Route 101.
- The Project will construct four new roundabouts in Goleta Old Town. Two of the roundabouts are located on Hollister Avenue at the State Route 217 on and off ramps. These roundabouts will eliminate all traffic signals at the Hollister Avenue/State Route 217 Interchange and substantially reduce congestion and vehicular idling. Another roundabout will be constructed at the Pine Avenue intersection with proposed Ekwil Street and at the Fairview Avenue intersection with proposed Fowler Road.

ATTACHMENT J

DEPARTMENT OF TRANSPORTATION

50 HIGUERA STREET
SAN LUIS OBISPO, CA 93401-5415
PHONE (805) 549-3101
FAX (805) 549-3329
TTY 711
<http://www.dot.ca.gov/dist05/>



*Serious drought
Help save water!*

May 16, 2014

Rosemarie Gaglione, P.E.
Interim Public Works Director
City of Goleta
130 Cremona Dr, Suite B,
Goleta, CA 93117-5514

Dear Mrs. Gaglione,

This letter is acknowledgement of the Active Transportation Program Cycle 1 (ATP) Grant Application process for the project name, "EKWILL-FOWLER BICYCLE AND PEDESTRIAN IMPROVEMENT PROJECT."

An Encroachment Permit will be required for all work within the Department Right of Way. District 5 Traffic Operations concurs the proposed project addresses a state highway need. As proposed, there are significant design details that will need to be addressed before an encroachment permit can be issued.

Operation and maintenance costs for the proposed project could be significant and will need to be addressed before a permit can be issued. A Maintenance Agreement will need to be executed and signed prior to issuance of the required encroachment permit for work and features within the Department Right of Way.

Sincerely,

A handwritten signature in black ink, appearing to read "Steve Price".

Steve Price
Deputy District Director Maintenance and Operations



■ 260 North San Antonio Road., Suite B ■ Santa Barbara, CA ■ 93110
■ Phone: 805/961-8900 ■ Fax: 805/961-8901 ■ www.sbcag.org

Board of Directors

May 15, 2014

Steve Lavagnino
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Santa Barbara
County
5th District

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Caltrans District 5

William Dillon
*Agency Counsel *
County Counsel

Ms. Teresa McWilliam
CALTRANS
Division of Local Assistance, MS 1
Attn: Office of Active Transportation & Sec. Prog.
P.O. Box 942874
Sacramento, CA 94274-0001

Dear Ms. McWilliam:

The Santa Barbara County Association of Governments' (SBCAG) is the Regional Transportation Planning Agency for Santa Barbara County. Our highest priorities for Cycle 1 Active Transportation Program funding are the California Coastal Trail Gap Closure: Rincon, the California Coastal Trail Gap Closure: Santa Claus Lane, and the California Coastal Trail Safety Project: Cabrillo Boulevard, in priority order. SBCAG is submitting grant applications for all of these projects.

One of our member agencies, the city of Goleta, is submitting three grant applications for Cycle 1 of the Active Transportation Program. The following are the city of Goleta's project applications:

- Ekwill-Fowler Bicycle and Pedestrian Improvements
- Hollister Avenue Class 1 Bike Lane
- Old Town Goleta: Hollister Avenue Complete Streets Project Study

The projects are consistent with our Regional Transportation Plan. We hope you will give careful consideration of Goleta's projects which have the support of our agency.

If you have any questions about this letter, please do not hesitate to contact Sarkes Khachek of my staff at (805) 961-8913.

Sincerely,

Jim Kemp

Executive Director

Member Agencies

Buellton ■ Carpinteria ■ Goleta ■ Guadalupe ■ Lompoc ■ Santa Barbara ■ Santa Maria ■ Solvang ■ Santa Barbara County



May 19, 2014

CITY COUNCIL

Michael T. Bennett
Mayor

Paula Perotte
Mayor Pro Tempore

Roger S. Aceves
Councilmember

Edward Easton
Councilmember

Jim Farr
Councilmember

**INTERIM
CITY MANAGER**
Michelle Greene

Ms. Teresa McWilliam
CALTRANS
Division of Local Assistance
1120 N Street
Sacramento, CA 95814

**Re: City of Goleta Active Transportation Program (ATP) –
Ekwill-Fowler Bike and Pedestrian Improvements**

Dear Ms. McWilliam:

I am writing to support the City of Goleta's (City) application to the State of California Department of Transportation Active Transportation Program (ATP) for funds for the Bicycle and Pedestrian Improvements (Project) associated with the larger Ekwill Street and Fowler Road Extensions project. The Project includes public infrastructure improvements that will improve pedestrian and bicyclist access and safety along the Ekwill Street and Fowler Road extensions.

Old Town Goleta south of Hollister Avenue is not pedestrian- or bicyclist friendly; there are no Class II bike facilities within the Project vicinity and many of the existing streets have no sidewalks. The Project includes construction of new pedestrian and bicycle facilities along new and existing roads within the project area consisting of the following: 5 foot and 8 foot wide class II bike lanes, 5 foot wide sidewalks, several roundabouts with 10 foot wide sidewalks for shared pedestrian and bicycling uses, and a multi-use bicycle and pedestrian trail along the north side of Ekwill Street. The multi-use bicycle and pedestrian trail would complete a portion of the Old San Jose Creek Trail. The pedestrian and bicycle improvements associated with the Ekwill Street and Fowler Road Extensions Project will improve access to Old Town and Santa Barbara Airport for all users, and will improve pedestrian and bicycle circulation and safety.

The Ekwil Street and Fowler Road Extensions Project was originally identified in the County of Santa Barbara's Old Town Revitalization Plan which was adopted by the City upon incorporation. The Project is needed to continue the work started under the City's Redevelopment Agency (RDA). Following the dissolution of the RDA and subsequent loss of Redevelopment funds, the City has been working to identify funding sources to complete the project construction.

Virtually no infrastructure improvements have been made to the southern portion of Old Town Goleta for over 50 years. Furthermore, vehicular, pedestrian, and bicycle access to and within southern Old Town is cumbersome. One of the major infrastructure improvements identified in the Goleta Old Town Revitalization Plan was the construction of two new roads, Ekwil Street and Fowler Road, to provide east-west routes linking Fairview Avenue to Kellogg Avenue.

I strongly support the City's application to this ATP call for projects. A successful application will enable the City to move forward with construction of bicycle and pedestrian improvements for the Ekwil Fowler project.

Should you have any questions regarding the City's grant application or require further information, please feel free to contact Mr. James Winslow, Senior Project Manager, at 805.961.7577.

Sincerely,



Michael T. Bennett
Mayor

Cc: Michelle Greene, Interim City Manager
Rosemarie Gaglione, Interim Public Works Director
James Winslow, Senior Project Manager

May 16, 2014

CALTRANS
Division of Local Assistance
Attn: Teresa McWilliam
1120 N Street
Sacramento, CA 95814

Re: City of Goleta Active Transportation Program (ATP) – Ekwil-Fowler Bike and Pedestrian Improvements

Dear Ms. McWilliam:

I am writing to support the City of Goleta's (City) application to the State of California Department of Transportation Active Transportation Program (ATP) for funds for the Bicycle and Pedestrian Improvements (Project) associated with the larger Ekwil Street and Fowler Road Extensions project. The Project includes public infrastructure improvements that will improve pedestrian and bicyclist access and safety along the Ekwil Street and Fowler Road extensions.

Old Town Goleta south of Hollister Avenue is not pedestrian- or bicyclist friendly; there are no Class II bike facilities within the Project vicinity and many of the existing streets have no sidewalks. The Project includes construction of new pedestrian and bicycle facilities along new and existing roads within the project area consisting of the following: 5 foot and 8 foot wide class II bike lanes, 5 foot wide sidewalks, several roundabouts with 10 foot wide sidewalks for shared pedestrian and bicycling uses, and a multi-use bicycle and pedestrian trail along the north side of Ekwil Street. The multi-use bicycle and pedestrian trail would complete a portion of the Old San Jose Creek Trail. The pedestrian and bicycle improvements associated with the Ekwil Street and Fowler Road Extensions Project will improve access to Old Town and Santa Barbara Airport for all users, and will improve pedestrian and bicycle circulation and safety.

The Ekwil Street and Fowler Road Extensions Project was originally identified in the County of Santa Barbara's Old Town Revitalization Plan which was adopted by the City upon incorporation. The Project is needed to continue the work started under the City's Redevelopment Agency (RDA). Following the dissolution of the RDA and subsequent loss of Redevelopment funds, the City has been working to identify funding sources to complete the project construction.

Virtually no infrastructure improvements have been made to the southern portion of Old Town Goleta for over 50 years. Furthermore, vehicular, pedestrian, and bicycle access to and within southern Old Town is cumbersome. One of the major infrastructure improvements identified in the Goleta Old Town Revitalization Plan was the construction of two new roads, Ekwil Street and Fowler Road, to provide east-west routes linking Fairview Avenue to Kellogg Avenue.

I strongly support the City's application to this ATP call for projects. A successful application will enable the City to move forward with construction of bicycle and pedestrian improvements for the Ekwil Fowler project.

Should you have any questions regarding my support of the City's grant application or require further information, please feel free to contact me at 968-2451 ext. 4503 or scarey@sbsdk12.org.

Sincerely,

A handwritten signature in black ink, appearing to read 'SCarey', written over the printed name.

Shawn Carey
Principal of Dos Pueblos High School

UNIVERSITY OF CALIFORNIA

BERKELEY • DAVIS • IRVINE • LOS ANGELES • MERCED • RIVERSIDE • SAN DIEGO • SAN FRANCISCO



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SANTA BARBARA

Office of the Chancellor
Santa Barbara, CA 93106-2030
Phone: (805) 893-2231
Fax: (805) 893-8717
<http://www.chancellor.ucsb.edu>

May 19, 2014

CALTRANS
Division of Local Assistance
Attn: Teresa McWilliam
1120 N Street
Sacramento, CA 95814

Re: City of Goleta Active Transportation Program (ATP) – Ekwil-Fowler Bike and Pedestrian Improvements

Dear Ms. McWilliam:

The University of California, Santa Barbara is aware that the City of Goleta (City) is applying to the State of California Department of Transportation Active Transportation Program (ATP) for funds for the Bicycle and Pedestrian Improvements (Project) associated with the larger Ekwil Street and Fowler Road Extensions project. As this Project will positively impact both the City and university communities, I would like to pledge our support of the application. The construction of bicycle and pedestrian improvements will benefit local residents as well as our students who live and work in the community.

Should you have any questions regarding the university's support of the City's grant application or require further information, please feel free to contact Kirsten Deshler, Director of Governmental Relations, at (805) 893-4588.

Sincerely,

A handwritten signature in cursive script that reads "Henry T. Yang".

Henry T. Yang
Chancellor

cc: Kirsten Deshler

STATE CAPITOL
P.O. BOX 942849
SACRAMENTO, CA 94249-0037
(916) 319-2037
FAX (916) 319-2137

DISTRICT OFFICE
101 WEST ANAPAMU STREET
SUITE A
SANTA BARBARA, CA 93101
(805) 564-1649
FAX (805) 564-1651

89 SOUTH CALIFORNIA STREET
SUITE F
VENTURA, CA 93001
(805) 641-3700
FAX (805) 641-3708



May 16, 2014

CALTRANS
Division of Local Assistance
Attn: Teresa McWilliam
1120 N Street
Sacramento, CA 95814

Re: City of Goleta Active Transportation Program (ATP) – Ekwill-Fowler Bike and Pedestrian Improvements

Dear Ms. McWilliam:

I am writing to support the City of Goleta's (City) application to the State of California Department of Transportation Active Transportation Program (ATP) for funds for the Bicycle and Pedestrian Improvements (Project) associated with the larger Ekwill Street and Fowler Road Extensions project. The Project includes public infrastructure improvements that will improve pedestrian and bicyclist access and safety along the Ekwill Street and Fowler Road extensions.

Old Town Goleta south of Hollister Avenue is not pedestrian- or bicyclist friendly; there are no Class II bike facilities within the Project vicinity and many of the existing streets have no sidewalks. The Project includes construction of new pedestrian and bicycle facilities along new and existing roads within the project area consisting of the following: 5 foot and 8 foot wide class II bike lanes, 5 foot wide sidewalks, several roundabouts with 10 foot wide sidewalks for shared pedestrian and bicycling uses, and a multi-use bicycle and pedestrian trail along the north side of Ekwill Street. The multi-use bicycle and pedestrian trail would complete a portion of the Old San Jose Creek Trail. The pedestrian and bicycle improvements associated with the Ekwill Street and Fowler Road Extensions Project will improve access to Old Town and Santa Barbara Airport for all users, and will improve pedestrian and bicycle circulation and safety.

The Ekwill Street and Fowler Road Extensions Project was originally identified in the County of Santa Barbara's Old Town Revitalization Plan which was adopted by the City upon incorporation. The Project is needed to continue the work started under the City's Redevelopment Agency (RDA). Following the dissolution of the RDA and subsequent loss of Redevelopment funds, the City has been working to identify funding sources to complete the project construction.

Virtually no infrastructure improvements have been made to the southern portion of Old Town Goleta for over 50 years. Furthermore, vehicular, pedestrian, and bicycle access to and within southern Old Town is cumbersome. One of the major infrastructure improvements identified in the Goleta Old Town Revitalization Plan was the construction of two new roads, Ekwill Street and Fowler Road, to provide east-west routes linking Fairview Avenue to Kellogg Avenue.

I strongly support the City's application to this ATP call for projects. A successful application will enable the City to move forward with construction of bicycle and pedestrian improvements for the Ekwill Fowler project. Should you have any questions regarding my support of the City's grant application or require further information, please feel free to contact Hillary Blackerby in my Santa Barbara office.

Sincerely,

A handwritten signature in black ink, appearing to read 'Das Williams', written in a cursive style.

DAS WILLIAMS
Assemblymember, 37th District

May 16, 2014

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infrastructure improvements identified in the Goleta Old Town Revitalization Plan was the construction of two new roads, Ekwil Street and Fowler Road, to provide east-west routes linking Fairview Avenue to Kellogg Avenue.

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Should you have any questions regarding my support of the City's grant application or require further information, please feel free to contact Eva Inbar at (805) 910-7764.

Sincerely,

A handwritten signature in black ink, appearing to read "Eva Inbar". The signature is fluid and cursive, with the first name "Eva" written in a larger, more prominent script than the last name "Inbar".

Eva Inbar, Director
COAST



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Office of the Superintendent

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Superintendent

William Banning

May 16, 2014

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Should you have any questions regarding my support of the City's grant application or require further information, please feel free to contact me at (805) 681-1200 ext. 201.

Sincerely,

A handwritten signature in black ink, appearing to read "Wm Banning". The signature is stylized with a large, sweeping flourish that extends to the right and then curves back down.

William Banning



May 16, 2014

**Santa Barbara
Bicycle Coalition**
PO Box 92047
Santa Barbara
CA 93190-2047
bike@sbbike.org
www.sbbike.org
805-617 32 55

Bici Centro
506 E. Haley St.
Santa Barbara
CA 93103
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805-617 32 55

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Should you have any questions regarding my support of the City's grant application or require further information, please feel free to contact Ed France.

Sincerely,

Ed France
Executive Director
Santa Barbara Bicycle Coalition

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Bill Terre
*FLIR Commercial Vision
Systems*

Emma Torres
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Jim Youngson
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Craig Zimmerman
The Towbes Group



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GOLETA

GOLETA VALLEY CHAMBER OF COMMERCE

I strongly support the City's application to this ATP call for projects. A successful application will enable the City to move forward with construction of bicycle and pedestrian improvements for the Ekwil Fowler project.

Should you have any questions regarding my support of the City's grant application or require further information, please feel free to contact me at 805.967.2500 ext 8.

Sincerely,



Kristen Miller
President & CEO
Goleta Valley Chamber of Commerce

ATTACHMENT K

COOPERATIVE AGREEMENT

This agreement, effective on 1/4/2012, is between the State of California, acting through its Department of Transportation, referred to as CALTRANS, and:

City of Goleta, a body politic and municipal corporation or chartered city of the State of California, referred to as CITY.

For the purpose of this agreement, the term PARTNERS collectively refers to CALTRANS and CITY (all signatory parties to this agreement). The term PARTNER refers to any one of those signatory parties individually.

RECITALS

1. California Streets and Highways Code sections 114 and 130 authorize PARTNERS to enter into a cooperative agreement for performance of work within the State Highway System (SHS) right of way.
2. This agreement outlines the terms and conditions of cooperation between PARTNERS to produce PS&E and obtain R/W Certification for the Hollister Avenue/State Route 217 and Hollister Avenue/Kellogg Avenue intersections and the extensions of Ekwill Street and Fowler Road in the City of Goleta.

For the purpose of this agreement, the Hollister Avenue/State Route 217 and Hollister Avenue/Kellogg Avenue intersections and the extensions of Ekwill Street and Fowler Road in the City of Goleta will be referred to as PROJECT. All responsibilities assigned in this agreement to produce PS&E and obtain R/W Certification will be referred to as OBLIGATIONS.

3. Prior to the execution of this agreement, CITY has been working to develop the Project Report under the terms and conditions defined in Cooperative Agreement No.05-CA 0169.This Cooperative Agreement No. 05-0256 shall supersede any and all articles pertaining to PS&E in Cooperative Agreement No. 05-CA-0169
4. CITY is preparing the Project Report and environmental documentation for PROJECT with CALTRANS' approval estimated on March 31, 2012.
5. The estimated date for OBLIGATION COMPLETION is December 31, 2014.
6. In this agreement capitalized words represent defined terms and acronyms. The Definitions section contains a complete definition for each capitalized term.

7. From this point forward, PARTNERS define in this agreement the terms and conditions under which they will accomplish OBLIGATIONS.

RESPONSIBILITIES

8. CITY is SPONSOR for 100% of PROJECT.
9. CALTRANS will provide IQA for the portions of WORK within existing and proposed SHS right of way. CALTRANS retains the right to reject noncompliant WORK, protect public safety, preserve property rights, and ensure that all WORK is in the best interest of the SHS.
10. CITY may provide IQA for the portions of WORK outside existing and proposed SHS right of way.
11. CITY is the only FUNDING PARTNER for this agreement. CITY's funding commitment is defined in the FUNDING SUMMARY.
12. CITY is the CEQA lead agency for PROJECT.
13. CALTRANS is the CEQA responsible agency for PROJECT.
14. CALTRANS is the NEPA lead agency for PROJECT.
15. CITY is IMPLEMENTING AGENCY for PS&E and R/W.

SCOPE

Scope: General

16. PARTNERS will perform all OBLIGATIONS in accordance with federal and California laws, regulations, and standards; FHWA STANDARDS; and CALTRANS STANDARDS.
17. IMPLEMENTING AGENCY for a PROJECT COMPONENT will provide a Quality Management Plan (QMP) for that component as part of the PROJECT MANAGEMENT PLAN.
18. Any PARTNER may, at its own expense, have representatives observe any OBLIGATIONS performed by another PARTNER. Observation does not constitute authority over those OBLIGATIONS.
19. Each PARTNER will ensure that all of its personnel participating in OBLIGATIONS are appropriately qualified, and if necessary licensed, to perform the tasks assigned to them.
20. PARTNERS will invite each other to participate in the selection and retention of any consultants who participate in OBLIGATIONS.

21. If WORK is done under contract (not completed by a PARTNER's own employees) and is governed by the California Labor Code's definition of "public works" (section 1720(a)(a)), that PARTNER will conform to sections 1720 – 1815 of the California Labor Code and all applicable regulations and coverage determinations issued by the Director of Industrial Relations.
22. IMPLEMENTING AGENCY for each PROJECT COMPONENT included in this agreement will be available to help resolve problems generated by that component for the entire duration of PROJECT.
23. CALTRANS will issue, upon proper application, the encroachment permits required for WORK within SHS right of way.

Contractors and/or agents, and utility owners will not perform WORK without an encroachment permit issued in their name.

24. If any PARTNER discovers unanticipated cultural, archaeological, paleontological, or other protected resources during WORK, all WORK in that area will stop and that PARTNER will notify all PARTNERS within 24 hours of discovery. WORK may only resume after a qualified professional has evaluated the nature and significance of the discovery and a plan is approved for its removal or protection.
25. PARTNERS will hold all administrative draft and administrative final reports, studies, materials, and documentation relied upon, produced, created, or utilized for PROJECT in confidence to the extent permitted by law. Where applicable, the provisions of California Government Code section 6254.5(e) will govern the disclosure of such documents in the event that PARTNERS share said documents with each other.

PARTNERS will not distribute, release, or share said documents with anyone other than employees, agents, and consultants who require access to complete PROJECT without the written consent of the PARTNER authorized to release them, unless required or authorized to do so by law.

26. If any PARTNER receives a public records request, pertaining to OBLIGATIONS, that PARTNER will notify PARTNERS within five (5) working days of receipt and make PARTNERS aware of any disclosed public records. PARTNERS will consult with each other prior to the release of any public documents related to the PROJECT.
27. If HM-1 or HM-2 is found during a PROJECT COMPONENT, IMPLEMENTING AGENCY for that PROJECT COMPONENT will immediately notify PARTNERS.
28. CALTRANS, independent of PROJECT, is responsible for any HM-1 found within the existing SHS right of way. CALTRANS will undertake HM MANAGEMENT ACTIVITIES related to HM-1 with minimum impact to PROJECT schedule.
29. CITY, independent of PROJECT, is responsible for any HM-1 found within PROJECT limits and outside the existing SHS right of way. CITY will undertake or cause to be

undertaken HM MANAGEMENT ACTIVITIES related to HM-1 with minimum impact to PROJECT schedule.

30. If HM-2 is found within PROJECT limits, the public agency responsible for the advertisement, award, and administration (AAA) of the PROJECT construction contract will be responsible for HM MANAGEMENT ACTIVITIES related to HM-2.
31. CALTRANS' acquisition or acceptance of title to any property on which any HM-1 or HM-2 is found will proceed in accordance with CALTRANS' policy on such acquisition.
32. PARTNERS will comply with all of the commitments and conditions set forth in the environmental documentation, environmental permits, approvals, and applicable agreements as those commitments and conditions apply to each PARTNER's responsibilities in this agreement.
33. IMPLEMENTING AGENCY for each PROJECT COMPONENT will furnish PARTNERS with written quarterly progress reports during the implementation of OBLIGATIONS in that component.
34. Upon OBLIGATION COMPLETION, ownership or title to all materials and equipment constructed or installed for the operations and/or maintenance of the SHS within SHS right of way as part of WORK become the property of CALTRANS.

CALTRANS will not accept ownership or title to any materials or equipment constructed or installed outside SHS right of way.
35. IMPLEMENTING AGENCY for a PROJECT COMPONENT will accept, reject, compromise, settle, or litigate claims of any non-agreement parties hired to do WORK in that component.
36. PARTNERS will confer on any claim that may affect OBLIGATIONS or PARTNERS' liability or responsibility under this agreement in order to retain resolution possibilities for potential future claims. No PARTNER will prejudice the rights of another PARTNER until after PARTNERS confer on claim.
37. PARTNERS will maintain, and will ensure that any party hired by PARTNERS to participate in OBLIGATIONS will maintain, a financial management system that conforms to Generally Accepted Accounting Principles (GAAP), and that can properly accumulate and segregate incurred PROJECT costs, and provide billing and payment support.
38. PARTNERS will comply with the appropriate federal cost principles and administrative requirements outlined in the Applicable Cost Principles and Administrative Requirements table below. These principles and requirements apply to all funding types included in this agreement.
39. PARTNERS will ensure that any party hired to participate in OBLIGATIONS will comply with the appropriate federal cost principles and administrative requirements outlined in the Applicable Cost Principles and Administrative Requirements table below.

Applicable Cost Principles and Administration Requirements		
The federal cost principles and administrative requirements associated with each organization type apply to that organization.		
Organization Type	Cost Principles	Administrative Requirements
Federal Governments	2 CFR Part 225	OMB A-102
State and Local Government	2 CFR, Part 225	49 CFR, Part 18
Educational Institutions	2 CFR, Part 220	2 CFR, Part 215
Non-Profit Organizations	2 CFR, Part 230	2 CFR, Part 215
For Profit Organizations	48 CFR, Chapter 1, Part 31	49 CFR, Part 18
CFR (Code of Federal Regulations)		
OMB (Office of Management and Budget)		
Related URLs:		
• Various OMB Circular:	http://www.whitehouse.gov/omb/grants_circulars	
• Code of Federal Regulations:	http://www.gpoaccess.gov/CFR	

- 40. PARTNERS will maintain and make available to each other all OBLIGATIONS-related documents, including financial data, during the term of this agreement.
- 41. PARTNERS will retain all OBLIGATIONS-related records for three (3) years after the final voucher.
- 42. PARTNERS have the right to audit each other in accordance with generally accepted governmental audit standards.

CALTRANS, the state auditor, FHWA, and CITY will have access to all OBLIGATIONS-related records of each PARTNER, and any party hired by a PARTNER to participate in OBLIGATIONS, for audit, examination, excerpt, or transcription.

The examination of any records will take place in the offices and locations where said records are generated and/or stored and will be accomplished during reasonable hours of operation. The auditing PARTNER will be permitted to make copies of any OBLIGATIONS-related records needed for the audit.

The audited PARTNER will review the draft audit, findings, and recommendations, and provide written comments within 30 calendar days of receipt.

Upon completion of the final audit, PARTNERS have 30 days to refund or invoice as necessary in order to satisfy the obligation of the audit.

Any audit dispute not resolved by PARTNERS is subject to dispute resolution. Any costs arising out of the dispute resolution process will be paid within 30 calendar days of the final audit or dispute resolution findings.

- 43. Any PARTNER that hires another party to participate in OBLIGATIONS will conduct a pre-award audit of that party in accordance with the *Local Assistance Procedures Manual*.
- 44. PARTNERS will not incur costs beyond the funding commitments in this agreement. If IMPLEMENTING AGENCY anticipates that funding for WORK will be insufficient to complete WORK, IMPLEMENTING AGENCY will promptly notify SPONSOR.

IMPLEMENTING AGENCY has no obligation to perform WORK if funds to perform WORK are unavailable.

- 45. If WORK stops for any reason, IMPLEMENTING AGENCY will place all facilities impacted by WORK in a safe and operable condition acceptable to CALTRANS.
- 46. If WORK stops for any reason, each PARTNER will continue to implement all of its applicable commitments and conditions included in the PROJECT environmental documentation, permits, agreements, or approvals that are in effect at the time that WORK stops, as they apply to each PARTNER’s responsibilities in this agreement, in order to keep PROJECT in environmental compliance until WORK resumes.
- 47. Each PARTNER accepts responsibility to complete the activities that it selected on the SCOPE SUMMARY. Activities marked with “N/A” on the SCOPE SUMMARY are not included in the scope of this agreement.

Scope: Environmental Permits, Approvals and Agreements

- 48. Each PARTNER identified in the Environmental Permits table below accepts the responsibility to complete the assigned activities.

Environmental Permits						
Permit	Coordinate	Prepare	Obtain	Implement	Renew	Amend
404 USACOE	CITY	CITY	CITY	CITY	CITY	CITY
401 RWQCB	CITY	CITY	CITY	CITY	CITY	CITY
FESA Section 7 USFWS	CALTRANS	CITY	CITY	CITY	CITY	CITY
Coastal Development Permit CCC	CITY	CITY	CITY	CITY	CITY	CITY
1602 DFG	CITY	CITY	CITY	CITY	CITY	CITY

Scope: Plans, Specifications, and Estimate (PS&E)

49. CITY will ensure that the engineering firm preparing the plans, specifications, and estimate will not be employed by or under contract to the PROJECT construction contractor.

CITY will not employ the engineering firm preparing the plans, specifications, and estimate for construction management of PROJECT.

However, CITY may retain the engineering firm during the construction PROJECT COMPONENT to check shop drawings, do soil foundation tests, test construction materials, and perform construction surveys.

50. CITY will identify and locate all utility facilities within PROJECT area as part of PS&E responsibilities. The plans, specifications, and estimate for PROJECT will identify all utility facilities not relocated or removed in advance of the construction PROJECT COMPONENT.
51. CITY will make all necessary arrangements with utility owners for the timely accommodation, protection, relocation, or removal of any existing utility facilities that conflict with construction of PROJECT or that violate CALTRANS' encroachment policy.
52. The responsibility to advertise, open bids, award, and approve the construction contract will be the subject of a future agreement.

Scope: Right of Way (R/W)

53. CITY will provide a land surveyor licensed in the State of California to be responsible for surveying and right of way engineering. All survey and right of way engineering documents will bear the professional seal, certificate number, registration classification, expiration date of certificate, and signature of the responsible surveyor.
54. CITY will provide CALTRANS-approved verification of its arrangements for the protection, relocation, or removal of all conflicting facilities and that such work will be completed prior to construction contract award or as otherwise stated in the PROJECT plans, specifications, and estimate. This verification must include references to all required SHS encroachment permits.
55. CITY will utilize a public agency currently qualified by CALTRANS or a properly licensed consultant for all right of way activities. A qualified right of way agent will administer all right of way consultant contracts.

CITY will submit a draft Right of Way Certification document to CALTRANS six weeks prior to the scheduled milestone date for review.

56. CITY will prepare and provide to CALTRANS a Right of Way Certification prior to PROJECT advertisement.

57. All right of way conveyances must be completed prior to OBLIGATION COMPLETION.
58. CALTRANS' acceptance of right of way title is subject to review of an Updated Preliminary Title Report provided by CITY verifying that the title, including access rights, is free of all encumbrances and liens. Upon acceptance, CITY will provide CALTRANS with a Policy of Title Insurance in CALTRANS' name.
59. The California Transportation Commission will hear Resolutions of Necessity for any proposed right of way within the SHS. If CITY intends to hear any Resolutions of Necessity, for any proposed right of way within the SHS, CITY must obtain approval from CALTRANS prior to hearing any such Resolutions of Necessity

COST

Cost: General

60. The cost of any awards, judgments, or settlements generated by OBLIGATIONS is an OBLIGATIONS COST.
61. CALTRANS, independent of PROJECT, will pay all costs for HM MANAGEMENT ACTIVITIES related to HM-1 found within the existing SHS right of way.
62. CITY, independent of PROJECT, will pay, or cause to be paid, all costs for HM MANAGEMENT ACTIVITIES related to HM-1 found within PROJECT limits and outside of the existing SHS right of way.
63. HM MANAGEMENT ACTIVITIES costs related to HM-2 are CONSTRUCTION SUPPORT and CONSTRUCTION CAPITAL costs.
64. The cost to comply with and implement the commitments set forth in the environmental documentation is an OBLIGATIONS COST.
65. The cost to ensure that PROJECT remains in environmental compliance is an OBLIGATIONS COST.
66. The cost of any legal challenges to the CEQA or NEPA environmental process or documentation is an OBLIGATIONS COST.
67. Independent of OBLIGATIONS COST, CALTRANS will fund the cost of its own IQA for WORK done within existing or proposed future SHS right of way.
68. Independent of OBLIGATIONS COST, CITY will fund the cost of its own IQA for WORK done outside existing or proposed future SHS right of way.

69. CALTRANS will provide encroachment permits to PARTNERS, their contractors, consultants and agents, at no cost.
70. Fines, interest, or penalties levied against a PARTNER will be paid, independent of OBLIGATIONS cost, by the PARTNER whose actions or lack of action caused the levy. That PARTNER will indemnify and defend each other PARTNER.
71. Travel, per diem, and third-party contract reimbursements are an OBLIGATIONS COST only after those hired by PARTNERS to participate in OBLIGATIONS incur and pay those costs.

Payments for travel and per diem will not exceed the rates paid rank and file state employees under current California Department of Personnel Administration (DPA) rules current at the effective date of this agreement.

If CITY invoices for rates in excess of DPA rates, CITY will fund the cost difference and reimburse CALTRANS for any overpayment.

72. The cost of any engineering support performed by CALTRANS includes all direct and applicable indirect costs. CALTRANS calculates indirect costs based solely on the type of funds used to pay support costs. State and federal funds are subject the current Program Functional Rate. Local funds are subject to the current Program Functional Rate and the current Administration Rate. Caltrans periodically adjusts the Program Functional Rate and the Administration Rate.
73. If CALTRANS reimburses CITY for any costs later determined to be unallowable, CITY will reimburse those funds.
74. The cost to place PROJECT right of way in a safe and operable condition and meet all environmental commitments is an OBLIGATIONS cost.
75. Because IMPLEMENTING AGENCY is responsible for managing the scope, cost, and schedule of a project component, if there are insufficient funds available in this agreement to place the right of way in a safe and operable condition, the appropriate IMPLEMENTING AGENCY accepts responsibility to fund these activities until such time as PARTNERS amend this agreement.

That IMPLEMENTING AGENCY may request reimbursement for these costs during the amendment process.

76. If there are insufficient funds in this agreement to implement applicable commitments and conditions included in the PROJECT environmental documentation, permits, agreements, and/or approvals that are in effect at a time that WORK stops, each PARTNER implementing commitments or conditions accepts responsibility to fund these activities, as they apply to each PARTNER's responsibilities, until such time are PARTNERS amend this agreement.

Each PARTNER may request reimbursement for these costs during the amendment process.

77. PARTNERS will pay invoices within 30 calendar days of receipt of invoice.

Cost: Environmental Permits, Approvals and Agreements

78. The cost of coordinating, obtaining, complying with, implementing, and if necessary renewing and amending resource agency permits, agreements, and/or approvals is an OBLIGATIONS COST.

Cost: Plans, Specifications, and Estimate (PS&E)

79. CITY will determine the cost to positively identify and locate, protect, relocate, or remove any utility facilities whether inside or outside SHS right of way in accordance with federal and California laws and regulations, and CALTRANS' policies, procedures, standards, practices, and applicable agreements including, but not limited to, Freeway Master Contracts.

80. Each PARTNER listed below may submit invoices for PS&E:

- CITY may invoice CALTRANS

81. PARTNERS will exchange funds for actual costs.

CITY will invoice CALTRANS for a \$82,000 initial deposit upon execution of this agreement. This deposit represents two (2) months' estimated support costs.

Thereafter, CITY will submit to CALTRANS monthly invoices for estimated monthly costs based on the prior month's actual expenditures.

After PARTNERS agree that all WORK is complete, CITY will submit a final accounting for all OBLIGATIONS COSTs. Based on the final accounting, PARTNERS will refund or invoice as necessary in order to satisfy the financial commitments of this agreement.

Cost: Right of Way (R/W) Support

82. CITY agrees to fund one hundred percent (100%) of actual R/W support costs related to acquisitions within SHS needed to complete PROJECT using local funding.

Cost: Right of Way (R/W) Capital

83. PARTNERS will exchange funds for actual costs.

CITY will invoice CALTRANS for a \$200,000 initial deposit upon execution of this agreement. This deposit represents one (1) month's estimated capital costs.

Thereafter, CITY will submit to CALTRANS monthly invoices for estimated monthly costs based on the prior month's actual expenditures.

After PARTNERS agree that all WORK is complete, CITY will submit a final accounting for all OBLIGATIONS COSTs. Based on the final accounting, PARTNERS will refund or invoice as necessary in order to satisfy the financial commitments of this agreement.

SCHEDULE

84. PARTNERS will manage the schedule for OBLIGATIONS through the work plan included in the PROJECT MANAGEMENT PLAN.

GENERAL CONDITIONS

85. PARTNERS understand that this agreement is in accordance with and governed by the Constitution and laws of the State of California. This agreement will be enforceable in the State of California. Any PARTNER initiating legal action arising from this agreement will file and maintain that legal action in the Superior Court of the county in which the CALTRANS district office that is signatory to this agreement resides, or in the Superior Court of the county in which PROJECT is physically located.
86. All OBLIGATIONS of CALTRANS under the terms of this agreement are subject to the appropriation of resources by the Legislature, the State Budget Act authority, and the allocation of funds by the California Transportation Commission.
87. Any PARTNER performing IQA does so for its own benefit. No one can assign liability to that PARTNER due to its IQA activities.
88. Neither CITY nor any officer or employee thereof is responsible for any injury, damage or liability occurring by reason of anything done or omitted to be done by CALTRANS and/or its agents under or in connection with any work, authority, or jurisdiction conferred upon CALTRANS under this agreement.

It is understood and agreed that CALTRANS and/or its agents will fully defend, indemnify, and save harmless CITY and all of its officers and employees from all claims, suits, or actions of every name, kind, and description brought forth under, but not limited to, tortious, contractual, inverse condemnation, or other theories or assertions of liability occurring by reason of anything done or omitted to be done by CALTRANS and/or its agents under this agreement.

89. Neither CALTRANS nor any officer or employee thereof is responsible for any injury, damage, or liability occurring by reason of anything done or omitted to be done by CITY

and/or its agents under or in connection with any work, authority, or jurisdiction conferred upon CITY under this agreement.

It is understood and agreed that CITY and/or its agents will fully defend, indemnify, and save harmless CALTRANS and all of its officers and employees from all claims, suits, or actions of every name, kind, and description brought forth under, but not limited to, tortious, contractual, inverse condemnation, or other theories or assertions of liability occurring by reason of anything done or omitted to be done by CITY and/or its agents under this agreement.

90. PARTNERS do not intend this agreement to create a third party beneficiary or define duties, obligations, or rights in parties not signatory to this agreement. PARTNERS do not intend this agreement to affect their legal liability by imposing any standard of care for fulfilling OBLIGATIONS different from the standards imposed by law.
91. PARTNERS will not assign or attempt to assign OBLIGATIONS to parties not signatory to this agreement.
92. PARTNERS will not interpret any ambiguity contained in this agreement against each other. PARTNERS waive the provisions of California Civil Code section 1654.
93. A waiver of a PARTNER's performance under this agreement will not constitute a continuous waiver of any other provision. An amendment made to any article or section of this agreement does not constitute an amendment to or negate all other articles or sections of this agreement.
94. A delay or omission to exercise a right or power due to a default does not negate the use of that right or power in the future when deemed necessary.
95. If any PARTNER defaults in its OBLIGATIONS, a non-defaulting PARTNER will request in writing that the default be remedied within 30 calendar days. If the defaulting PARTNER fails to do so, the non-defaulting PARTNER may initiate dispute resolution.
96. PARTNERS will first attempt to resolve agreement disputes at the PROJECT team level. If they cannot resolve the dispute themselves, the CALTRANS district director and the executive officer of CITY will attempt to negotiate a resolution. If PARTNERS do not reach a resolution, PARTNERS' legal counsel will initiate mediation. PARTNERS agree to participate in mediation in good faith and will share equally in its costs.

Neither the dispute nor the mediation process relieves PARTNERS from full and timely performance of OBLIGATIONS in accordance with the terms of this agreement. However, if any PARTNER stops fulfilling OBLIGATIONS, any other PARTNER may seek equitable relief to ensure that OBLIGATIONS continue.

Except for equitable relief, no PARTNER may file a civil complaint until after mediation, or 45 calendar days after filing the written mediation request, whichever occurs first.

PARTNERS will file any civil complaints in the Superior Court of the county in which the CALTRANS district office signatory to this agreement resides. The prevailing PARTNER will be entitled to an award of all costs, fees, and expenses, including reasonable attorney fees as a result of litigating a dispute under this agreement or to enforce the provisions of this article including equitable relief.

97. PARTNERS maintain the ability to pursue alternative or additional dispute remedies if a previously selected remedy does not achieve resolution.
98. If any provisions in this agreement are deemed to be, or are in fact, illegal, inoperative, or unenforceable, those provisions do not render any or all other agreement provisions invalid, inoperative, or unenforceable, and PARTNERS will automatically sever those provisions from this agreement.
99. PARTNERS intend this agreement to be their final expression and supersede any oral understanding or writings pertaining to OBLIGATIONS.
100. If during performance of WORK additional activities or environmental documentation is necessary to keep PROJECT in environmental compliance, PARTNERS will amend this agreement to include completion of those additional tasks.
101. PARTNERS will execute a formal written amendment if there are any changes to OBLIGATIONS.
102. This agreement will terminate upon OBLIGATION COMPLETION or an amendment to terminate this agreement, whichever occurs first.

However, all indemnification, document retention, audit, claims, environmental commitment, legal challenge, and ownership articles will remain in effect until terminated or modified in writing by mutual agreement.

103. The following documents are attached to, and made an express part of this agreement: SCOPE SUMMARY, FUNDING SUMMARY.

DEFINITIONS

CALTRANS – The California Department of Transportation

CALTRANS STANDARDS – CALTRANS policies and procedures, including, but not limited to, the guidance provided in the *Guide to Capital Project Delivery Workplan Standards* (previously known as WBS Guide) available at <http://www.dot.ca.gov/hq/projmgmt/guidance.htm>.

CEQA (California Environmental Quality Act) – The act (California Public Resources Code, sections 21000 et seq.) that requires state and local agencies to identify the significant environmental impacts of their actions and to avoid or mitigate those significant impacts, if feasible.

CFR (Code of Federal Regulations) – The general and permanent rules published in the Federal Register by the executive departments and agencies of the federal government.

COOPERATIVE AGREEMENT CLOSURE STATEMENT – A document signed by PARTNERS that verifies the completion of all OBLIGATIONS included in this agreement and in all amendments to this agreement.

COST – The responsibility for cost responsibilities in this agreement can take one of three assignments:

- **OBLIGATIONS COST** – A cost associated with fulfilling OBLIGATIONS that will be funded as part of this agreement. The responsibility is defined by the funding commitments in this agreement.
- **PROJECT COST** – A cost associated with PROJECT that can be funded outside of OBLIGATIONS. A PROJECT COST may not necessarily be part of this agreement. This responsibility is defined by the PARTNERS' funding commitments at the time the cost is incurred.
- **PARTNER cost** – A cost that is the responsibility of a specific PARTNER, independent of PROJECT.

FHWA – Federal Highway Administration

FHWA STANDARDS – FHWA regulations, policies and procedures, including, but not limited to, the guidance provided at www.fhwa.dot.gov/topics.htm.

FUNDING PARTNER – A PARTNER that commits a defined dollar amount to fulfill OBLIGATIONS. Each FUNDING PARTNER accepts responsibility to provide the funds identified on the FUNDING SUMMARY under its name.

FUNDING SUMMARY – The table that designates an agreement's funding sources, types of funds, and the PROJECT COMPONENT in which the funds are to be spent. Funds listed on the FUNDING SUMMARY are "not-to-exceed" amounts for each FUNDING PARTNER.

GAAP (Generally Accepted Accounting Principles) – Uniform minimum standards and guidelines for financial accounting and reporting issued by the Federal Accounting Standards Advisory Board that serve to achieve some level of standardization. See <http://www.fasab.gov/accepted.html>.

HM-1 – Hazardous material (including, but not limited to, hazardous waste) that may require removal and disposal pursuant to federal or state law whether it is disturbed by PROJECT or not.

HM-2 – Hazardous material (including, but not limited to, hazardous waste) that may require removal and disposal pursuant to federal or state law only if disturbed by PROJECT.

HM MANAGEMENT ACTIVITIES – Management activities related to either HM-1 or HM-2 including, without limitation, any necessary manifest requirements and disposal facility designations.

IMPLEMENTING AGENCY – The PARTNER responsible for managing the scope, cost, and schedule of a PROJECT COMPONENT to ensure the completion of that component.

IQA (Independent Quality Assurance) – Ensuring that IMPLEMENTING AGENCY’s quality assurance activities result in WORK being developed in accordance with the applicable standards and within an established Quality Management Plan (QMP). IQA does not include any work necessary to actually develop or deliver WORK or any validation by verifying or rechecking work performed by another partner.

NEPA (National Environmental Policy Act of 1969) – The federal act that establishes a national policy for the environment and a process to disclose the adverse impacts of projects with a federal nexus.

OBLIGATION COMPLETION – PARTNERS have fulfilled all OBLIGATIONS included in this agreement, and all amendments to this agreement, and have signed a COOPERATIVE AGREEMENT CLOSURE STATEMENT.

OBLIGATIONS – All responsibilities included in this agreement.

OBLIGATIONS COST – See COST.

OMB (Office of Management and Budget) – The federal office that oversees preparation of the federal budget and supervises its administration in Executive Branch agencies.

PARTNER – Any individual signatory party to this agreement.

PARTNERS – The term that collectively references all of the signatory agencies to this agreement. This term only describes the relationship between these agencies to work together to achieve a mutually beneficial goal. It is not used in the traditional legal sense in which one PARTNER’s individual actions legally bind the other partners.

PROJECT – The undertaking of improvements to the Hollister Avenue/State Route 217 and Hollister Avenue/Kellogg Avenue intersections and the extensions of Ekwil Street and Fowler Road in the City of Goleta.

PROJECT COMPONENT – A distinct portion of the planning and project development process of a capital project as outlined in California Government Code, section 14529(b).

- **PID (Project Initiation Document)** – The activities required to deliver the project initiation document for PROJECT.
- **PA&ED (Project Approval and Environmental Document)** – The activities required to deliver the project approval and environmental documentation for PROJECT.
- **PS&E (Plans, Specifications, and Estimate)** – The activities required to deliver the plans, specifications, and estimate for PROJECT.
- **R/W (Right of Way) SUPPORT** – The activities required to obtain all property interests for PROJECT.
- **R/W (Right of Way) CAPITAL** – The funds for acquisition of property rights for PROJECT.

- **CONSTRUCTION SUPPORT** – The activities required for the administration, acceptance, and final documentation of the construction contract for PROJECT.
- **CONSTRUCTION CAPITAL** – The funds for the construction contract.

PROJECT COST – See COST.

PROJECT MANAGEMENT PLAN – A group of documents used to guide a project's execution and control throughout that project's lifecycle.

PS&E (Plans, Specifications, and Estimate) – See PROJECT COMPONENT.

QMP (Quality Management Plan) – An integral part of the Project Management Plan that describes IMPLEMENTING AGENCY's quality policy and how it will be used.

R/W (Right of Way) CAPITAL – See PROJECT COMPONENT.

R/W (Right of Way) SUPPORT – See PROJECT COMPONENT.

SAFETEA-LU – Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users

SCOPE SUMMARY – The attachment in which each PARTNER designates its commitment to specific scope activities within each PROJECT COMPONENT as outlined by the *Guide to Capital Project Delivery Workplan Standards* (previously known as WBS Guide) available at <http://www.dot.ca.gov/hq/projmgmt/guidance.htm>.

SHS (State Highway System) – All highways, right of way, and related facilities acquired, laid out, constructed, improved, or maintained as a state highway pursuant to constitutional or legislative authorization.

SPONSOR – Any PARTNER that accepts the responsibility to establish scope of PROJECT and the obligation to secure financial resources to fund PROJECT. SPONSOR is responsible for adjusting the PROJECT scope to match committed funds or securing additional funds to fully fund the PROJECT scope. If a PROJECT has more than one SPONSOR, funding adjustments will be made by percentage (as outlined in Responsibilities). Scope adjustments must be developed through the project development process and must be approved by CALTRANS as the owner/operator of the SHS.

WORK – All scope activities included in this agreement.

CONTACT INFORMATION

The information provided below indicates the primary contact data for each PARTNER to this agreement. PARTNERS will notify each other in writing of any personnel or location changes. Contact information changes do not require an amendment to this agreement.

The primary agreement contact person for CALTRANS is:
Paul Martinez, Project Manager
50 Higuera Street
San Luis Obispo, California 93401
Office Phone: (805) 549-3407

The primary agreement contact person for CITY is:
Steven Wagner, Community Services Director
130 Cremona Drive, Suite B
Goleta, California 93117
Office Phone: (805) 961-7500

SIGNATURES

PARTNERS declare that:

1. Each PARTNER is an authorized legal entity under California state law.
2. Each PARTNER has the authority to enter into this agreement.
3. The people signing this agreement have the authority to do so on behalf of their public agencies.

STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

CITY OF GOLETA

APPROVED

APPROVED

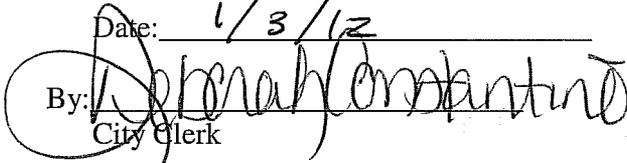
By: 
 RICHARD KRUMHOLZ
 District Director

By: 
 Daniel Singer
 City Manager

Date: 1/4/12

Date: 1/3/12

CERTIFIED AS TO FUNDS:

By: 
 City Clerk

By: 
 Julia Bolger
 Resource Manager

Date: 1/3/12

Date: 1/4/12

APPROVED AS TO FORM AND PROCEDURE

By: 
 City Counsel

Date: 12/28/11

SCOPE SUMMARY

4	5	6	7	8	Description	CALTRANS	CITY	N/A
3	185				Prepare Base Maps and Plan Sheets for PS&E Development		X	
4	195				Right of Way Property Management and Excess Land		X	
4	200				Utility Relocation		X	
3	205				Permits, Agreements during PS&E Component	X	X	
		05			Required permits		X	
		10			NOTE: all permits under 3.205.10 are addressed in the text of this agreement.			
		15			Railroad Agreements		X	
		25			Agreement for Material Sites		X	
		30			Executed Maintenance Agreement		X	
		45			MOU From Tribal Employment Rights Office (TERO)		X	
		55			NEPA Delegation	X		
4	220				RIGHT OF WAY ENGINEERING		X	
4	225				Obtain Right of Way Interests for Project Right of Way Certification		X	
3	230				Prepare Draft Plans, Specifications, and Estimates	X	X	
		05			Draft Roadway Plans		X	
		10			Draft Highway Planting Plans		X	
		15			Draft Traffic Plans		X	
		20			Transportation Management Plan		X	
		25			Draft Utility Plans		X	
		30			Draft Drainage Plans		X	
		35			Draft Specifications		X	
		40			Draft Plans, Specifications, and Estimates Quantities and Estimates		X	
		55			Structures Draft Plans, Specifications, and Estimates Incorporation		X	
		60			Updated Project Information for Plans, Specifications, and Estimates Package		X	
		90			NEPA Delegation	X		
		99			Other Draft Plans, Specifications, and Estimates Products		X	
3	235				Mitigate Environmental Impacts and Clean Up Hazardous Waste	X	X	
		05			Environmental Mitigation		X	
		10			Detailed Site Investigation for Hazardous Waste		X	
		15			Hazardous Waste Management Plan		X	
		20			Hazardous Waste Plans, Specifications, and Estimates		X	
		25			Hazardous Waste Clean-Up		X	
		30			Hazardous Substances Disclosure Document (HSDD)		X	
		35			Long Term Mitigation Monitoring		X	
		40			Updated Environmental Commitments Record		X	

		45		NEPA Delegation	X		
3	240			Draft Structures Plans, Specifications, and Estimates		X	
4	245			Post Right of Way Certification Work		X	
3	250			FINAL STRUCTURES PS&E PACKAGE		X	
3	255			Circulate, Review, and Prepare Final District Plans, Specifications, and Estimates Package	X	X	
		05		Circulated and Reviewed Draft District Plans, Specifications, and Estimates Package		X	
		10		Updated Plans, Specifications, and Estimates Package		X	
		15		Environmental Re-Evaluation	X		
		20		Final District Plans, Specifications, and Estimates Package		X	
		25		Geotechnical Information Handout		X	
		30		Materials Information Handout		X	
		35		Construction Staking Package and Control		X	
		40		Resident Engineer's Pending File		X	
		45		NEPA Delegation	X		
		50		Secured Lease for Resident Engineer Office Space or Trailer		X	
		55		Contractor Outreach		X	
		65		Right of Way Certification Document		X	
		70		Right of Way Engineering Products		X	
		75		Upgraded/Updated Right of Way Certification Document		X	
3	260			Contract Bid Documents Ready to List		X	
3	265			Awarded and Approved Construction Contract			X

FUNDING SUMMARY

Funding Source	Funding Partner	Fund Type	PS&E	R/W Capital	R/W Support	Subtotal Support	Subtotal Capital	Subtotal	Funds Type
STATE	CITY	STIP/RIP	\$980,000	\$3,581,000	\$0	\$980,000	\$3,581,000	\$4,561,000	
LOCAL	CITY	City	\$0	\$906,105	\$100,000	\$100,000	\$906,105	\$1,006,105	
		Subtotals by Component	\$980,000	\$4,487,105	\$100,000	\$1,080,000	\$4,487,105	\$5,567,105	